

Merton Council

Planning Applications Committee Agenda

Membership

Councillors:

Aidan Mundy (Chair)
Simon McGrath (Vice-Chair)
Thomas Barlow
Sheri-Ann Bhim
Michael Butcher
Edward Foley
Susie Hicks
Dan Johnston
Gill Manly
Martin Whelton

Substitute Members:

Caroline Charles
Kirsten Galea
Nick McLean
Stephen Mercer
Stuart Neaverson
Matthew Willis

Date: Thursday 18 August 2022

Time: 7.15 pm

Venue: Council Chamber, Merton Civic Centre, London Road, Morden, SM4 5DX

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Planning Applications Committee Agenda

18 August 2022

- | | | |
|---|----------------------------------------------------------------------------------------------------------------------------------------|-----------|
| 1 | Apologies for absence | |
| 2 | Declarations of Pecuniary Interest | |
| 3 | Minutes of the previous meeting | 1 - 8 |
| 4 | Town Planning Applications | |
| | The Chair will announce the order of Items at the beginning of the Meeting. | |
| | A Supplementary Agenda with any modifications will be published on the day of the meeting. | |
| | Note: there is no written report for this item | |
| 5 | Burlington Gate, 42 Rothesay Avenue, Wimbledon Chase, SW20 8JU | 9 - 38 |
| | <i>Application No: 21/P3292</i> | |
| | <i>Ward: Merton Park</i> | |
| | <i>Officer Recommendation: Grant Permission subject to conditions and s.106 legal agreement.</i> | |
| 6 | 9 Hamilton Road, South Wimbledon, SW19 1JD | 39 - 62 |
| | <i>Application No: 21/P3286</i> | |
| | <i>Ward: Abbey</i> | |
| | <i>Officer Recommendation: Grant Permission subject to conditions and S106 Agreement.</i> | |
| 7 | 19 Worple Road, Wimbledon, SW19 4JS | 63 - 128 |
| | <i>Application No: 21/P0400</i> | |
| | <i>Ward: Hillside</i> | |
| | <i>Officer Recommendation: GRANT Planning Permission Subject to completion of a S106 Agreement, and conditions</i> | |
| 8 | Sandham House, Boundary Business Court, 92 - 94 Church Road, Mitcham, CR4 3TD | 129 - 160 |
| | <i>Application No: 21/P2571</i> | |
| | <i>Ward: Cricket Green</i> | |
| | <i>Recommendation: Grant planning permission subject to relevant conditions and a s106 agreement for highway alterations/adoption.</i> | |
| 9 | Sandham House, Boundary Business Court, 92 - 94 Church Road, Mitcham, CR4 3TD | 161 - 176 |
| | <i>Application No: 21/P2570</i> | |
| | <i>Ward: Cricket Green</i> | |
| | <i>Recommendation: Grant advertisement consent subject to conditions</i> | |

10	Blagdons Sports Ground Beverley Way New Malden KT3 4PU <i>Application No: 21/P4190</i> <i>Ward: West Barnes</i> <i>Recommendation: Grant Planning Permission subject to any direction from the Mayor of London and conditions.</i>	177 - 236
11	Planning Appeal Decisions Officer Recommendation: That Members note the contents of the report.	237 - 240
12	Planning Enforcement - Summary of Current Cases Officer Recommendation: That Members note the contents of the report.	241 - 248
13	Glossary of Terms	249 - 254

Note on declarations of interest

Members are advised to declare any Disclosable Pecuniary Interest in any matter to be considered at the meeting. If a pecuniary interest is declared they should withdraw from the meeting room during the whole of the consideration of that matter and must not participate in any vote on that matter. For further advice please speak with the Managing Director, South London Legal Partnership.

Agenda Item 3

All minutes are draft until agreed at the next meeting of the committee/panel. To find out the date of the next meeting please check the calendar of events at your local library or online at www.merton.gov.uk/committee.

PLANNING APPLICATIONS COMMITTEE

14 JULY 2022

(7.20 pm - 10.28 pm)

PRESENT Councillors (in the Chair), Councillor Aidan Mundy, Councillor Simon McGrath, Councillor Edward Foley, Councillor Thomas Barlow, Councillor Sheri-Ann Bhim, Councillor Susie Hicks, Councillor Dan Johnston, Councillor Stuart Neaverson, Councillor Gill Manly and Councillor Martin Whelton

ALSO PRESENT Jonathan Berry (Interim Head of Development Management and Building Control), Tim Bryson (Development Control Team Leader – North), Stuart Adams (Development Control Team Leader – South), Leigh Harrington (Planning Officer), Andrew Robertson (Head of Democracy and Electoral Services) and Amy Dumitrescu (Democracy Services Manager)

1 APOLOGIES FOR ABSENCE (Agenda Item 1)

Apologies for absence were received from Councillor Charles. Councillor Neaverson attended as substitute.

2 DECLARATIONS OF PECUNIARY INTEREST (Agenda Item 2)

Councillor Barlow declared that he had taken part in the consultation for agenda item 8, Oriel House and therefore would not be voting on that item.

3 MINUTES OF THE PREVIOUS MEETING (Agenda Item 3)

RESOLVED: That the minutes of the meeting held on 16 June 2022 are agreed as an accurate record.

4 TOWN PLANNING APPLICATIONS (Agenda Item 4)

The Committee noted the amendments and modifications to the officer's report. The Chair advised that items would be taken in the published agenda order.

5 LAND REAR OF 20 PELHAM ROAD, WIMBLEDON, SW19 1SX (Agenda Item 5)

The Development Control Team Leader (North) presented the report, noting that an additional condition had been added to the application.

The Committee received presentations from two objectors, who made points including:

- The proposal was inappropriate
- There were concerns regarding the impact of the building on nearby properties
- The proposal would increase density in an already crowded area
- The proposal would breach Policy D5 of the London Plan
- The steps are insufficiently able to accommodate a stairlift
- Access to the public footpath would cease during construction works and this footpath forms a main route to local schools
- An Equality Impact Assessment should be undertaken
- Greening works within the alleyway which had been undertaken by residents would be damaged by the proposed works and a condition should be imposed to repair any damage caused
- The proposal would have an adverse effect on the local community
- There were concerns relating to the removal of green spaces and noise pollution
- The proposal would have an effect on local wildlife and remove charm and character in a conservation area

The Applicant spoke in response and raised points including:

- The application sought to provide an additional home and met the needs of the relevant plan policies
- The design was contemporary and low impact to the surroundings
- The property would not overlook any neighbours
- The site was within a Conservation area however would not be detrimental to the street frontage or the area as a whole and would not be seen from Pelham Road
- The development would be sustainable, designed in accordance with passivhaus standards. The property would be car-free.
- The application was supported by a construction and logistics plan

Councillor Brunt, Ward Councillor spoke to raise concern about the proximity of the proposed building to neighbouring properties, ambient noise, loss of privacy and vehicular access. Councillor Brunt queried where refuse containers would be collected from as there is no clear curtilage for collection and questioned whether the green roof would be used as an extension of the garden and therefore cause issues of overlooking. Councillor Brunt felt the proposal was not adding to the area and proposed a number of possible conditions.

The Development Control Team Leader (North) responded to the points raised noting that the covenants were not a material Planning consideration and that in terms of precedent, each application is assessed on its' own merits. Highways officers had been consulted in relation to the footpath and had advised that a licence to close the highway would be required if permission were granted. The green roof would not be used as an outdoor space and the application contained a condition which restricted that.

In response to questions from the Committee, Planning Officers advised that:

- As part of the landscaping scheme, mitigation with tree planting could be put in place
- Not every scheme could provided the outdoor space provision detailed within policy DMD2 and it would be a judgement for the Committee whether this was deemed sufficient for this proposal
- Environmental Health condition a standard noise limit for construction works
- As is standard for basement applications, there is a condition within the application seeking further detail on the drainage scheme
- If any damage were caused to the highway as part of the development this would be required to be returned to the prior standard, however it might be unreasonable to add planting in the alleyway as wider public realm improvements as part of this development

Members commented on the application raising concern around diminution of amenity, welcoming the use of cross-laminated timber and noting that the loss of a private garden would not be a reason for refusal particularly as the area is well served by parks and public spaces.

In regards to conditions, members requested that a condition be added to retain access to the public footpath and replacement of lost trees.

Planning Officers advised that in regards to the public footpath, the condition would be pursued as far as possible if achievable and that the landscaping condition would be made more robust.

With the two additional conditions, the recommendation was put to the vote and it was

RESOLVED:

That the Committee GRANTED planning permission subject to conditions and S106 Agreement

6 43 WOODSIDE, WIMBLEDON, SW19 7AF (Agenda Item 6)

The Development Control Team Leader (North) presented the report.

The Committee received a presentation from one objector who raised points including:

- The area mainly consisted of family dwellings and a large HMO (House of Multiple Occupation) would be unsuitable
- The proposal would change the character of the area, with no other HMOs in the immediate vicinity and have a detrimental effect on the neighbourhood
- The use of the property garden by the occupants would lead to excessive noise
- The new entrance would be sited opposite a nursery which was an unsuitable location

- The size of the shared kitchen/diner would be unable to accommodate 8 households and there were only 3 bathrooms included in the proposal.

The Application responded to the points raised and advised:

- The proposed site was close to public transport and provided one off-street parking space
- The proposal met a local need in providing affordable accommodation
- The property had a large rear garden and the proposal would require no additional works as the property already contained 8 bedrooms
- All rooms could be used as single occupancy and the property was already designed to host 8 people
- The property had a large dining space and outdoor amenity
- The property is detached and would be let out to working professionals and the level of noise was not expected to increase
- Security cameras were sited at entrance points

The Development Control Team Leader (North) responded to the points raised noting that officers would be guided by the Merton HMO 2021 guidance document and the application did conform to the requirements within this guidance. The property would not be occupied by children or families and therefore an increase in noise would be less likely, however noise nuisance could be reported to the Council.

In response to questions from members, Planning Officers responded that the proposal was for 8 occupants, however there may be occasions where there were more people in the property, however should a complaint be submitted that there were regularly more than 8 persons within the property, the Planning Enforcement team could assess this and this would also lead to a breach of the HMO licence by the Applicant. An informative could be added to the application to enforce that the property must only be occupied by a maximum of 8 persons.

Members commented on the proposal, requesting that a further condition be placed on the application to ensure that the refuse bins outside the property were capable of holding the waste generated and raising concerns that there would be more than 8 persons in the property.

The Interim Head of Development Management and Building Control responded that a condition could be imposed in relation to the refuse space and the details would be referred to the waste team as well as informing them of the views of the Committee. It could also be conditioned that the development implementation be entirely in accordance with the plans.

The Chair moved to the vote and it was

RESOLVED:

That the Committee GRANTED Planning permission subject to conditions and S106 Agreement

7 9 LANCASTER ROAD, WIMBLEDON VILLAGE, LONDON, SW19 5DA
(Agenda Item 7)

The Development Control Team Leader (North) presented the report.

Councillor Max Austin, Ward Councillor spoke on the application and raised points including:

- Residents had raised concerns over the scale of the site which was located in a small cul-de-sac
- The development would be reliant on the daily use of HGVs and this would lead to issues with the accessibility of the cul-de-sac particularly for emergency service vehicles, carers and other visitors
- If approved, the application would require a construction management plan which protected the cul-de-sac which required constant access to and from the road

The Development Control Team Leader (North) responded that whilst officers could never fully mitigate against the effects of construction works, condition 9 had been imposed to assist with those issues.

In response to questions from members, officers advised that the green area was outside of the application area and therefore this restricted the ability of officers to impose conditions as this would be a third party civil issue. There would be a land ownership process to go through with a requirement to serve notice and an informative could be added to advise the applicant of this. The documents could be circulated for comments in consultation with the members of the Committee and ward councillors. Members agree that they would like to proceed with this approach and requested a condition be added to the application to this effect.

The Chair moved to the vote including the additional condition and it was

RESOLVED:

That the Committee GRANTED planning permission subject to conditions.

8 ORIEL HOUSE, 26 THE GRANGE, WIMBLEDON, SW19 4PS (Agenda Item 8)

The Development Control Team Leader (North) presented the report, noting that a further condition had been added as detailed within the modifications sheet.

In response to questions from members, officers advised that a flood risk and structural engineer had been consulted on the application and an information had been added to the application in regards to swift boxes.

The Chair moved to the vote and it was

RESOLVED

That the Committee GRANTED permission subject to conditions.

The meeting was adjourned briefly between 21.06 and 21.16

9 HADLEY ROAD COMMUNITY ALLOTMENT, MITCHAM, CR4 1LG (Agenda Item 9)

The Planning Officer presented the report.

Members raised concerns regarding the consultation between ploholders and the landowner of the site. Planning officers advised that the land was Council property with the Allotment Association taking over the management of it. Planning officers advised that this would be a legal agreement and not a planning matter however a condition could be added that a legal agreement for terms of use and defining the relationship between the landowner and the users be drawn up and submitted under a condition under a discharge of condition application. Officers would make the Committee aware once this had been received.

Members requested that this also include confirming that their constitution had been followed.

Members commented on the application and the Chair moved to the vote.

RESOLVED:

That the Committee GRANTED permission subject to relevant conditions.

10 TPO 5 PARKSIDE AVENUE, WIMBLEDON, SW19 5ES (Agenda Item 10)

The Interim Head of Development Management and Building Control presented the report.

RESOLVED:

That the Merton (No.777) Tree Preservation Order 2022 be confirmed without modification.

11 TPO 1 WEIR ROAD, WIMBLEDON, SW19 8UG (Agenda Item 11)

The Interim Head of Development Management and Building Control presented the report.

In response to a question from members regarding whether trees being cut back and then cut down could be prevent elsewhere, officers advised that they would look at tree retention schemes and conditions to fence off trees and serve TPOs where applicable.

RESOLVED:

Recommendation: That the Merton (No.772) Tree Preservation Order 2022 be confirmed but be modified by removing T1 (Elder) from the Order.

12 TPO 296 COOMBE LANE, RAYNES, PARK, SW20 0RW (Agenda Item 12)

The Interim Head of Development Management and Building Control presented the report.

In response to questions from members, officers advised that if a tree were causing a structural problem this would be a major factor weighing against the retention of it, however in this case, officers felt this could be monitored and if cut back sufficiently the tree shouldn't cause damage and the tree was not causing sufficient damage that would outweigh the visual contribution of the tree.

RESOLVED:

Recommendation: That the Merton (No.773) Tree Preservation Order 2022 be confirmed without modification.

13 ADVERTISING PANEL OUTSIDE 87 THE BROADWAY, LONDON, SW19 1QE (Agenda Item 13)

The Development Control Team Leader (North) presented the report.

Councillor Fairclough, Ward Councillor spoke on the item and raised points including:

- The panel was located in an area of high footfall and there were a number of other pieces of street furniture on the pavement
- A number of groups had objected to the application on numerous grounds
- Councillor Fairclough quoted para 127 of the National Planning Policy Framework stating spaces should be safe inclusive and accessible and noting that guidance stated design of approach routes should meet the needs of wheelchair users and persons with visual impairments
- Circumstances had changed since 2016 particularly post-Covid

In response, the Development Control Team Leader (North) advised that the highways officer had assessed the application and requested it to be cited further to increase pedestrian space however had not objected to the application. Technical advice had been sought and officers were satisfied with it in terms of pedestrian safety.

In response to questions from members, officers advised that the previous application for citing the panel had been granted in 2016 and permissions lasted for 5 years. If granted, the panel would remain in place for an additional 5 years, unless

complaints were received which led to enforcement action. Permission had to be granted or refused based on the plan submitted, however officers noted that the area of land was Council owned and Councillors would be able to contact the appropriate Council departments to request movement of bicycle racks and other street furniture.

In response to further questions, officers advised that if permission were not granted, the consent had run out, noting the 5 years was a standard term set out in legislation. If that consent were not renewed, the panel could be discontinued and removed. Relocation of the panel would require a separate planning application.

Members raised concerns about the space for pedestrians.

The Chair moved to the vote on the officer recommendation to grant the application. The vote fell and it was therefore proposed to refuse the application on the basis it was not safe or inclusive and affected the amenity of future users.

The Chair moved to the vote to refuse and it was

RESOLVED: The Committee agreed to:

1. REFUSE the application for the following reasons: That the proposal, by reason of its location and size, does not accord with the obligation to create places that are safe, inclusive and accessible which promote health and well-being and high standards of amenity for existing and future users. The proposal is therefore in conflict with Policies DM D2 and DM D5 of the Sites and Policies Plan 2014, Policy CS14 of the Core Strategy 2011, Policies D4 and D8 of the London Plan 2021 and Paragraph 130 of the NPPF 2021.

2. DELEGATE to the Director of Environment & Regeneration the authority to make any appropriate amendments in the context of the above to the wording of the grounds of refusal including references to appropriate policies

14 PLANNING APPEAL DECISIONS (Agenda Item 14)

The report was noted.

15 PLANNING ENFORCEMENT - SUMMARY OF CURRENT CASES (Agenda Item 15)

The report was noted.

PLANNING APPLICATIONS COMMITTEE
18th August 2022

Item No:

<u>UPRN</u>	<u>APPLICATION NO.</u>	<u>DATE VALID</u>
	21/P3292	02/11/2020
Address/Site	Burlington Gate 42 Rothesay Avenue Wimbledon Chase SW20 8JU	
(Ward)	Merton Park	
Proposal:	CONVERSION OF ROOFSPACE OF SOUTH BLOCK, WITH AN INCREASE IN HEIGHT OF THE RIDGELINE BY 2M, TO PROVIDE 3 X SELF-CONTAINED FLATS (1B, 2P) WITH ASSOCIATED WORKS, INCLUDING THE FORMATION OF A NEW ROOF TERRACE, CYCLE STORE AND LANDSCAPING.	
Drawing Nos:	P-Si-D-011 Rev B, P-R2-D-014 Rev D, P-04-D-015 Rev D, P-R-D-016 Rev D, E-E/N-D-017 Rev D, E-S/W-D-018 Rev D and X-AA-D-019/1 Rev D.	
Contact Officer:	Tim Lipscomb (0208 545 3496)	

RECOMMENDATION

Grant Permission subject to conditions and s.106 legal agreement.

CHECKLIST INFORMATION

- Heads of Agreement: Yes, restrict parking permits.
- Is a screening opinion required: No
- Is an Environmental Statement required: No
- Has an Environmental Statement been submitted: No
- Press notice: No
- Site notice: No
- Design Review Panel consulted: No
- Number of neighbours consulted: 72
- External consultations: No
- Conservation area: No
- Listed building: No
- Tree protection orders: No
- Controlled Parking Zone: Yes (MP2)

- 1.1 This application is being brought to the Planning Applications Committee for determination due to the number of objections contrary to the officer recommendation. This proposal does not qualify to be considered under any permitted development or prior approval process for the erection of extensions of up to two additional storeys to flatted blocks, as there is some debate as to the building's original construction date and the proposed internal floor to ceiling height being higher than in parts of the existing building, both of which are restricting factors in the prior approval assessment.

2. **SITE AND SURROUNDINGS**

- 2.1 The application site comprises a gated residential development located at the end of Rothesay Avenue, which provides the entrance to Wimbledon Chase Train Station. The site adjoins the train line to the west, with residential dwellings to the east and south. The site has an area of 0.19ha.
- 2.2 The existing development comprises 34 one and two bedroom flats within two separate blocks. The northern block consists of a three storey building containing 10 flats. While the larger southern block (the subject of this application) is built over four storeys, with a semi-basement car park, incorporating 24 flats. The site was previously industrial land, which had been converted in the 1990's through extensions and refurbishment. The southern block, the subject of this application has an eaves height of 11.4m and a height to the ridge of 13.8m (with a rooftop conservatory extending above this, to a height of 15.6m).
- 2.3 The larger block of flats accommodates a shared terrace at the 4th floor as a communal amenity space for the residents (146sqm). There is also a space to the northeast of the building, adjacent to the rear of properties on Sandringham Avenue, of approximately 75sqm, this is currently not used for communal amenity. The residents from the smaller block of flats share a rear garden at ground level to the rear of the building.
- 2.4 Car park spaces are located at street level and at basement level underneath the larger block of flats.
- 2.5 The site is not located within a conservation area, nor is it within the setting of a listed building. The site benefits from a PTAL rating of 3 and is within a Controlled Parking Zone. The site is within Flood Zone 1 (low risk).
- 2.6 The site is subject to the following planning constraints:
- Flood Zone 1
 - PTAL 3
 - Controlled Parking Zone MP2
 - Adjacent to green corridor (railway embankment)
 - Adjacent to Site of Importance for Nature Conservation (railway embankment)

3. **PROPOSAL**

- 3.1 The proposal is for a rooftop extension to provide three flats, each with at least one balcony/roof terrace. The roof extension would effectively extend the existing mono-pitch roof to allow for new accommodation within the roofspace. The proposed rooftop extension will maintain the existing eaves line with the

roof above enlarged and increased in height by approximately 2.2m (up to a ridge height of 16.0m – from an existing height of 13.8m). The angle of the roof pitch would rise from 35 degrees to 44 degrees.

- 3.2 The proposed flats would be served by dormer features inserted into the enlarged roofscape. The proposed roof tiles would match the existing
- 3.3 Each of the three proposed units would be dual aspect but no windows would be positioned in the northeast facing elevation (towards properties on Sandringham Avenue).
- 3.4 The roof addition would reduce the size of the existing communal roof terrace, with a resultant space of 69sqm but with an enhanced offering of planting and seating - approximately 21 potted plants of varying maturity up to 2m in height along with five heavy duty benches. An existing strip of land to the northeast of the building would be re-landscaped to provide an additional external amenity space of approximately 52sqm, although this space exists currently, it is not landscaped to form useable amenity space or used as amenity space.
- 3.5 A new landscaped strip to the perimeter fence to the frontage with Rothesay Avenue is proposed.
- 3.6 Bike and bin enclosures (6 cycle parking spaces) would be provided adjacent to the smaller block of flats on site. Servicing would be carried out in the same way as for the existing flatted units on site.
- 3.7 The proposal would provide the following accommodation:

	Type	Habitable rooms	GIA (sqm)	Private external space (sqm)
1	1b/2p	2	53	4
2	1b/2p	2	54.5	7.7
3	1b/2p	2	58	4

- 3.8 The application was amended by way of revised plans on the 29th December 2021. A revised Daylight and Sunlight Assessment, to reflect the changes made to the plans was submitted on 22nd June 2022. The amendment does not substantially alter the nature of the proposal, it simply corrects the pitch of the roof of the existing building. The originally submitted plans show a roof pitch marginally lower than existed on site. The building has since been re-surveyed and the existing plans now accurately reflect the roof pitch of the existing building. The proposed plans remain unchanged.
- 3.9 The application is accompanied by the following supporting documents:
 - Statement relating to energy and water usage 23.09.2021
 - Daylight & Sunlight Report updated 22.06.2022
 - Design & Access Statement amended 29.12.2021
 - Draft s.106 agreement (restricting parking permits)
 - Planning Statement August 2021
 - Sustainability Statement August 2021
- 3.10 N.B. It is noted that the application form states that the increase in height of the building would be 2.75m. However, the application is assessed on the basis of the submitted plans which show an increase in ridge height of 2.2m.

4. PLANNING HISTORY

4.1 WIM3202 - WAREHOUSE. Granted 28/07/1937.

WIM5621A - TEMPORARY OFFICES. Granted 25/11/1949.

WIM6087 - CIRCULAR SAW AND MANUFACTURE OF PACKING CASES. Granted 21/08/1951.

91/P0587 - OUTLINE PLANNING APPROVAL TO REDEVELOP EXISTING SITE FOR RESIDENTIAL PURPOSES. Granted 04/09/1991.

92/P0023 - REFURBISHMENT OF EXISTING 3 STOREY WAREHOUSE BUILDING TO PROVIDE 12 NO. 2-BED FLATS 7 NO. 1-BED FLATS AND 5 STUDIO UNITS; INCLUDING ERECTION OF A FOUR-STOREY BUILDING PROVIDING 7 NO. 2-BED FLATS AND 3 NO. 1-BED FLATS; TOGETHER WITH LANDSCAPING WORKS AND PROVISION OF RELATED CAR PARKING. Refused 25/03/1992. Allowed at appeal 09/09/1992.

21/P0181 - APPLICATION TO DETERMINE WHETHER PRIOR APPROVAL IS REQUIRED IN RESPECT OF THE PROPOSED ERECTION OF EXTENSIONS TO ROOFSPACE OF BLOCK 1 TO 24 TO PROVIDE 3 X SELF CONTAINED FLATS WITH ASSOCIATED WORKS. Prior Approval Refused 18/02/2021 for the following reasons:

1. The proposed development, by reason of the buildings original construction date falling before 1st July 1948, would fail to comply with Schedule 2, Part 20, Class A.1 (c) of The Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended).
2. The proposed development, by reason of the additional storey floor to ceiling height exceeding that of the existing floor to ceiling heights of any other existing storeys, would fail to comply with Schedule 2, Part 20, Class A.1 (e)(ii) of The Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended).
3. The proposed development, by reason of the loss of significant external amenity provision, would result in a detrimental impact to enjoyment of the existing resident's amenity, contrary to DMD2 and DMD3 of the Adopted Sites and Policies Plan 2014. The proposal would therefore fail comply with Schedule 2, Part 20, Class A.2 (1)(g) of The Town and Country Planning (General Permitted Development) (England) Order 2015 (as amended).

5. Relevant policies.

5.1 The key policies of most relevance to this proposal are as follows:

5.2 **National Planning Policy Framework (2021)**

2. Achieving sustainable development
4. Decision-making
5. Delivering a sufficient supply of homes
6. Building a strong, competitive economy
7. Ensuring the vitality of town centres
8. Promoting healthy and safe communities
9. Promoting sustainable transport
11. Making effective use of land
12. Achieving well-designed places

- 14. Meeting the challenge of climate change, flooding and coastal change
- 15. Conserving and enhancing the natural environment

5.3 **London Plan (2021):**

- D1 London's form, character and capacity for growth
- D2 Infrastructure requirements for sustainable densities
- D3 Optimising site capacity through the design-led approach
- D4 Delivering good design
- D5 Inclusive design
- D6 Housing quality and standards
- D7 Accessible housing
- D8 Public realm
- D11 Safety, security and resilience to emergency
- D12 Fire safety
- D13 Agent of Change
- D14 Noise
- H1 Increasing housing supply
- H10 Housing size mix
- G6 Biodiversity and access to nature
- G7 Trees and woodlands
- SI 1 Improving air quality
- SI 2 Minimising greenhouse gas emissions
- SI 3 Energy infrastructure
- SI 4 Managing heat risk
- SI 5 Water infrastructure
- SI 7 Reducing waste and supporting the circular economy
- SI 8 Waste capacity and net waste self-sufficiency
- SI 10 Aggregates
- SI 13 Sustainable drainage
- T1 Strategic approach to transport
- T2 Healthy Streets
- T3 Transport capacity, connectivity and safeguarding
- T4 Assessing and mitigating transport impacts
- T5 Cycling
- T6 Car parking
- T6.1 Residential parking
- T7 Deliveries, servicing and construction
- T9 Funding transport infrastructure through planning

5.4 **Merton Local Development Framework Core Strategy – 2011 (Core Strategy)**

Relevant policies include:

- CS 8 Housing choice
- CS 9 Housing provision
- CS 11 Infrastructure
- CS 13 Open space, leisure and nature conservation
- CS 14 Design
- CS 15 Climate change
- CS 17 Waste management
- CS 18 Transport
- CS 19 Public transport
- CS 20 Parking servicing and delivery

5.5 **Merton Sites and Policies Plan – 2014 (SPP)**

Relevant policies include:

- DM H2 Housing mix
- DM H3 Support for affordable housing

DM O2 Nature conservation, Trees, hedges and landscape features
DM D1 Urban Design
DM D2 Design considerations
DM D3 Extensions and alterations to existing buildings
DM EP2 Reducing and mitigating noise
DM EP3 Allowable solutions
DM EP4 Pollutants
DM F2 Sustainable urban drainage systems (SuDS) and; Wastewater and Water Infrastructure
DM T2 Transport impacts of development
DM T3 Car parking and servicing standards
DM T4 Transport infrastructure

5.6 **Supplementary planning considerations**

National Design Guide – October 2019
Draft Merton Local Plan
DCLG: Technical housing standards - nationally described space standard March 2015
Merton's Design SPG 2004
GLA Guidance on preparing energy assessments – 2018
London Environment Strategy - 2018
Mayor's Air Quality Strategy - 2010
Mayor's SPG - Housing 2016
Mayor's SPG – Sustainable Design and Construction 2014
Mayor's SPG – Character and Context 2014
Mayor's SPG – Play and Informal Recreation 2012
LB Merton – Air quality action plan - 2018-2023.
LB Merton - Draft Sustainable Drainage (SUDS) Design and Evaluation Supplementary Planning Document (SPD) 2018
Merton's Waste and Recycling Storage Requirements – A Guidance for Architects
Merton's Small Sites Toolkit SPD 2021

6. CONSULTATION

6.1 Press Notice, Standard 21-day site notice procedure and individual letters to neighbouring occupiers. 37 Representations have been received, raising objection on the following grounds:

Visual impact:

- Excessive scale and massing.
- Greater bulk and massing than the refused Prior Approval scheme (height would be 1m greater than in refused Prior Approval application).
- Additional bulk and massing at roof level to a building that is already significantly taller and dominating than the surrounding two storey dwellinghouses on Rothesay Avenue and Sandringham Avenue.
- Visual harm to character by reason of being the largest building in the area.
- Awkward staircase arrangement at roof level which interrupts the roof form and profile.
- Density too high.
- Query the vagueness of the term “comprehensive landscaping” (and lack of detail in plans).
- The roof pitch would increase from 35 degrees to 60 degrees

Inaccuracies in submission:

- Application form states that the increase in height is 2.75m but a measurement of 2m is given in the development description.
- Incorrect dimensions on plans – the new roof cannot be built at the same pitch as the existing and the new roof could not be lower than the existing conservatory. (existing roof is 33 degrees, proposed would be 42 degrees).
- Request that dimensions be added to plans.
- The heights comparison elevations on page 12 of the Design and Access statement appear to be misleading as the 'existing' elevation drawing is placed on the page at a higher level than the 'proposed' elevation.

Impact on neighbouring amenity and standard of accommodation:

- Overlooking (also perception of overlooking) and loss of light.
- Greater impact on sunlight/daylight to neighbouring properties than indicated in the Daylight and Sunlight Analysis.
- Reduction in rooftop amenity for existing residents and loss of all-weather conservatory both of which are often used. Landscaped strip at ground floor does not get sunlight.
- Query whether the wall around the rooftop amenity space would be sufficiently high to be safe (existing walls are 1.7m high, proposed would be 1.3m high).
- No access to the amenity space by wheelchair and no views available from amenity space for wheelchair users.
- Concerns over means of evacuation from rooftop amenity space.
- Concerns regarding extensive disturbance from construction process, particularly at a time when people are working from home more due to the pandemic. (Noise, dirt, dust, traffic, construction vehicles parking etc)
- Financial compensation will be sought for disturbance from construction works.
- The entire roof would need to be removed to carry out the works and residents could not live there whilst works were being carried out.
- Query whether the flats would meet the relevant internal floor space standards due to sloping roofs.
- Balconies fall short of the minimum size standards in the London Plan.
- Sound insulation to existing windows is poor which would exacerbate noise disturbance from the construction process.

Other matters:

- Concerns over impacts on structural integrity of the building.
- A lift should be included if an additional floor is to be added.
- Query whether relevant sustainability targets would be met.
- The applicant has not referred to the Council's Small Sites Toolkit in their submission.
- The management of refuse is already a major problem that will only worsen with the proposed project.
- Owners of top floor flats paid a premium cost.
- Devaluation of existing flats.
- Company submitting the application does not pay tax in the UK.

- The application does not make it clear how the parking situation would be addressed for the 3 new flats. The existing visitor parking spaces are well used by visitors, workmen, health visitors etc so not available for use by the proposed 3 new flats.
- Concerns over impact on existing drainage infrastructure.
- Concerns there may be asbestos in the building.
- More homes in the area are not needed.
- Any significant change adjacent to the railway embankment is also likely to require comment from Network Rail.
- Location of cycle stores would make it attractive to thieves.
- There is already adequate cycle storage.
- Area already highly populated.
- Difficulties relating to mortgages due to additional floors.
- The revised plans and daylight/Sunlight Analysis does not overcome the concerns previously raised.

6.2 Wimbledon Swift Group:

Highlight the need for the inclusion of Swift friendly design features.

6.3 Internal consultees:

6.5 LBM Highway Officer:

No objection, subject to informatives relating to works on the public highway (INF9 and INF12)

6.6 LBM Transport Officer:

No comments received.

7. **PLANNING CONSIDERATIONS**

7.1 Key Issues for consideration

7.1.1 The key issues in the assessment of this planning application are:

- Principle of development
- Residential density
- Design and impact upon the character and appearance of the area
- Impact on neighbouring amenity
- Standard of accommodation
- Transport, highway network, parking and sustainable travel
- Safety and Security considerations
- Sustainability
- Air quality
- Flooding and site drainage
- S.106 requirements/planning obligations
- Response to issues raised in objection letters

7.2 Principle of development

7.2.1 Policy H1 of the London Plan 2021 states that development plan policies should seek to identify new sources of land for residential development including intensification of housing provision through development at higher densities.

Core Strategy policies CS8 & CS9 seek to encourage proposals for well-designed and conveniently located new housing that will create socially mixed and sustainable neighbourhoods through physical regeneration and effective use of space.

7.2.2 Policy H1 of the London Plan 2021 has set Merton a ten-year housing target of 9,180 new homes.

7.2.3 The proposal to intensify residential use to this site is considered to respond positively to London Plan and Core Strategy planning policies to increase housing supply and optimising sites and the principle of development is considered to be acceptable subject to compliance with the relevant policies of the Development Plan.

7.3 Residential density

7.3.1 London plan policy D3, Optimising site capacity through the design-led approach, sets out that higher density developments should generally be promoted in locations that are well connected to jobs, services, infrastructure and amenities by public transport, walking and cycling.

7.3.2 The London Plan explains that comparing density between schemes using a single measure can be misleading as it is heavily dependent on the area included in the planning application site boundary as well as the size of residential units.

7.3.3 The existing residential density across the site is 244 units per hectare, with the proposed density being 300 units per hectare. Whilst residential density can be a useful tool identifying the impact of a proposed development, officers would advise Members to primarily consider the impact on the character of the area and the amenity of neighbouring occupiers in this assessment.

7.4 Design and impact upon the character and appearance of the area

7.4.1 The NPPF, London Plan policies D3 and D4, Core Strategy policy CS 14 and SPP Policy DM D2 require well designed proposals which make a positive contribution to the public realm, are of the highest quality materials and design and which are appropriate in their context. Thus, development proposals must respect the appearance, materials, scale, bulk, proportions and character of their surroundings.

7.4.2 The proposal would increase the height, scale, bulk and massing of the building. However, the eaves height would remain the same as existing. The additional roof massing would have some limited impact in the streetscene but the additional bulk and massing is not considered to be harmful to the character of the area. The increase in roof pitch would not appear so conspicuous or out of keeping with the area to warrant a refusal of permission.

7.4.3 It is noted that the building is taller than the surrounding two-storey housing and is somewhat of an anomaly in the streetscene. The additional bulk to the roof would be noticeable from surrounding gardens and residential windows and on the approach along Rothesay Avenue. However, the replaced roof would appear proportionate in scale in relation to the existing building.

7.4.4 The proposal is considered to be acceptable in terms of the impact on the character and appearance of the area and would comply with Policies D3 and

D4 of the London Plan, Policy CS14 of the Core Planning Strategy and Policies DMD2 and DMD3 of the Sites and Policies Plan 2014.

7.5 Impact on neighbouring amenity

7.5.1 Policy DM D2 seeks to ensure that development does not adversely impact on the amenity of nearby residential properties.

7.5.2 Privacy and overlooking

7.5.3 The proposal would involve windows at a higher level than exists currently. The main outlook is provided to the northwest and southwest elevations, which look towards the street and the existing car park. Three proposed bedroom windows would face towards the southeast. However, these would be dormer windows, set up the roofslope, which reduces the available angle of viewing. In addition, these windows would be set back further than the existing windows below and there would be no material increase in overlooking as a result of the proposed development.

7.5.4 The proposed flats would not result in material harm to the existing flats below by way of overlooking or loss of privacy as no direct views would be provided.

7.5.5 Loss of light, shadowing and visual intrusion

7.5.6 The proposal involved increasing the roof massing of the already substantial building. However, the eaves level would remain the same as existing and the majority of the additional bulk and massing is focused towards the centre of the building.

7.5.7 There would be some marginal impact on outlook and daylight to all nearby residential properties but the increased ridge height would not be particularly intrusive and this impact is not considered to be materially harmful.

7.5.8 In terms of overshadowing, the properties to the south would not be overshadowed to any material extent. To the immediate east and northeast, the properties would lose some late afternoon sun but not to a significant extent. The existing flatted block to the north would experience a very minor impact on sunlight but due to the separation distances this would not be materially harmful.

7.5.9 The applicant has submitted a daylight and sunlight assessment, which has been revised, as the existing plans had been revised. The assessment concludes that the effects upon adjoining properties daylight/sunlight is de minimis and would not be discernible to the human eye and accords with the relevant guidance. Officers concur with this conclusion and consider that the impacts in terms of daylight and sunlight, would not be materially harmful to neighbouring amenity.

7.5.10 The proposal is considered to be acceptable in terms of the impact on neighbouring amenity, in accordance with Policy DM D2 of the Sites and Policies Plan 2014.

7.7 Standard of Accommodation

7.7.1 Policy D6 of the London Plan states that housing developments should be of the highest quality internally and externally. New residential development should ensure that it reflects the minimum internal space standards (specified as Gross Internal Areas).

- 7.7.2 The proposed units would exceed the minimum GIA set out in the London Plan, which requires 50sqm of floor space per unit.
- 7.7.3 Whilst there is a minimum requirement of 5sqm of private amenity space per unit, if no communal amenity space is provided, the London Plan does not explicitly set out minimum standards for communal amenity space.
- 7.7.4 There is currently approximately 146sqm of amenity space in the form of the existing roof terrace. There is an area to the side of the building, which acts as an informal visual buffer between the building and residential dwellings, however, this area is not landscaped as an amenity area and is shaded throughout much of the afternoon. The proposed layout includes a roof terrace of 69sqm (with an additional area of 52sqm to the side of the building). If the area to the side of the building were included in the existing amenity space there would be a total of 215sqm existing amenity space. The proposal seeks to reduce this to 121sqm. There are 24 flats in the existing building, which would equate to approximately 9sqm of communal amenity space per unit (or 6sqm per flat if the area to the side of the building is discounted, as it is not currently used as amenity space). In the proposed scenario, there would be 27 flats in the building, which would equate to 4.5sqm per unit.
- 7.7.5 The London Plan includes space standards for children's play space – the existing building theoretically requires a minimum of 28sqm of play space. The proposed layout (including 27 flats) would yield a requirement for 32sqm of play space. As the scheme provides in excess of this figure, a refusal based on reduction of communal amenity space could not be substantiated under policy grounds.
- 7.7.6 Whilst the concerns of neighbours are noted, there are no minimum requirements for communal amenity space, other than the provision of children play space outlined above. The proposed units would provide internal floor areas in excess of that required by the space standards and would also provide for a degree of private amenity space for each unit, in addition to communal amenity space. Whilst there is an overall reduction in communal amenity space, subject to conditions to secure landscaping works, to include benches, planting etc, the quality of the communal amenity space would be improved and overall it is concluded that the impact on the living standards of existing flats, in terms of communal amenity space, would be acceptable.
- 7.7.7 The proposed units would provide in excess of the minimum internal space standards. The London Housing SPG sets out that "In exceptional circumstances, where site constraints make it impossible to provide private open space for all dwellings, a proportion of dwellings may instead be provided with additional internal living space equivalent to the area of the private open space requirement. This area must be added to the minimum GIA." Therefore, the principle of providing additional internal floor area in lieu of private external amenity space has some policy support. Therefore, whilst some of the units are marginally under providing external amenity space (and do not include balconies of a minimum depth of 1.5m), this is mitigated by the additional floor area for each unit, over and above the minimum standards.
- 7.7.8 The proposed arrangements would result in the reduction of communal amenity space for existing residents. The proposed communal amenity space on the rooftop can be improved with the addition of landscaping, planting and benches etc. The area to the side of the building is not ideal as an amenity space as it is shaded. However, it would allow for some degree of access for disabled people

(although this demand may be limited given the layout of the building). On balance, it is concluded that the standard of accommodation is acceptable and the proposal would comply with Policy D6 of the London Plan 2021.

7.8 Transport, highway network, parking and sustainable travel

7.8.1 Policy T6 of the London Plan states that car-free development should be the starting point for all development proposals in places that are (or are planned to be) well-connected by public transport. At a local level Policy CS20 requires developers to demonstrate that their development will not adversely affect on-street parking or traffic management. Policies DMT1-T3 seek to ensure that developments do not result in congestion, have a minimal impact on existing transport infrastructure and provide suitable levels of parking.

7.8.2 The proposed development would provide three new dwellings. The site is within a Controlled Parking Zone and therefore, in order to minimise the impact on the local highway network and to minimise impact on parking pressure, officers advise that the application should be subject to a s.106 agreement to preclude the issuing of parking permits to future occupiers.

7.8.3 The proposed development would provide for suitable levels of cycle parking in an accessible location and would meet London Plan requirements.

7.8.4 The concerns raised by neighbours in relation to the increased use of visitor spaces is noted, however, this impact could not reasonably amount to a reason for refusal. Subject to legal agreement and conditions, the proposed development is considered to be acceptable in term of transport and highway impacts.

7.9 Refuse storage and collection

7.9.1 Policies SI8 and SI 10 of the London Plan and policy CS 17 of the Core Strategy requires details of refuse storage and collection arrangements.

7.8.2 A storage area for refuse has been indicated at ground floor level, which provides suitable access to residents and for the transportation of refuse for collection. It is considered this arrangement would be acceptable and a condition requiring its implementation and retention will be included to safeguard this.

7.9 Safety and Security considerations

7.7.1 Policy DMD2 sets out that all developments must provide layouts that are safe, secure and take account of crime prevention and are developed in accordance with Secured by Design principles.

7.7.2 The proposal introduces three new units at roof top level and would not have a significant impact in terms of safety and security considerations.

7.8 Sustainability

7.8.1 London Plan policies SI 2 to SI 5 and CS policy CS15 seek to ensure the highest standards of sustainability are achieved for developments which includes minimising carbon dioxide emissions, maximising recycling, sourcing materials with a low carbon footprint, ensuring urban greening and minimising the usage of resources such as water.

7.8.2 Subject to conditions to secure the necessary details, the proposal is considered to be acceptable in terms of sustainability and climate change considerations.

7.9 Air quality and potentially contaminated land

7.9.1 The whole of Merton is an Air Quality Management Area (AQMA).

7.9.2 Whilst the development is a minor application, as opposed to a major, it is important that the impact on air quality is minimised and therefore, officers recommend conditions relating to the construction process and air quality.

7.10 Flooding and site drainage

7.10.1 Policy SI 13 of the London Plan (Sustainable drainage) sets out that development proposals should aim to achieve greenfield run-off rates and ensure that surface water run-off is managed as close to its source as possible. There should also be a preference for green over grey features.

7.10.2 The site is within Flood Zone 1 (low probability of flooding) and is not within a critical drainage area. However, notwithstanding that, the final scheme should include details of a Sustainable Urban Drainage System and demonstrate a sustainable approach to the management of surface water on site. This matter can be satisfactorily addressed by way of condition and officers raise no objection in this regard.

7.11 S.106 requirements/planning obligations

7.11.1 It will be necessary for the development to be parking permit free, by way of legal agreement.

7.11.2 The proposed development would be subject to the Community Infrastructure Levy (CIL). This would require a contribution of £220 per additional square metre of floor space to be paid to Merton Council and an additional £60 per additional square meter to be paid to the Mayor. Further information on this can be found at:

<http://www.merton.gov.uk/environment/planning/cil.htm>

7.12 Response to issues raised in objection letters

7.12.1 The majority of uses raised by objectors are addressed in the body of this report and a number of issues relate to the original application scheme, rather than the amended scheme. However, in addition, the following comments are provided:

- Issues relating to disturbance throughout the construction process cannot reasonably amount to a reason for refusal but safeguarding conditions are recommended to minimise any adverse impact.
- In terms of landscaping, this can be controlled by way of condition.
- Amended plans have been received to rectify the inaccuracy in terms of the roof pitch of the existing building.
- Any cladding of the top floor would be required to meet relevant Building regulation requirements (along with means of evacuation) and is not a matter that can be considered under this minor planning application (only major planning applications are required to provide a Fire Safety Statement).
- Issues of whether leaseholders have agreed to additional floors above is a private, civil matter and does not affect the planning assessment of the proposal. Planning permission does not convey an ultimate right to develop and

if there are other legal obstacles the granting of planning permission may not necessarily overrule these legal obstacles.

- Issues relating to re-mortgaging, building insurance and service charges are not matters that can be considered under the planning assessment.
- Some degree of disturbance caused by the construction process is inevitable. However, this cannot reasonably amount to a reason for refusal provided reasonable efforts are made to minimise and mitigate for the impact. Therefore, conditions for method of construction statements are sought which would detail how the impacts of the construction process are to be minimised. Any compensation sought by existing occupiers would be a private civil matter – in planning terms, provided the impact is minimised as far as possible there would be no reasonable grounds for objection.
- The impact on property values is not a material planning consideration (however, members are advised that the impact on visual and residential amenity are material considerations that can be taken into account).
- Issues of soundproofing would be addressed through the Building Regulations as opposed to at the planning stage.
- Concerns relating to displacement parking in neighbouring streets has been carefully considered but officers conclude that it would not be reasonable to withhold planning permission on this basis, as the application would be subject to a restriction on the issuing of parking permits by way of s.106 which would meet the relevant policy requirements. In addition, there are legislative pathways that would allow for consideration of parts of the borough to be included in a CPZ in the future were the demand established.

8. Conclusion

- 8.1 The proposal would provide three additional units, all with some degree of external amenity space, which would contribute to meeting the borough's overall housing need.
- 8.2 The form and appearance of the proposed addition is considered to complement the existing building and would not appear visually discordant in the streetscene despite the increased height.
- 8.3 The proposal, as a result of the increased height over the existing, would result in some limited impact on properties to the front and rear of the site. However, as explained in this report, the impact is considered to be minimal and would not warrant a reason for refusal in this urban context.
- 8.4 Officers consider that the proposal is acceptable in planning terms, subject to conditions and a legal agreement and therefore the recommendation is for approval.

9. **RECOMMENDATION**

Grant planning permission subject to s106 agreement securing the following:

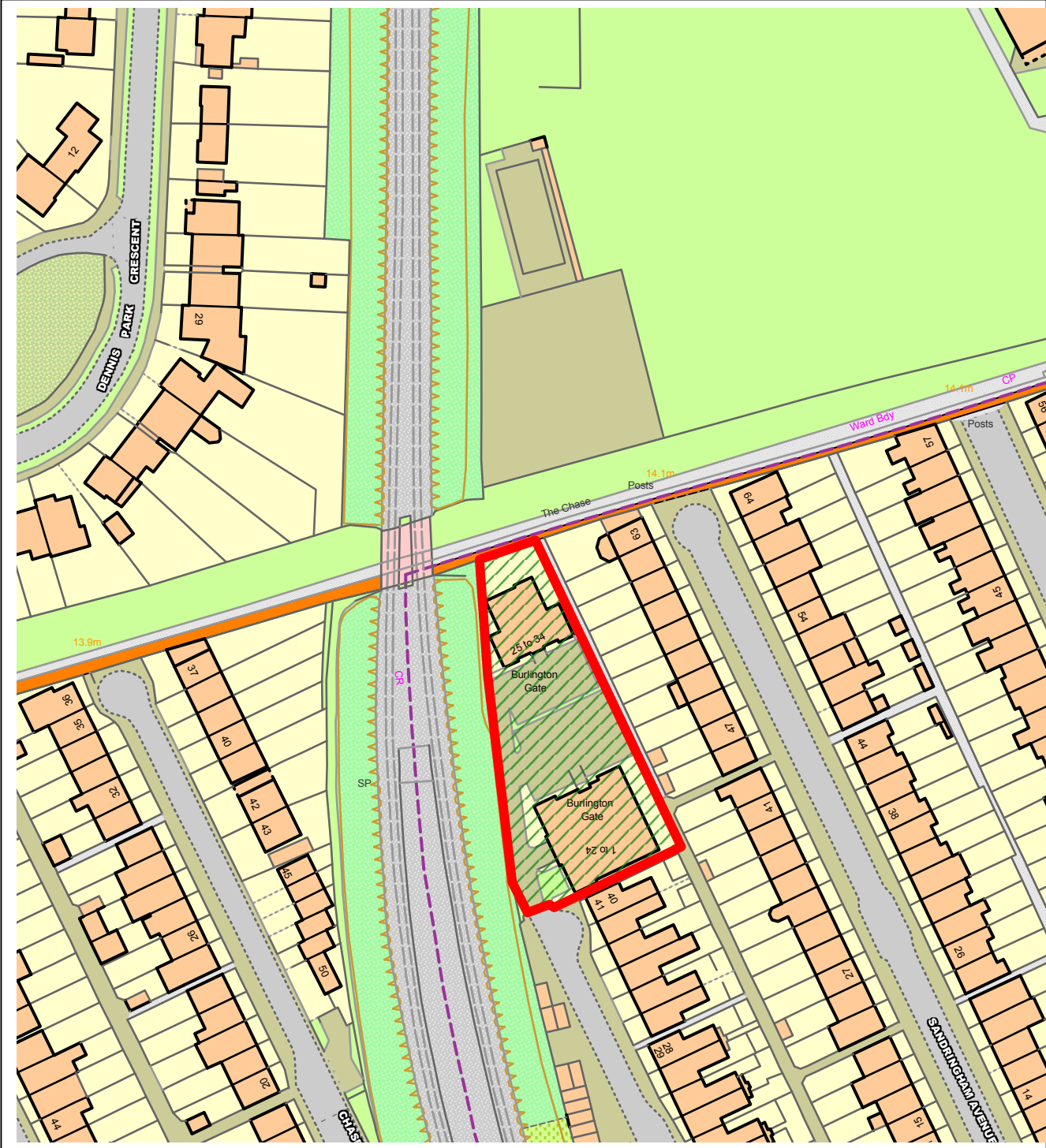
- Restrict parking permits for all new units.
- and cost to Council of all work in drafting S106 and monitoring the obligations.

And the following conditions:

1. Time limit
2. Approved Plans

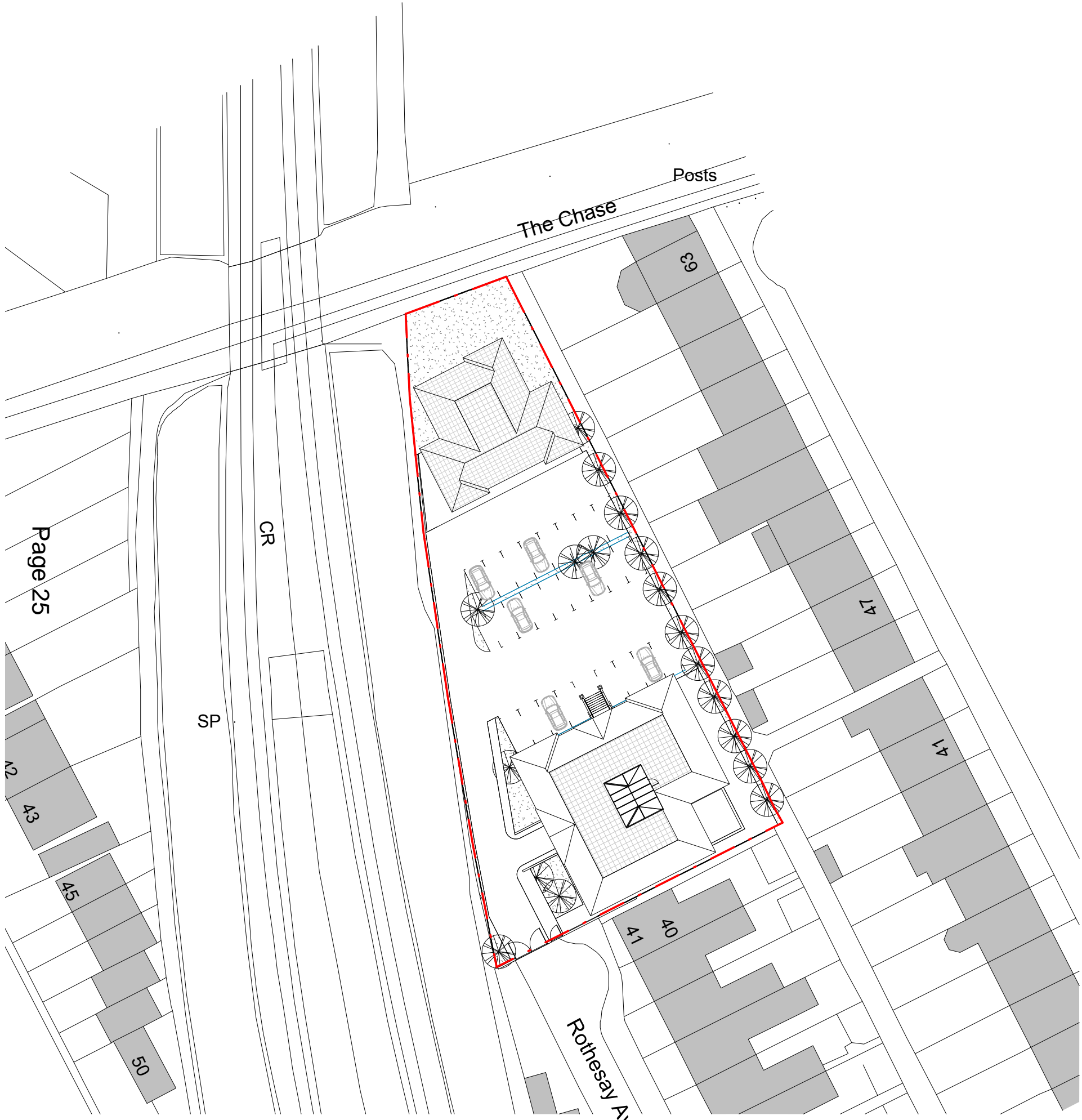
3. B1 External Materials to be Approved
4. B4 Details of surface treatment
5. C07 Refuse & Recycling (Implementation)
6. C08 No Use of Flat Roof
7. D09 No External Lighting
8. H06 Cycle Parking (Implementation)
9. H10 Construction Vehicles, Washdown Facilities etc
10. H12 Delivery and Servicing Plan
11. H13 Demolition/Construction Logistics Plan, including a Construction Management Plan to be submitted to cover:
 - hours of operation
 - the parking of vehicles of site operatives and visitors
 - loading and unloading of plant and materials
 - storage of plant and materials used in constructing the development
 - the erection and maintenance of security hoarding including decorative displays and facilities for public viewing, where appropriate
 - wheel washing facilities
 - measures to control the emission of noise and vibration during construction/demolition.
 - demonstration to show compliance with BS5228
 - measures to control the emission of dust and dirt during construction/demolition
 - a scheme for recycling/disposing of waste resulting from demolition and construction works
12. L2 Sustainability - Pre-Commencement (New build residential)
13. A Non Standard Condition: Noise levels, (expressed as the equivalent continuous sound level) LAeq (10 minutes), from any fixed external new plant/machinery shall not exceed LA90-10dB at the boundary with any residential property or noise sensitive premises.
14. A Non Standard Condition: All Non-road Mobile Machinery (NRMM) used during the course of the development that is within the scope of the Greater London Authority 'Control of Dust and Emissions during Construction and Demolition' Supplementary Planning Guidance (SPG) dated July 2014, or any subsequent amendment or guidance, shall comply with the emission requirements therein.
15. A Non Standard Condition: No development approved by this permission shall be commenced until a detailed scheme for the provision of surface and foul water drainage has been implemented in accordance with details that have been submitted to and approved in writing by the local planning authority. The drainage scheme will dispose of surface water by means of a sustainable drainage system (SuDS) via infiltration or at an agreed runoff rate, in accordance with drainage hierarchy contained within the London Plan and the advice contained within the National SuDS Standards.

NORTHGATE SE GIS Print Template

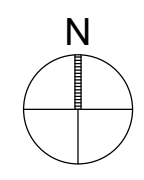
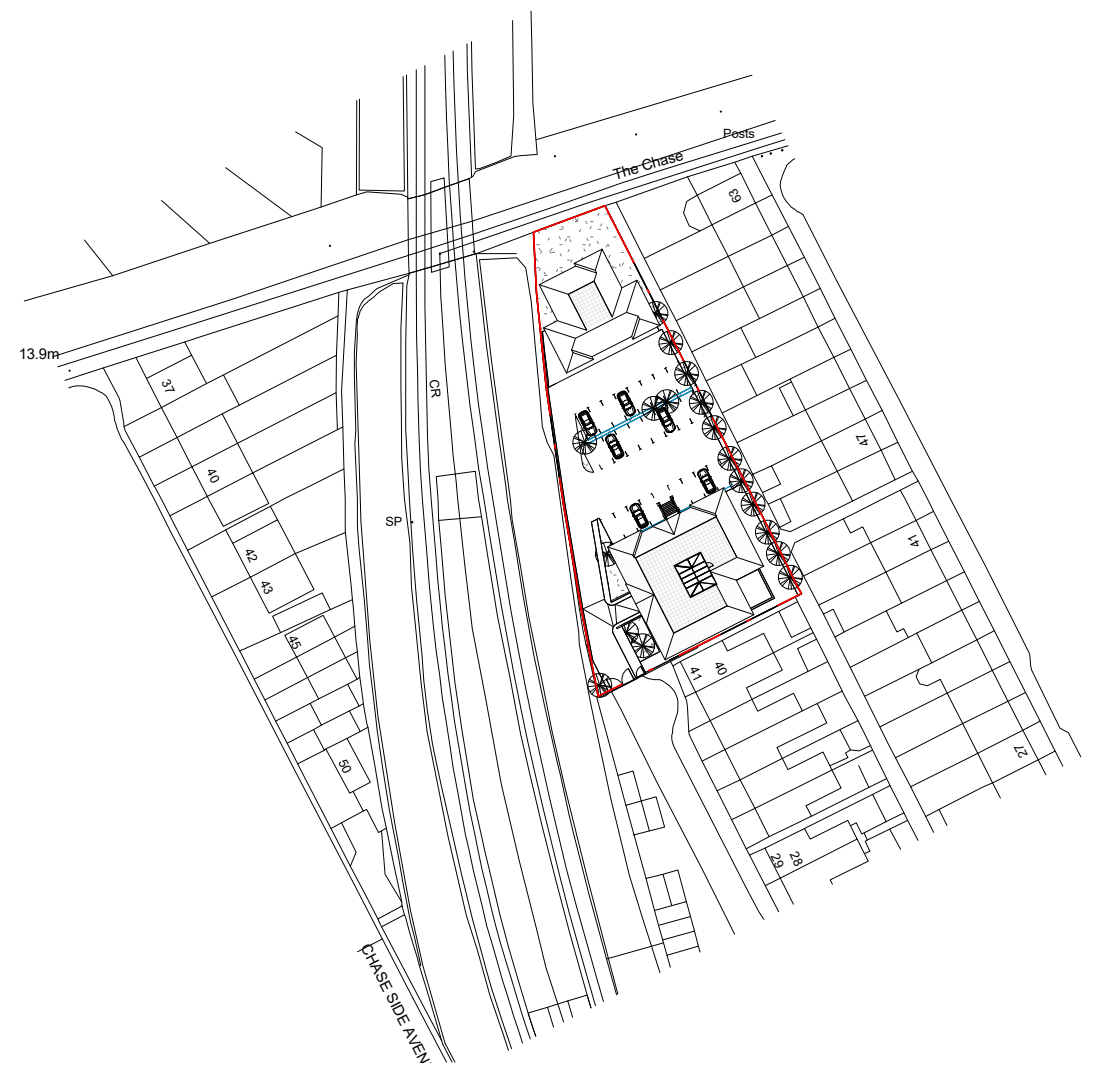


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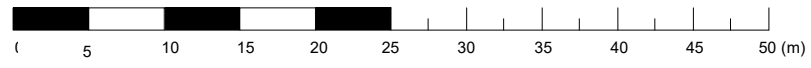
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--- SITE BOUNDARY



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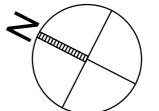
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DRAWING
Existing Site Plan

bubble architects STUDIO 205 THE BUSWORKS
39-41 NORTH ROAD
LONDON N7 9DP
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--- SITE BOUNDARY

The Chase

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Rothesay Ave

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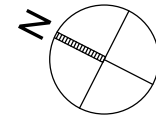
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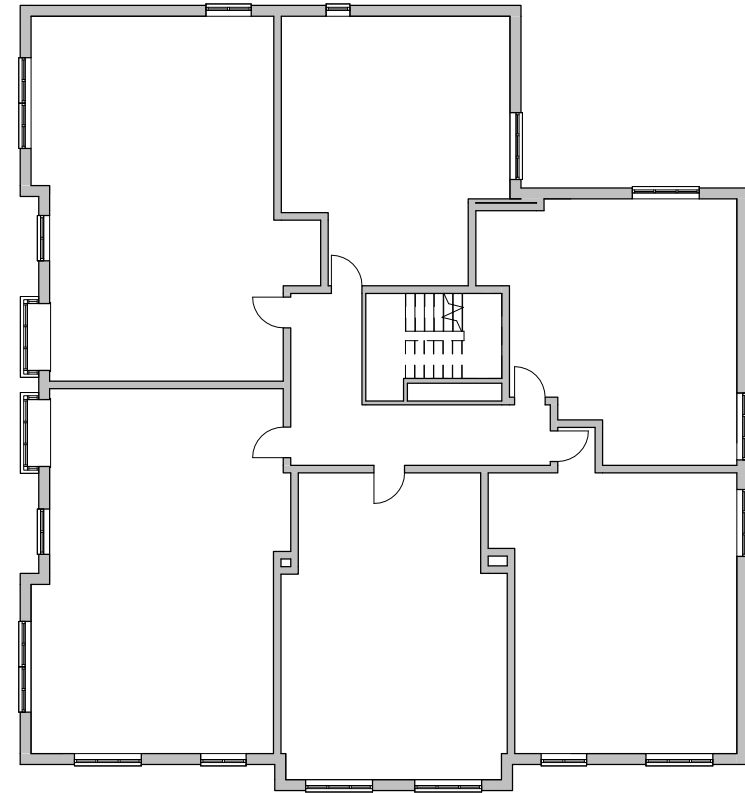
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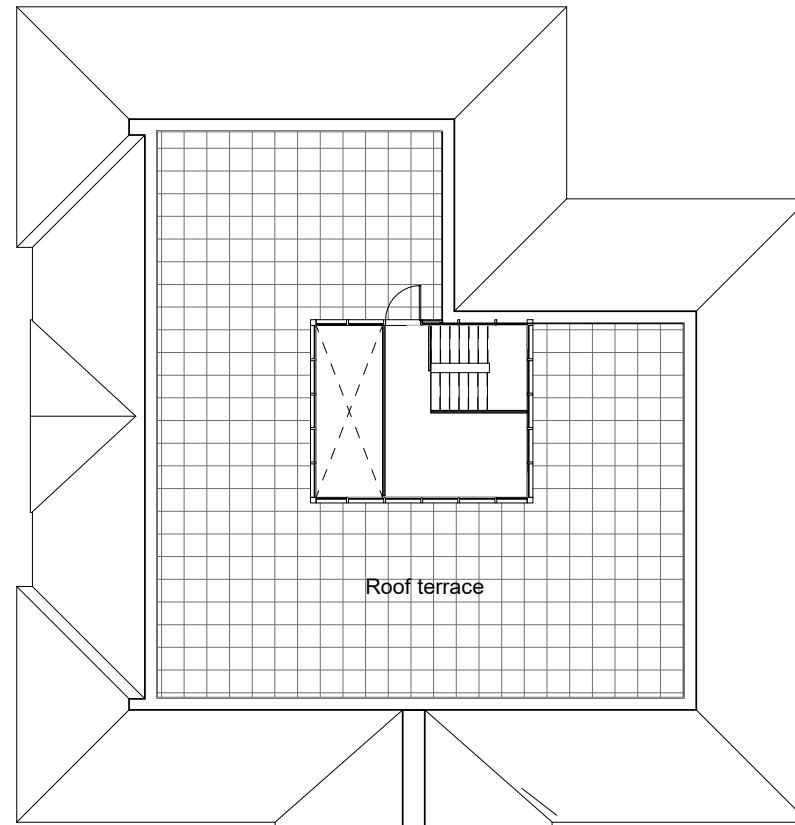
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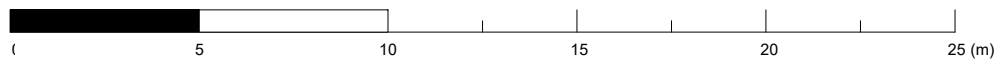
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FIRST, SECOND AND THIRD FLOOR PLAN
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ROOF PLAN
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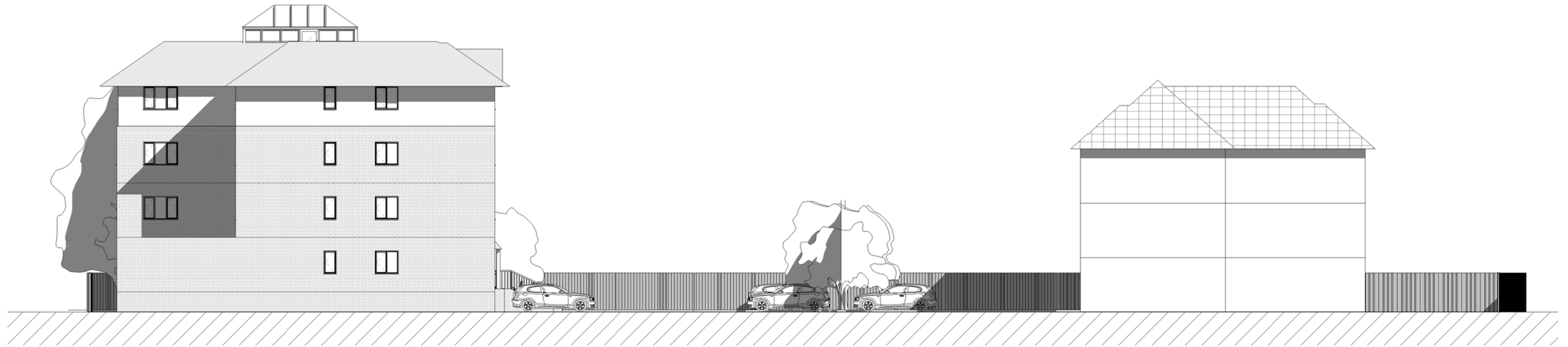
PROJECT
Burlington Gate, 42 Rothesay Ave, London SW20 8JU

DRAWING
Existing 1st, 2nd, 3rd and Roof Plan

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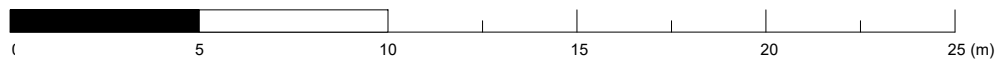


EAST ELEVATION
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NORTH ELEVATION
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A	DRAWINGS AMENDED TO SURVEY	AG	01.12.21
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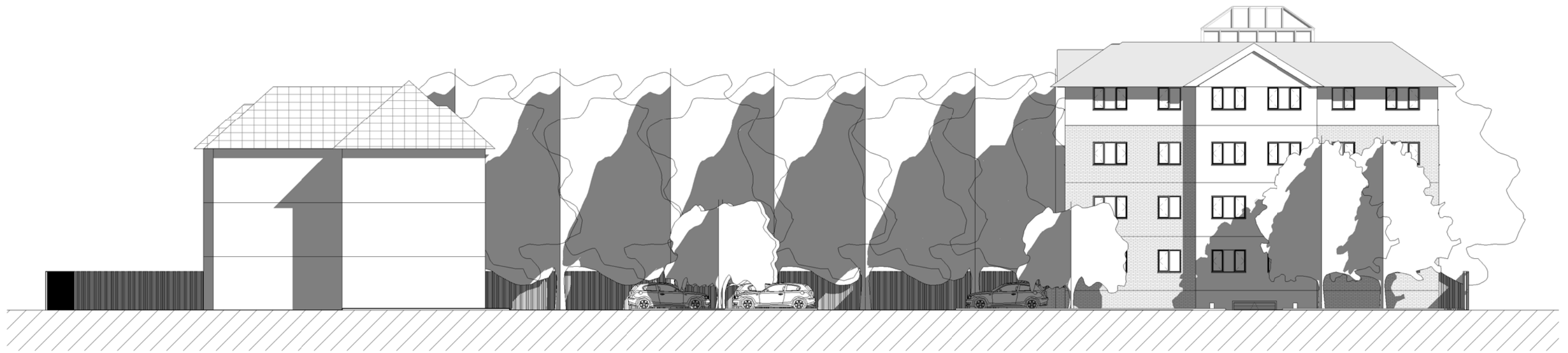
PROJECT
Burlington Gate, 42 Rothesay Ave, London SW20 8JU

DRAWING
Existing East and North Elevation

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39-41 NORTH ROAD
LONDON N7 9DP
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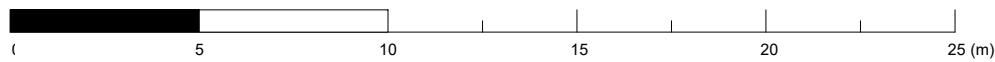


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WEST ELEVATION
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SOUTH ELEVATION
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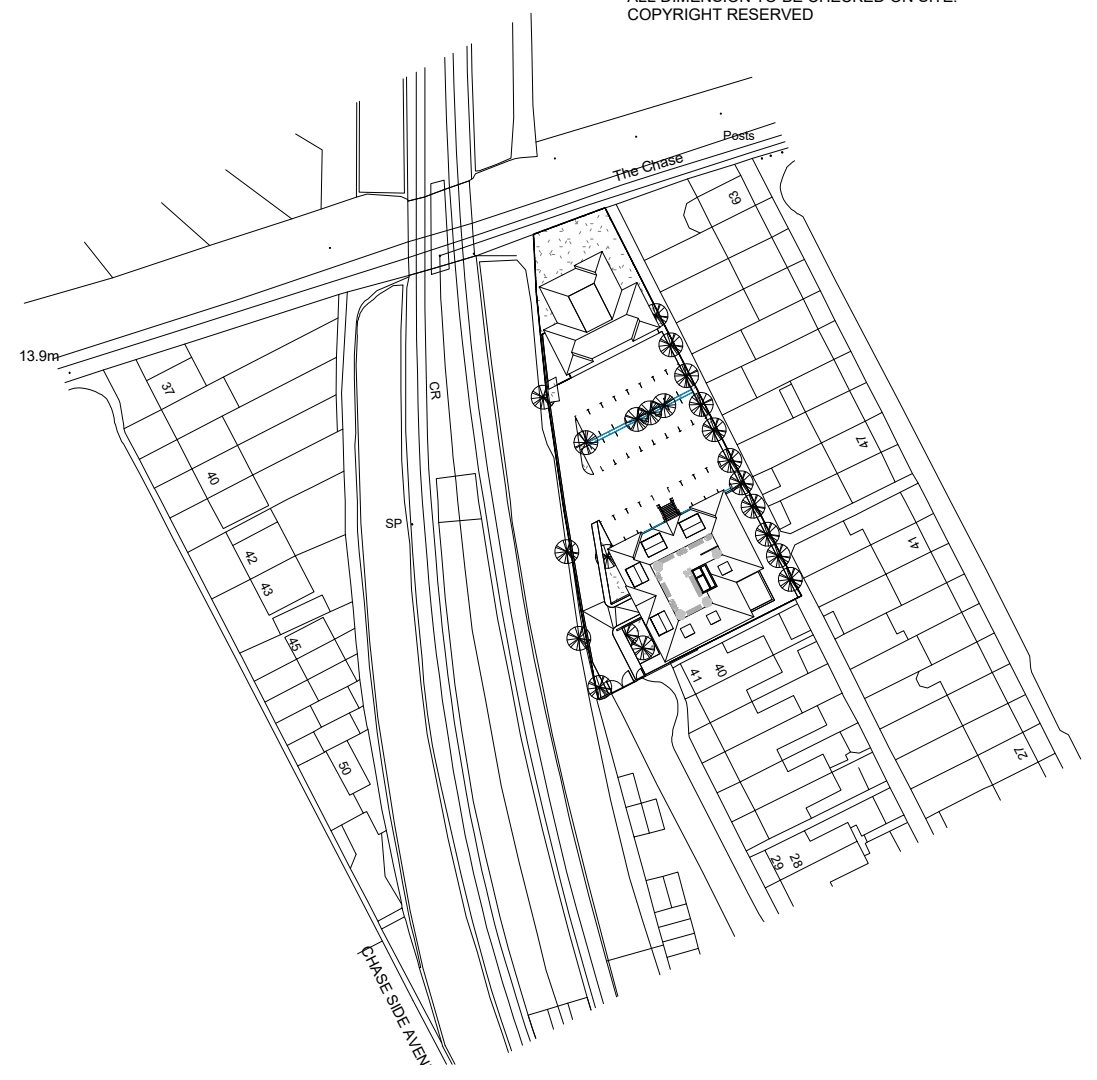
PROJECT
Burlington Gate, 42 Rothesay Ave, London SW20 8JU

DRAWING
Existing South and West Elevation

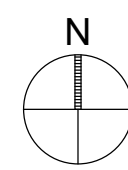
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Page 30



--- SITE BOUNDARY

REV/ISSUE	NOTE	DRN	DATE
E	Glazed Access to roof amenity	AG	17.12.21
D	Client's comments addressed	SR	13.12.21
C	Drawings amended to survey	AG	01.12.21
B	Full Planning Application	AG	29.07.21
A	Cycle shed and landscaping added	MH	23.12.20

SCALE	DATE	DRN	CHK
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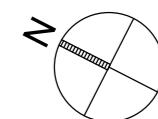
PROJECT
Burlington Gate, 42 Rothesay Ave, London SW20 8JU

DRAWING
Proposed Site Plan

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- - - - - SITE BOUNDARY

The Chase

63

47

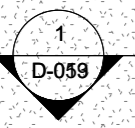
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40

41

Rothesay Ave

CR



B	Glazed Access to roof amenity	AG	17.12.21
A	Full Planning Application	AG	29.07.21
REV	NOTE	DRN	DATE

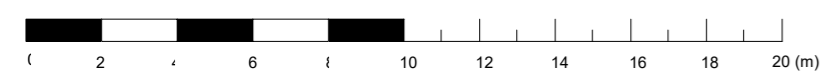
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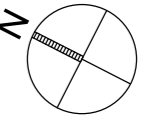
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Burlington Gate, 42 Rothesay Ave,
London SW20 8JU

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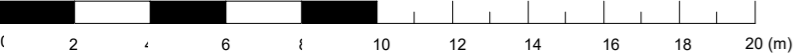
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D	Glazed Access to roof amenity	AG	17.12.21
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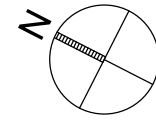
PROJECT
 Burlington Gate, 42 Rothesay Ave,
 London SW20 8JU

DRAWING
 Proposed Amenity Space
 roof

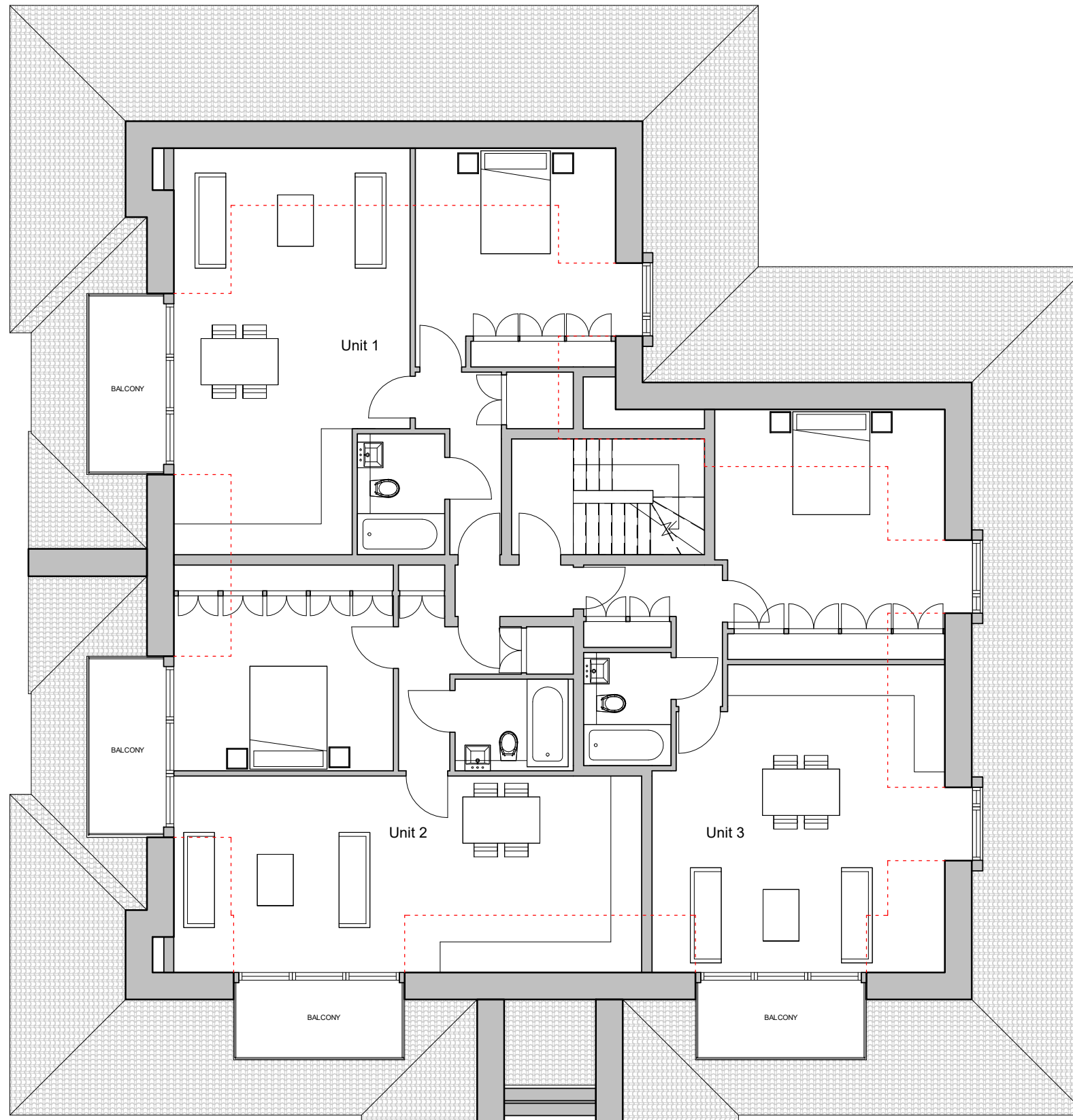
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--- SITE BOUNDARY



PROPOSED 4th FLOOR PLAN
1:100

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D-059



REV/ISSUE	NOTE	DRN	DATE
D	Glazed Access to roof amenity	AG	17.12.21
C	Client's comments addressed	SR	13.12.21
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A	Full Planning Application	AG	29.07.21

SCALE	DATE	DRN	CHK
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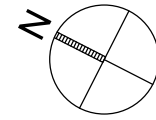
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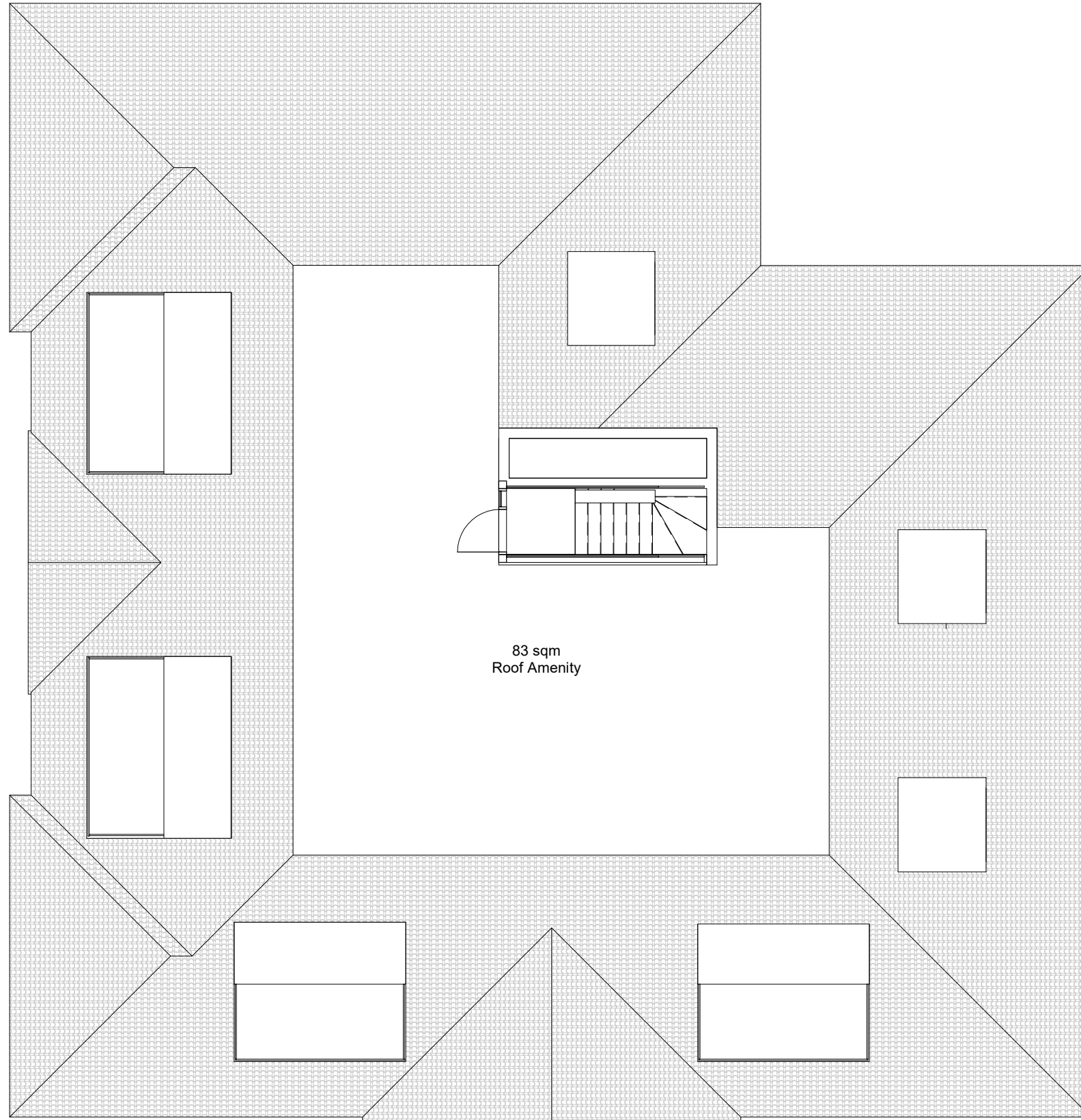
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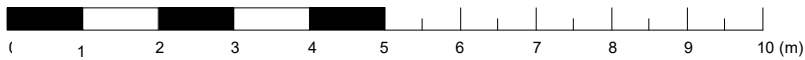
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83 sqm
Roof Amenity

Page 34

PROPOSED ROOF PLAN
1:100



REV/ISSUE	NOTE	DRN	DATE
D	Glazed Access to roof amenity	AG	17.12.21
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SCALE	DATE	DRN	CHK
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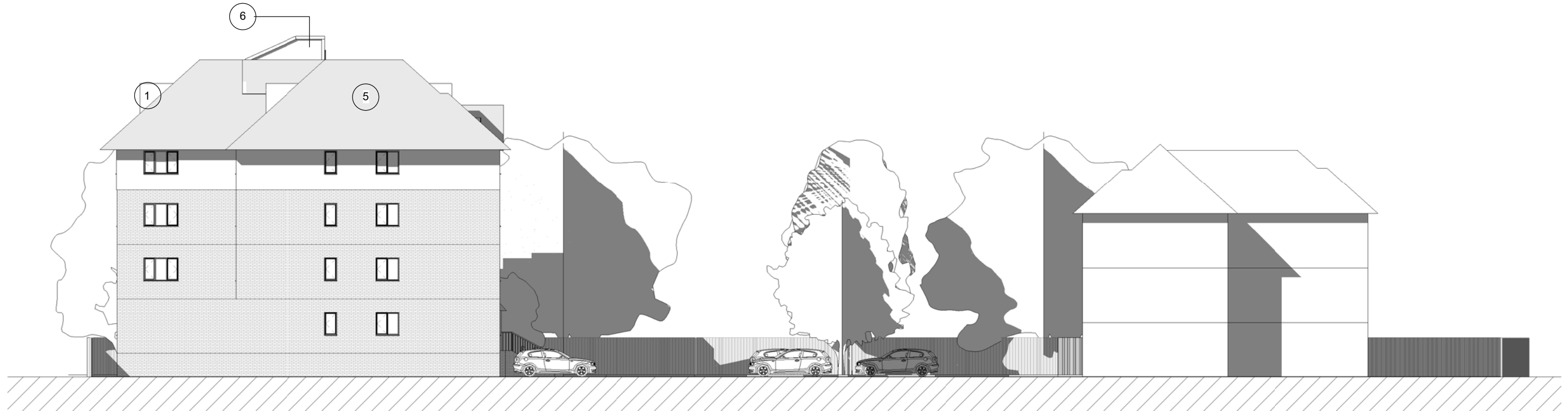
PROJECT
Burlington Gate, 42 Rothesay Ave, London SW20 8JU

DRAWING
Proposed Roof Plan

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EAST ELEVATION
1:200

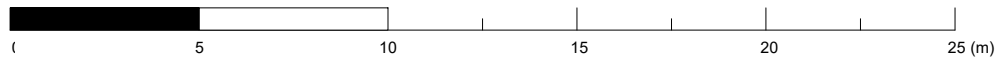
Page 35



NORTH ELEVATION
1:200

Key:

- 1 Zinc Cladding
- 2 PPC Aluminium Double-glazed window
- 3 Glass Panel Balustrade
- 4 White Render
- 5 Clay tiles on the new roof to match the existing
- 6 Glazed access to roof amenity
- 7 PPC Aluminium Double-glazed door



REV/ISSUE	NOTE	DRN	DATE
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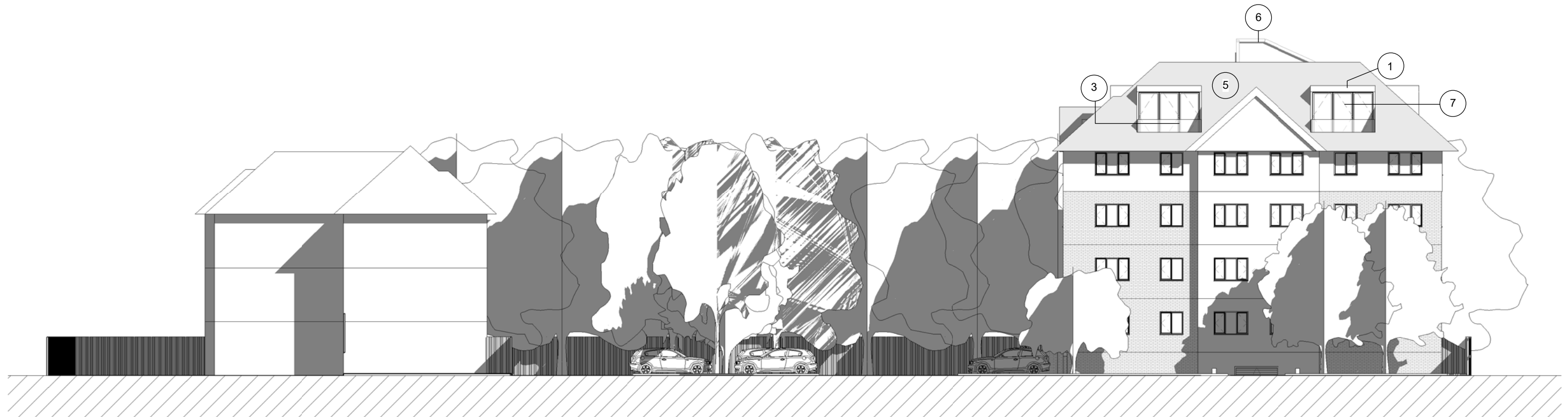
PROJECT
Burlington Gate, 42 Rothesay Ave, London SW20 8JU

DRAWING
Proposed East and North Elevation

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WEST ELEVATION
1:200

Page 36



SOUTH ELEVATION
1:200

Key:

- 1 Zinc Cladding
- 2 PPC Aluminium Double-glazed window
- 3 Glass Panel Balustrade
- 4 White Render
- 5 Clay Tiles to match the existing
- 6 Glazed access to roof amenity
- 7 PPC Aluminium Double-glazed door

REV/ISSUE	NOTE	DRN	DATE
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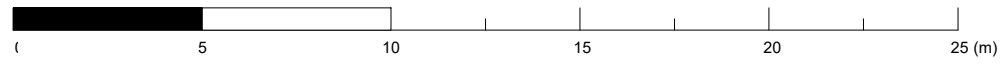
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PROJECT
Burlington Gate, 42 Rothesay Ave, London SW20 8JU

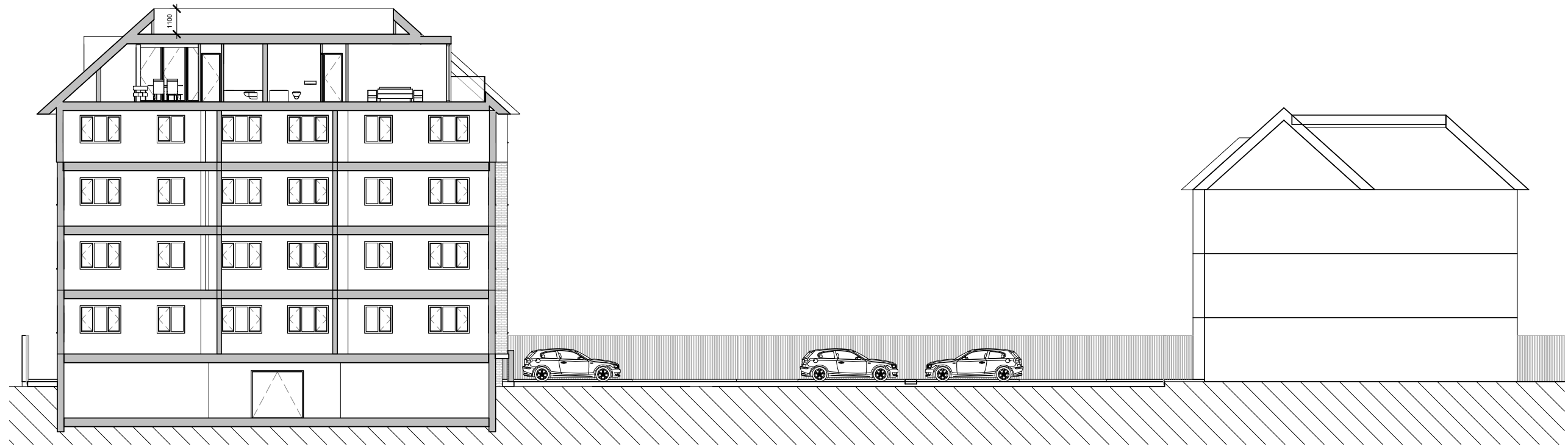
DRAWING
Proposed South and West Elevation

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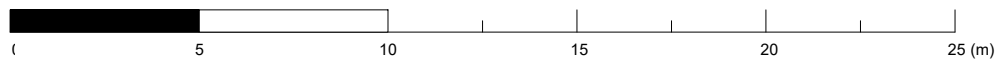
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PROJECT
Burlington Gate, 42 Rothesay Ave, London SW20 8JU

DRAWING
Proposed Section AA

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PLANNING APPLICATIONS COMMITTEE

18th August 2022

Item No:

UPRN

APPLICATION NO: 21/P3286

DATE VALID: 07/09/2021

Address/Site: 9 Hamilton Road, South Wimbledon, SW19 1JD

(Ward): Abbey

Proposal: ALTERATIONS AND ERECTION OF A SINGLE STOREY REAR EXTENSION TO FACILITATE CONVERSION OF SINGLE DWELLINGHOUSE INTO 3 x SELF-CONTAINED FLATS

Drawing Nos: 1394 300; 1394 301; 1394 302; 1394 303; 1394 304; 1394 305; 1394 306; 1394 307; mk 1b; DAS 9 Hamilton road rev a

Contact Officer: Jivan Manku (020 8545 4859)

RECOMMENDATION

Grant Permission subject to conditions and S106 Agreement.

CHECKLIST INFORMATION

- Heads of Agreement: Yes – Permit Free
- Is a screening opinion required: No
- Is an Environmental Statement required: No
- Has an Environmental Statement been submitted: No
- Press notice: No
- Site notice: Yes
- Design Review Panel consulted: No
- Number of neighbours consulted: 8
- External consultations: No
- Conservation area: No
- Listed building: No
- Tree protection orders: No
- Controlled Parking Zone: Yes

1. INTRODUCTION

1.1 This application is being brought to the Planning Applications Committee for determination due to the number and nature of representations received.

2. SITE AND SURROUNDINGS

2.1 The application relates to a two storey, detached dwelling located on the east side of Hamilton Road, South Wimbledon. The brick and tile dwelling is set back from the road by an area of hardstanding. The site does not lie within a Conservation Area. The site surroundings comprise other residential plots.

3. PROPOSAL

3.1 The application seeks planning permission for the erection of a single storey rear infill extension and conversion of the single dwelling to 3 x self-contained flats.

3.2 The single storey rear extension would have a depth of 1.5 metres and a width of 1.2 metres infilling the space alongside the recently constructed rear extension. The works would also comprise of converting the dwelling to a three bed, four person, self-contained flat on the ground floor, a one-bed, two person self-contained flat on the first floor and a studio flat on the loft floor. The recently approved and built outbuilding would be ancillary to the proposed ground floor flat.

4. PLANNING HISTORY

- 20/P2158 - APPLICATION FOR A LAWFUL DEVELOPMENT CERTIFICATE IN RESPECT OF THE PROPOSED ERECTION OF A GROUND FLOOR REAR AND SIDE INFILL EXTENSION, FIRST FLOOR WINDOW ALTERATIONS, LOFT CONVERSION INCLUDING REAR DORMERS AND VELUX AND RAISING PARAPET WALLS - Issue Certificate of Lawfulness - 17/09/2020
- 20/P2948 - ERECTION OF A GROUND FLOOR REAR AND SIDE EXTENSION AND FIRST FLOOR SIDE EXTENSION - Refuse Permission - 18/01/2021
- 21/P0516 - ERECTION OF A SINGLE STOREY REAR AND SIDE EXTENSION - Refuse Permission - 21/05/2021
- 21/P0523 - ERECTION OF A SINGLE STOREY REAR AND SIDE EXTENSION, FIRST FLOOR SIDE EXTENSION - Grant Permission subject to Conditions - 20/05/2021
- 21/P2267 - APPLICATION FOR A LAWFUL DEVELOPMENT CERTIFICATE IN RESPECT OF THE PROPOSED ERECTION OF A SINGLE STOREY OUT BUILDING TO BE USED AS A GAMES ROOM, INCIDENTAL TO THE MAIN DWELLING HOUSE - Issue Certificate of Lawfulness - 25/08/2021

5. POLICY CONSIDERATIONS

5.1 National Planning Policy Framework 2021

- Chapter 2 Achieving sustainable development
- Chapter 5 Delivering a sufficient supply of homes
- Chapter 6 Building a strong, competitive economy
- Chapter 9 Promoting sustainable transport
- Chapter 11 Making effective use of land
- Chapter 12 Achieving well-designed places
- Chapter 14 Meeting the challenge of climate change, flooding and coastal change
- Chapter 15 Conserving and enhancing the natural environment

5.2 London Plan 2021

- D4 Delivering good design
- D5 Inclusive design
- D6 Housing quality and standards
- D14 Noise
- H1 Increasing housing supply
- H10 Housing size mix
- G4 Open space
- T3 Transport capacity, connectivity and safeguarding
- T4 Assessing and mitigating transport impacts
- T5 Cycling
- T6 Car parking
- T6.1 Residential parking
- T7 Deliveries, servicing and construction

5.3 Merton Core Strategy (July 2011)

- CS6 - Wimbledon Sub Area
- CS8 - Housing Choice
- CS9 - Housing Provision
- CS11 - Infrastructure
- CS14 - Design
- CS15 - Climate Change
- CS18 - Active Transport
- CS19 - Public Transport
- CS20 - Parking, Servicing and Delivery

5.4 Merton Sites and Policies Plan (July 2014)

- DM H2 - Housing mix
- DM D1 - Urban Design and public realm
- DM D2 - Design considerations in all developments
- DM D3 – Alterations and extensions to existing buildings
- DM T1 - Support for sustainable transport and active travel
- DM T2 - Transport impacts of development
- DM T3 - Car parking and servicing standards

6. **CONSULTATIONS**

6.1 Standard 21-day site notice procedure and 8 neighbouring properties were notified of the proposed development as part of the consultation process on 18/10/2021. 8 objections were received from neighbouring properties and 1 objection was received from Councillor Eleanor Stringer. The objections are as follows:

7a Hamilton Road:

- The development would visually impact the neighbouring properties;
- The number of windows and their sizes will cause noise and light pollution for neighbours;
- Will cause negative environmental impacts and the waste facilities could potentially attract rodents;
- No fire suppression has been proposed;
- Development would result in an increase to parking within the area
- Future development issues in relation to the outbuilding

31 Hamilton Road:

- The look of the property with its recently constructed extensions and alterations is totally out of keeping with the remainder of properties adjacent and those in the remainder of the road;
- There is an issue of height, light and general overlooking of No. 9. The windows are large and not in keeping with the existing properties;
- Development would result in an increase to parking within the area

21 Hamilton Road:

- The family home has already been considerably extended, in the view of neighbours very unsympathetically, and there is an outstanding issue as to whether the extension is in fact within permitted development guidelines as it is believed to be more than 4m beyond the original demised premise of the house. This concern has been raised with the relevant planning officer but I understand has not yet been clarified. Nevertheless, the developers have proceeded with the extension at both ground and first floor level. As a result there is a considerable overlooking and daylight issue for neighbours on both sides;
- The developer has argued the impact is less as the dwelling is detached. However, there is only a 10cm gap between the properties;
- There is no fireproofing between the flats;
- The refuse arrangements are unclear;
- Future development issues in relation to the outbuilding.

11 Hamilton Road:

- The existing property with its extensions is overbearing and intrusive for the residents of the houses on either side of it but also for residents which can see the development;
- The windows look directly into the rear garden of No. 11 allowing no privacy at any time of the day;
- The rear extension is supposed to be 2.95 metres high but it actually measures to 3 metres and has additional height from the skylight;
- Will cause negative environmental impacts and the waste facilities could potentially attract rodents;
- There is no fireproofing between the flats.

5 Hamilton Road:

- Hazards for prospective tenants as the design of the studio flat is poor;

- Privacy issues for studio flat and neighbouring properties due to the floor to ceiling windows which have been installed;
- The proposed refuse facilities appear to be insufficient to accommodate three separate flats. The bin store and cycle store and their location at the front of the property would be out of keeping with the rest of the road;
- It is difficult to understand the internal living spaces for each of the flats due to the poor plans which have been submitted;
- The proposed development as built is out of keeping with the existing dwelling and wider area in terms of its character and appearance;
- Future development issues in relation to the outbuilding;
- The existing property is visually intrusive, overbearing and causes a strong sense enclosure to both Nos. 7 and 11 Hamilton Road, with the games room having the same impact on No. 11;
- It appears a gate has now been erected between the main property and the outbuilding. This lends itself to the concern I mentioned previously, that the purported 'games room' would in fact be used as another self-contained dwelling.

13 Hamilton Road:

- The existing extensions are blocking light into my garden and my views out of it. I also believe it is higher than it was permitted to be;
- The extension overlooks my property – particularly my garden so my privacy is compromised;
- There would be issues with parking and noise.

1 Hamilton Road:

- The development will lead to an overconcentration at the property with consequent impact on noise and privacy for neighbouring properties;
- The outbuilding was given permission based on its ancillary use to the main dwelling. With a proposed 3-bed ground floor flat which would have limited space, the outbuilding could possibly be used as an extension to the living space which would increase noise levels;
- The proposed bin store and cycle store are incongruous to the street scene;
- A commercial eurobin rather than a standard wheelie bin will be provided for the occupants of the flats but it would not be practical within the residential area;
- There is already a stress on parking along the road. This development would add to the existing parking problems.

8 Hamilton Road:

- There is already a stress on parking along the road. This development would add to the existing parking problems.

Councillor Eleanor Stringer:

- Residents of the area are concerned about the direct impact on their light, and concerns about this setting a precedent for such dramatic changes in other houses in this historic area.

Council Highway Officer:

- No objection, subject to conditions and informatives: H9, INF9 and INF12

Council Waste Officer:

- Shared bin arrangement proposed is acceptable. Kerbside waste collection arrangement acceptable.

7. PLANNING CONSIDERATIONS

7.1 The planning considerations for the proposed development would include the following:

- Principle of Development
- Design, Character and Appearance
- Neighbour Amenity
- Standard of Accommodation
- Outdoor Amenity
- Housing Mix
- Highway, Traffic and Parking
- Refuse
- Sustainable Development

7.2 Principle of Development

7.2.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 stated that when determining a planning application, the development plan must be regarded, and the determination shall be made in accordance with the development plan, unless material considerations indicate otherwise.

7.2.2 The National Planning Policy Framework, London Plan Policy H1 and the Council's Core Strategy Policy CS8 and CS9 all seek to increase sustainable housing provision and access to a mixture of dwelling types for the local community, providing that an acceptable standard of accommodation would be provided. Policy H2 of the London Plan 2021 states that incremental intensification of existing residential areas within PTALs 3-6 or within 800m distance of a station or town centre boundary is expected to play an important role in contributing towards the housing targets for small sites. Policy CS 14 of the Core Strategy also states that schemes involving dwelling conversions that result in the loss of an existing family sized unit must incorporate the re-provision of at least one family sized unit - a family sized unit is one which has at least 3 bedrooms.

7.2.3 The proposal would consist of a small rear infill extension and conversion of the single dwelling to a three bed flat, a one bed flat and a studio flat. In principle, the conversion of the dwelling to increase the number of dwellings can be considered acceptable, as long as a family sized unit (3+ bed) is maintained on site in accordance with Policy CS14. As such, the principle of development would be considered acceptable and compliant with the policies above. Whilst the principle of the conversion is considered acceptable, the scheme is also subject to the following criteria being equally fulfilled and compliant with the policies referred to above

7.3 Design, Character and Appearance

7.3.1 The National Planning Policy Framework sets out that achieving high quality places and buildings is fundamental to the planning and development process. It also leads to improvements in the quality of existing environments. It states that planning should always seek to secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings.

- 7.3.2 The regional planning policy advice in relation to design is found in the London Plan (2016), in Policy 7.4 - Local Character and 7.6 - Architecture. These policies state that Local Authorities should seek to ensure that developments promote high quality inclusive design, enhance the public realm, and seek to ensure that development promotes world class architecture and design.
- 7.3.3 Planning policy DM D2 (Design considerations in all development) of Merton's Site and Polices Plan 2014 seeks to achieve high quality design and protection of amenity within the Borough. Proposals are required to relate positively and appropriately to the siting, rhythm, scale, density, proportions, height, materials and massing of the surrounding buildings and existing street patterns, historic context, urban layout and landscape features of the surrounding area.
- 7.3.4 The site currently benefits from a roof extension, first floor side extension, single storey side/rear extension and single storey outbuilding to rear. The works are currently under construction and the application also seeks planning permission for the erection of an infill single storey extension to the rear with a depth of 1.5 metres and width of 1.2 metres. The works under construction are being implemented under a previous planning permission and Certificate of Lawfulness applications. The infill extension would be minimal compared to the existing extensions which are currently under construction and is therefore considered to be acceptable within its setting. Officers note the objections raised with the design and scale of the works that have been carried out, however, the works carried out have been undertaken under a previous planning permissions and Certificate of Lawfulness applications. Overall, the design, character and appearance of the development is considered to be acceptable.

8 Neighbouring Amenity

- 8.1.1 SPP policy DM D2 states that proposals must be designed to ensure that they would not have an undue negative impact upon the amenity of neighbouring properties in terms of loss of light, quality of living conditions, privacy, visual intrusion and noise.
- 8.1.2 A number of objections received have referred to the extensions under construction. The existing extensions under construction are all subject to a previous planning permission and Certificate of Lawfulness approvals. The proposal would effectively combine these extensions, with the additional infill extension to the north-east corner. The proposed infill extension would not cause additional harm to the neighbouring properties due to its size and proximity in comparison to the existing extensions which are under construction. The additional depth on the north-east corner of 1.5 m would match the height of the existing extensions and would maintain the same separation distance from the north side boundary. Other main reasons for objections are as follows:
- Increase in parking demand
 - Potential future development in relation to converting the outbuilding to yet another self contained flat
 - Poor design for flats
 - Bin and cycle store location
 - Privacy issues for studio flat

- 8.1.3 The available space for each of the flats would be compliant with the minimum space standards as detailed in the London Plan. There would be no possibility for additional parking as the applicant will be entering into a legal agreement to restrict additional parking being permitted along Hamilton Road.
- 8.1.3 Although concerns have been raised relating to the design of the flats, there would be ample levels of natural light entering each of the flats through windows and rooflights. As such, the living conditions for future occupants of each flat would have adequate living space and therefore considered acceptable.
- 8.1.4 Due to the recent developments which have been approved via various individual planning application and certificate of lawfulness applications at the site it is understandable the adjoining properties may be apprehensive about any future development regarding the outbuilding. However, the proposed use of the outbuilding would be ancillary to the ground floor flat only and there have been no applications regarding any proposed change of use to an additional unit. If this were to be the case in the future, the same planning process would be followed and any proposed development would be assessed on its own merits. As such, the outbuilding's proposed use as being ancillary to the ground floor flat would be acceptable.
- 8.1.5 The loft extension which has been built does not appear to be consistent with the approved plans as per application reference 20/P2158 as the side facing windows (as built) are not obscurely glazed. For the loft extension to be compliant with permitted development guidelines, these side facing windows would need to be obscurely glazed. As such, a condition will be attached to the application to ensure the windows are rectified.

9 Standard of Accommodation

9.1.1 Policy D6 of the London Plan 2021 requires housing development to be of the highest quality internally and externally, and should satisfy the minimum internal space standards (specified as Gross Internal Areas -GIA) as set out in Table 3.1 of the London Plan. Table 3.1 provides comprehensive detail of minimum space standards for new development; which the proposal would be expected to comply with. Policy DMD2 of the Adopted Sites and Policies Plan (2014) also states that developments should provide suitable levels of sunlight and daylight and quality of living conditions for future occupants.

Flat No.	Type of dwelling	No. of persons	GIA (m³)	Minimum GIA (m³) required
1	3-bed	4	76.8	74
2	1-bed	2	51.2	50
3	Studio	1	40.3	39

9.1.2 The table above demonstrates each of the flats is compliant with the minimum space standard for new development. Furthermore, each of the flats has a sufficient level of sunlight and outlook through the windows and rooflights. As such, the proposal is considered to have an acceptable standard of accommodation.

10 Outdoor Amenity

10.1.1 Policy D6 of the London Plan 2021 states where there are no higher local standards in the borough Development Plan Documents, a minimum of 5 sq. m of private outdoor space should be provided for 1-2 person dwellings and an extra 1 sq. m should be provided for each additional occupant, and it must achieve a minimum depth and width of 1.5m.

10.1.2 The proposed ground floor flat would utilise the entire amenity space available at the application site which is approximately 45 sq. m. In addition to this, the recently built outbuilding would also be used solely by the occupants of the ground floor flat. The other two flats would not have any available amenity space at the application site. However, Haydon Road Recreation Ground is approximately 200m from the site therefore this could be used as an open area for the occupants of the flats as it is within walking distance. Achieving private outdoor amenity space for flats part of house conversions can be difficult to achieve due to the potential overlooking issues with adding balconies at first floor and above. On balance, whilst it is a shortfall with the lack of provision of outdoor space for the first and second floor flats, it is not considered to warrant refusal of the application.

11 Housing Mix

11.1.1 Policy H10 of the London Plan 2021 and Policy DM H2 of the Merton SPP 2014 favours development proposals where they contribute to meeting the needs of different households by providing a mix of dwelling sizes.

11.1.2 The proposal consists of a three bed, four person, self-contained flat on the ground floor, a one-bed, two person self-contained flat on the first floor and a studio flat on the loft floor. This mix of dwelling sizes complies with the policies above and provides a range of accommodation for small families, couples and people intending to live alone.

12 Highway, Traffic and Parking

12.1.1 London Plan Policies T3, T4, T5 and T6, Merton Core Planning Strategy (2011) Policies CS20 (Parking, Servicing and Delivery) of the Adopted Merton Core Planning Strategy (2011) DM T2 (Transport Impacts of Developments) and Merton SPP Policies DMT1, DMT2 and DM T3 (Car Parking and Servicing Standards) of the Adopted Merton Sites and Policies Plan (2014) require developers to demonstrate that their development would not adversely impact the surrounding highway network and provide appropriate levels of parking.

12.1.2 The proposal would include cycle storage for three cycles which would be located at the front of the property. This would be considered sufficient for the area and the proposed flats.

12.1.3 The site is located within Controlled Parking Zone S2 and currently benefits from an existing off road parking space to the front which is accessed via the vehicular crossover. No additional parking is proposed and considering the site is within a CPZ, it is agreed this is acceptable subject to the applicant entering into a legal agreement restricting the occupants of the flats obtaining parking permits, in line with policy.

13 Refuse

13.1.1 Policy SI6 of the London Plan and Policy CS 17 of the Merton Core Strategy require developers to provide waste facilities appropriate in relation to the size and type of the development.

13.1.2 Submitted plans indicate a 660L communal recycling bin, a 360L communal waste bin and a 23L food waste bin would be provided for the flats. These would be located within a bin store

at the front of the property. The Council's waste officer has reviewed the proposed arrangement and confirms this would be acceptable as long as the bins are presented at the kerbside.

14 Sustainable Development

14.1.1 New building works must comply with the Mayor's and Merton's objectives on carbon emission, renewable energy and sustainable design and construction, green roofs and sustainable drainage as detailed in the London Plan (2011) – Chapter 5 and the Council's LDF Core Planning Strategy (2011 – Policies CS15 and CS16). Whilst no details have been submitted up front with the application, any permission would require a condition that would ensure that the details on how this reduction was to be met were explained in greater detail. Therefore, with such a condition, the proposal would comply with Policy CS15 of Merton's Core Planning Strategy 2011. A condition to this effect is recommended (condition 10).

15 CONCLUSION

15.1 Subject to conditions and the legal agreement restricting the occupants of the flats from obtaining parking permits in the Controlled Parking Zone, the proposed development is acceptable in respect of the planning considerations above. The development would not be considered to cause harm to the amenity of the neighbouring properties or character and appearance of the host dwelling and wider area.

16 RECOMMENDATION

Grant planning permission subject to conditions and S106 Agreement:

1. A1 Commencement of development (full application): The development to which this permission relates shall be commenced not later than the expiration of 3 years from the date of this permission.

Reason: To comply with Section 91 (as amended) of the Town & Country Planning Act 1990.

2. A7 Approved Plans: The development hereby permitted shall be carried out in accordance with the following approved plans: 1394 300; 1394 301; 1394 302; 1394 303; 1394 304; 1394 305; 1394 306; 1394 307; mk 1b; DAS 9 Hamilton road rev a.

Reason: For the avoidance of doubt and in the interests of proper planning.

3. B3 External Materials as Specified: The facing materials to be used for the development hereby permitted shall be those specified in the application form unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure a satisfactory appearance of the development and to comply with the following Development Plan policies for Merton: policies D4 and D8 of the London Plan 2021, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D2 and D3 of Merton's Sites and Policies Plan 2014.

4. C04 Obscured Glazing: Before the development hereby permitted is first occupied, the loft extension side facing windows shall be glazed with obscure glass and non-opening below a finished floor height of 1.7 metres and shall be permanently maintained as such thereafter.

Reason: To safeguard the amenities and privacy of the occupiers of nearby properties and to comply with the following Development Plan policies for Merton: policy D4 of the London Plan 2021, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D2 and D3 of Merton's Sites and Policies Plan 2014.

5. H09: The development shall not commence until details of the provision to accommodate all site workers', visitors' and construction vehicles and loading /unloading arrangements during the construction process have been submitted to and approved in writing by the Local Planning Authority. The approved details must be implemented and complied with for the duration of the construction process.

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies T4 and T7 of the London Plan 2021, policy CS20 of Merton's Core Planning Strategy 2011 and policy DM T2 of Merton's Sites and Policies Plan 2014.

6. E06 Ancillary Residential Accommodation: The existing outbuilding hereby permitted shall not be occupied at any time other than for purposes ancillary to the residential use of the dwelling known as ground floor flat, 9 Hamilton Road.

Reason: To safeguard the amenities of neighbouring residents, to prevent the unauthorised introduction of an independent use and to ensure compliance with the following Development Plan policies for Merton: policies D3 and D4 of the London Plan 2021, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D2 and D3 of Merton's Sites and Policies Plan 2014.

7. Access to the flat roof of the development hereby permitted shall be for maintenance or emergency purposes only, and the flat roof shall not be used as a roof garden, terrace, patio or similar amenity area.

Reason: To safeguard the amenities and privacy of the occupiers of nearby properties and to comply with the following Development Plan policies for Merton: policy D4 of the London Plan 2021, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D2 and D3 of Merton's Sites and Policies Plan 2014.

8. The development hereby approved shall not be occupied until the refuse and recycling storage facilities shown on the approved plans have been fully implemented and made available for use. These facilities shall thereafter be retained for use at all times.

Reason: To ensure the provision of satisfactory facilities for the storage of refuse and recycling material and to comply with the following Development Plan policies for Merton: policies T4 and T7 of the London Plan 2021, policy CS17 of Merton's Core Planning Strategy 2011 and policy DM D2 of Merton's Sites and Policies Plan 2014

9. The development hereby permitted shall not be occupied until the cycle parking shown on the plans hereby approved has been provided and made available for use. These facilities shall be retained for the occupants of and visitors to the development at all times.

Reason: To ensure satisfactory facilities for cycle parking are provided and to comply with the following Development Plan policies for Merton: policy T5 of the London Plan 2021, policy CS18 of Merton's Core Planning Strategy 2011 and policy DM T1 of Merton's Sites and Policies Plan 2014.

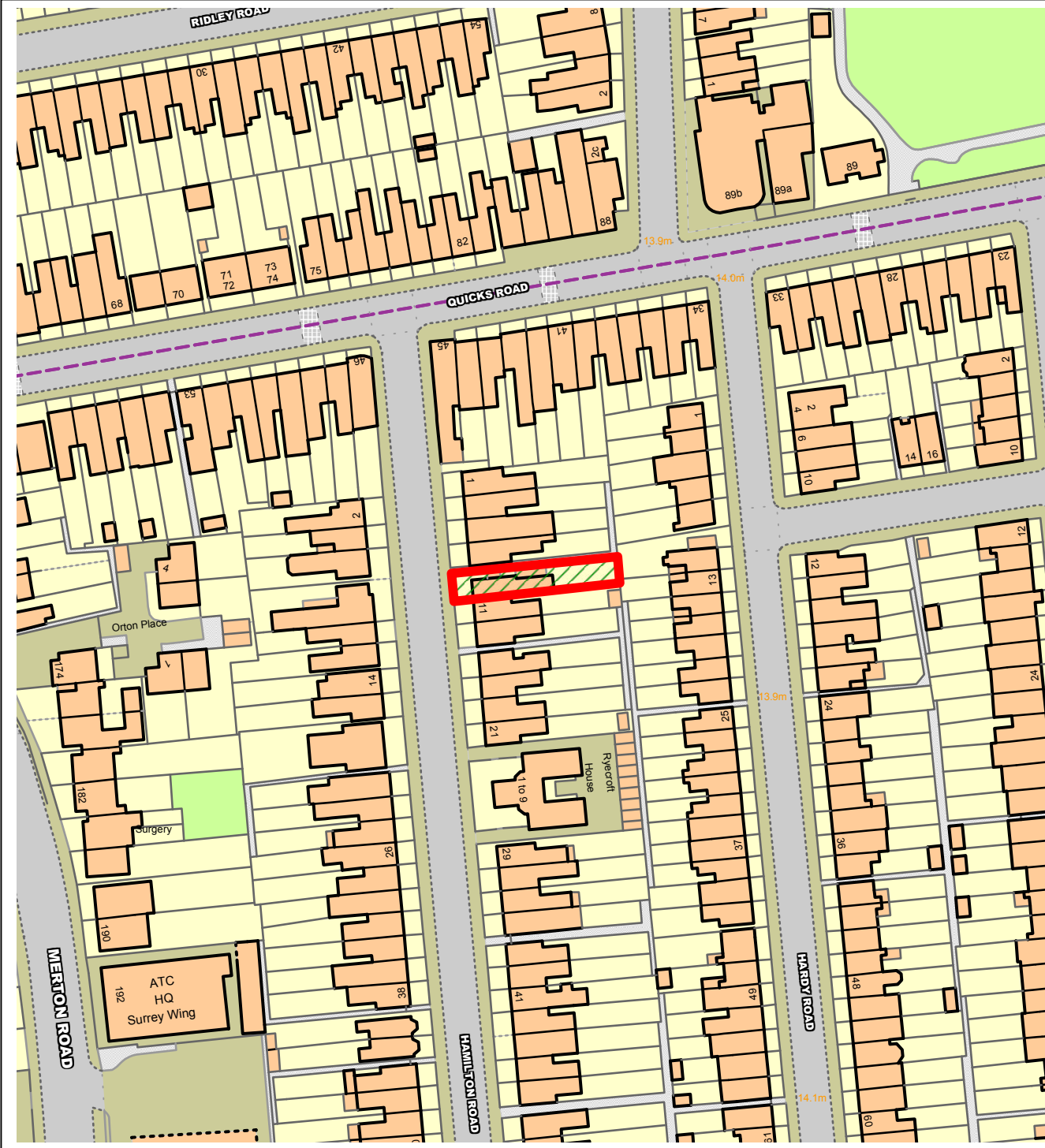
10. No part of the development hereby approved shall be occupied until evidence has been submitted to, and approved in writing by, the Local Planning Authority confirming that the development has achieved CO2 reductions of not less than a 19% improvement on Part L regulations 2013, and internal water consumption rates of no greater than 105 litres per person per day.

Reason: To ensure that the development achieves a high standard of sustainability and makes efficient use of resources and to comply with the following Development Plan policies for Merton: Policy SI 2 and SI 3 of the London Plan 2021 and Policy CS15 of Merton's Core Planning Strategy 2011.

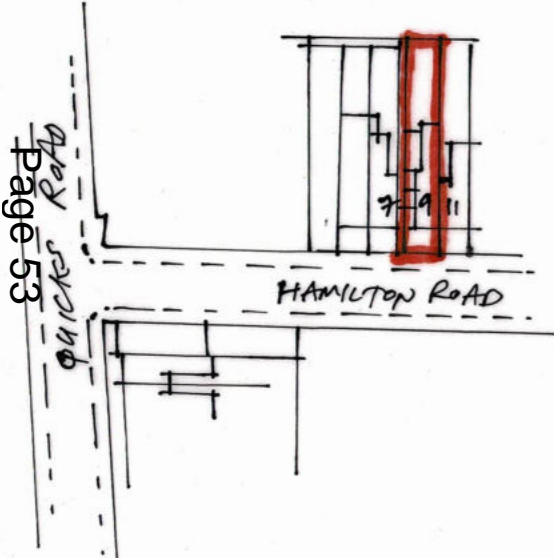
Informatives:

1. INFORMATIVE: You are advised to contact the Council's Highways team on 020 8545 3700 before undertaking any works within the Public Highway to obtain the necessary approvals and/or licences. Please be advised that there is a further charge for this work. If your application falls within a Controlled Parking Zone this has further costs involved and can delay the application by 6 to 12 months.
2. INFORMATIVE: Any works/events carried out either by, or at the behest of, the developer, whether they are located on, or affecting a prospectively maintainable highway, as defined under Section 87 of the New Roads and Street Works Act 1991, or on or affecting the public highway, shall be co-ordinated under the requirements of the New Roads and Street Works Act 1991 and the Traffic management Act 2004 and licensed accordingly in order to secure the expeditious movement of traffic by minimising disruption to users of the highway network in Merton. Any such works or events commissioned by the developer and particularly those involving the connection of any utility to the site, shall be co-ordinated by them in liaison with the London Borough of Merton, Network Coordinator, (telephone 020 8545 3976). This must take place at least one month in advance of the works and particularly to ensure that statutory undertaker connections/supplies to the site are co-ordinated to take place wherever possible at the same time.

NORTHGATE SE GIS Print Template

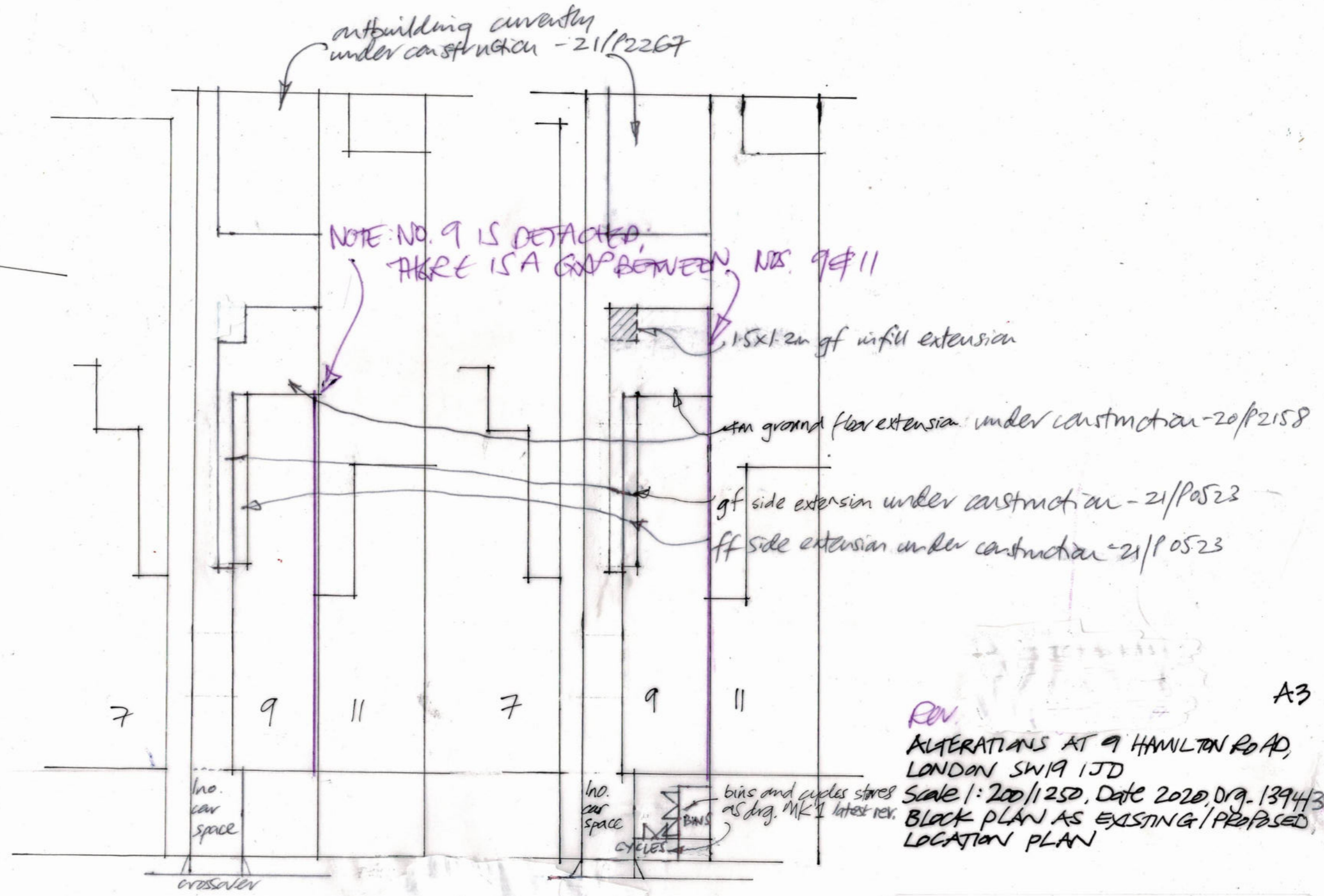


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Scale 1:1250

LOCATION PLAN 1:1250



outbuilding currently under construction - 21/P2267

NOTE: NO. 9 IS DETACHED; THERE IS A GAP BETWEEN NOS. 9 & 11

1.5x1.2m gf infill extension

4m ground floor extension under construction - 20/P2158

gf side extension under construction - 21/P0523

ff side extension under construction - 21/P0523

no. car space

no. car space

bins and cycles stores as diag. MK1 latest rev.

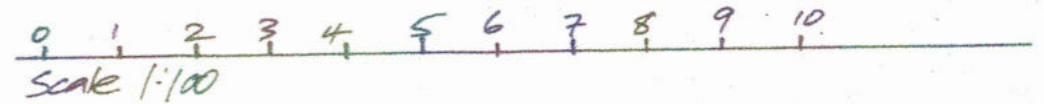
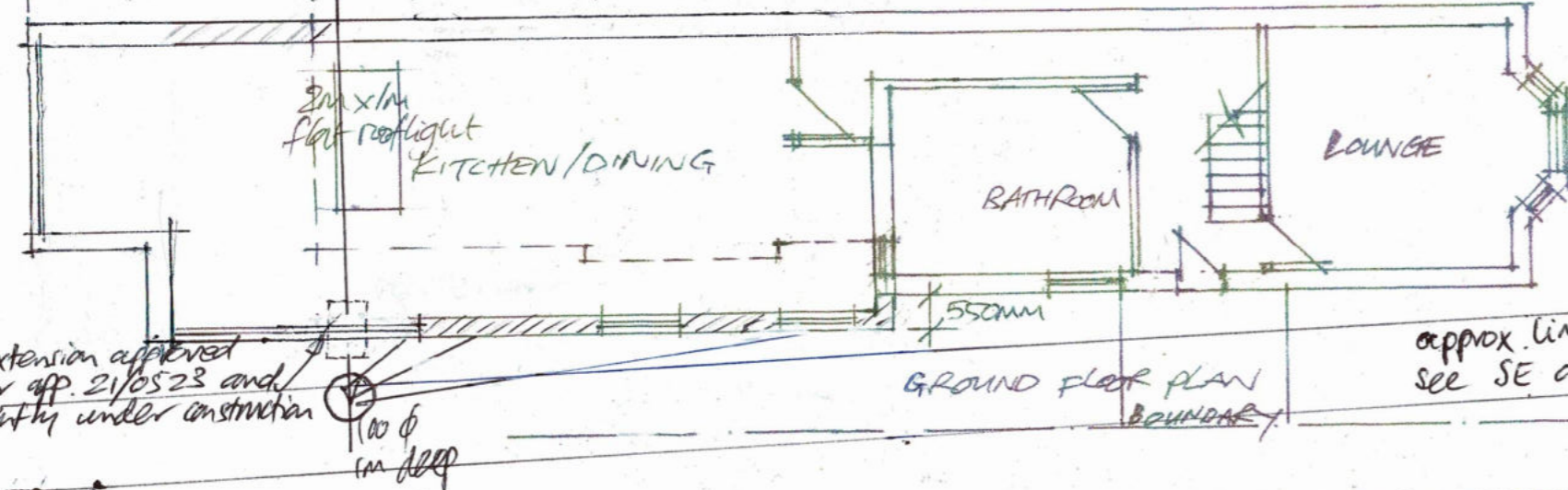
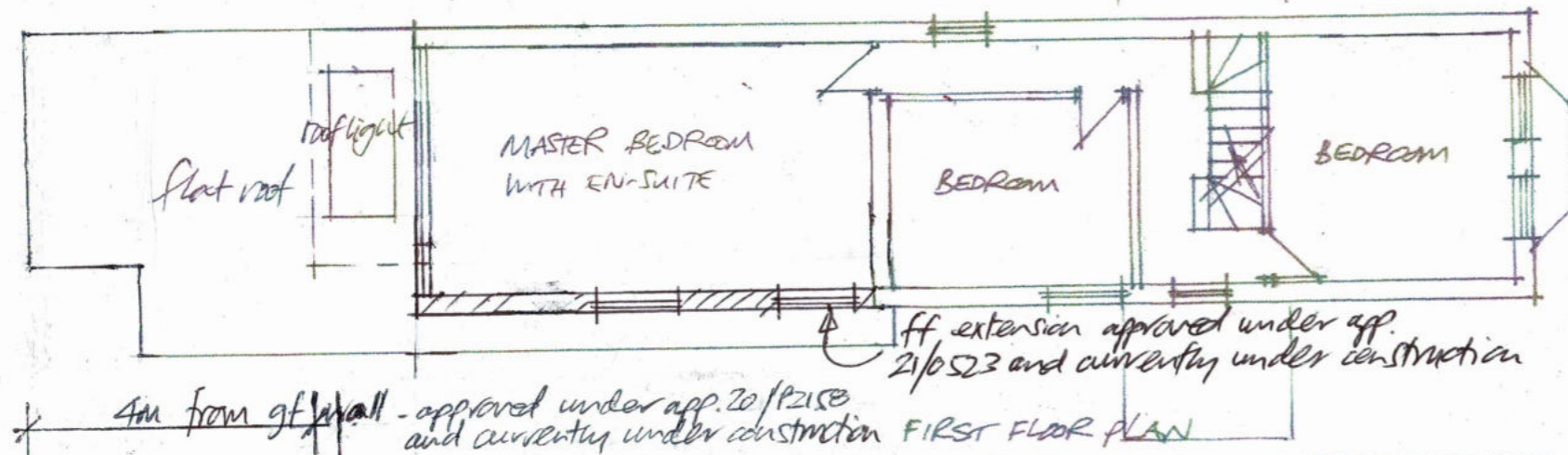
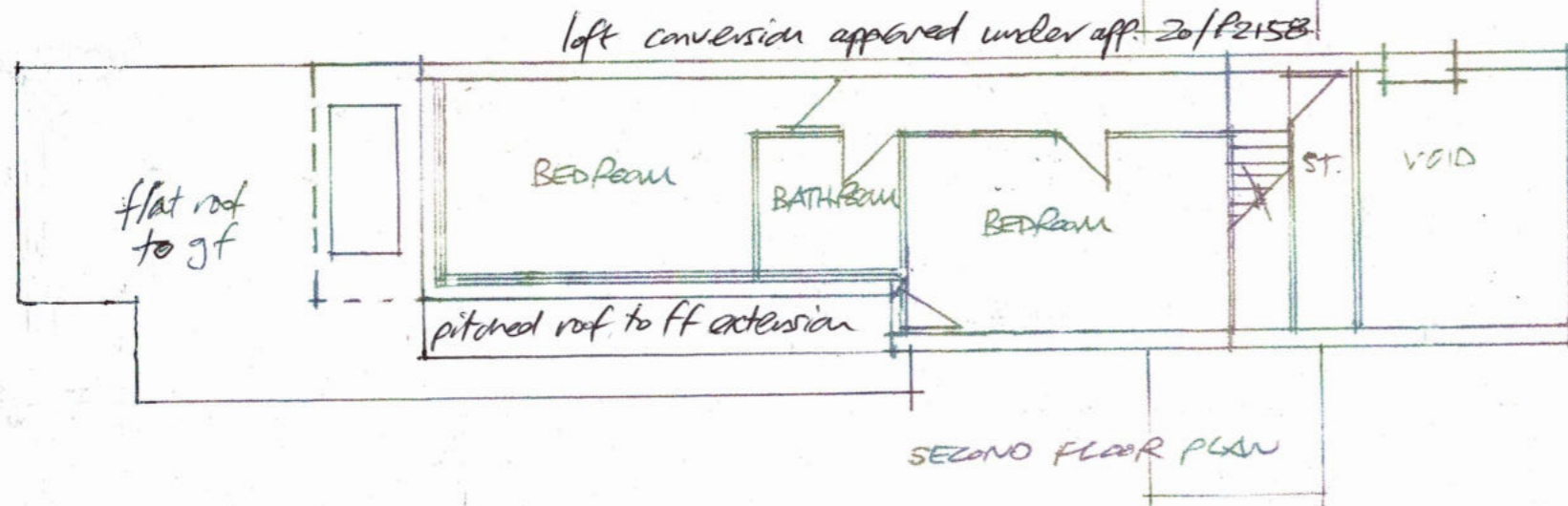
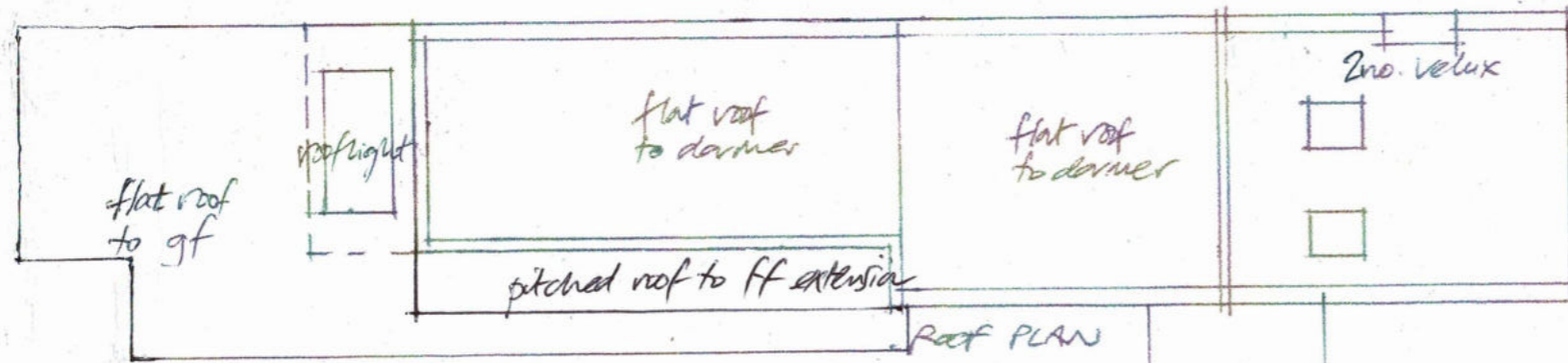
Block plan as existing

Block plan as proposed

Scale 1:200

REV A3 ALTERATIONS AT 9 HAMILTON ROAD, LONDON SW19 1JD Scale 1:200/1250, Date 2020, Drg. 1394/306 BLOCK PLAN AS EXISTING / PROPOSED, LOCATION PLAN

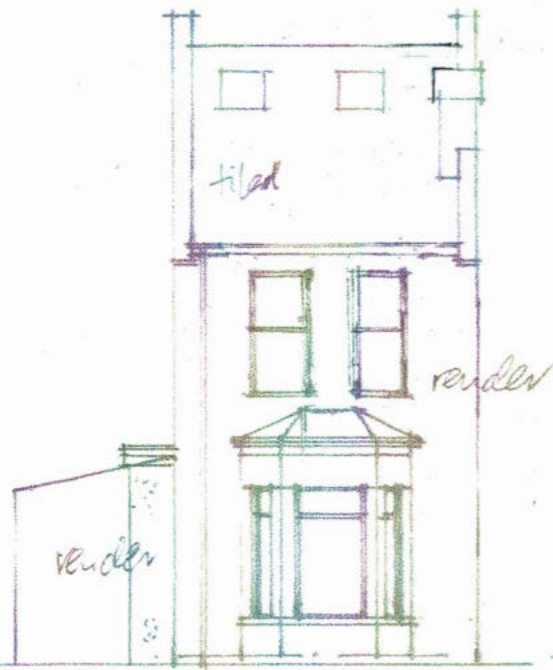
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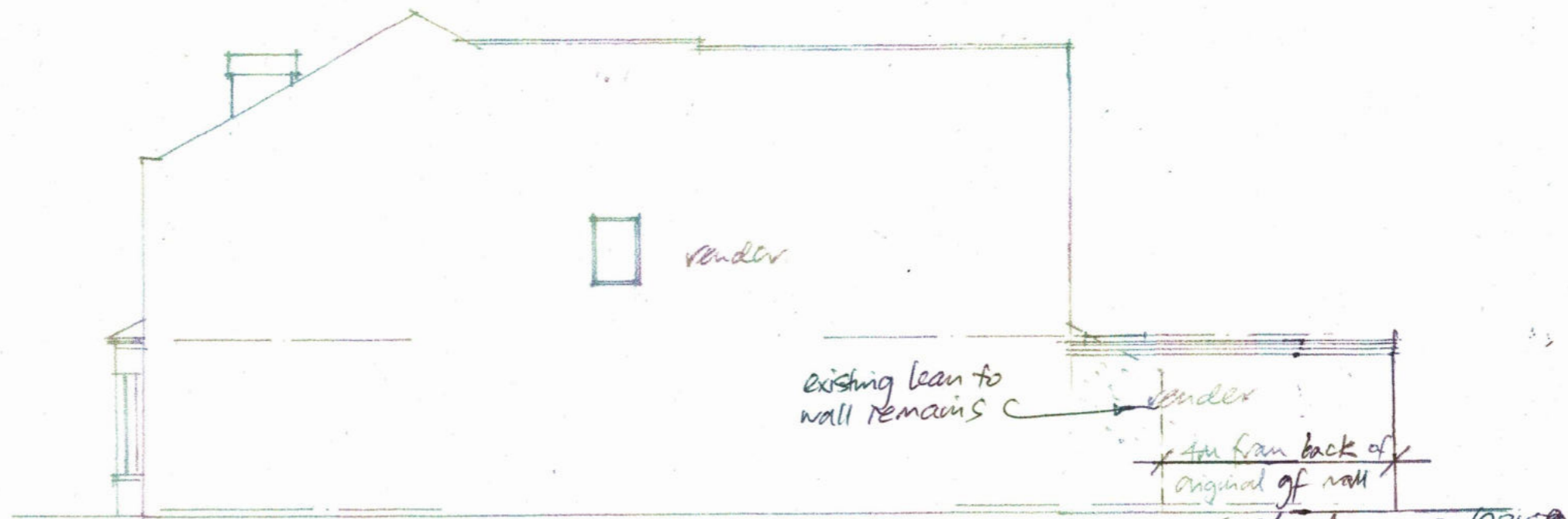
REV
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 LONDON, SW19 1JD
 Scale 1:100, Date 2020, Drg. 1394/300
 PLANS AS EXISTING

A3

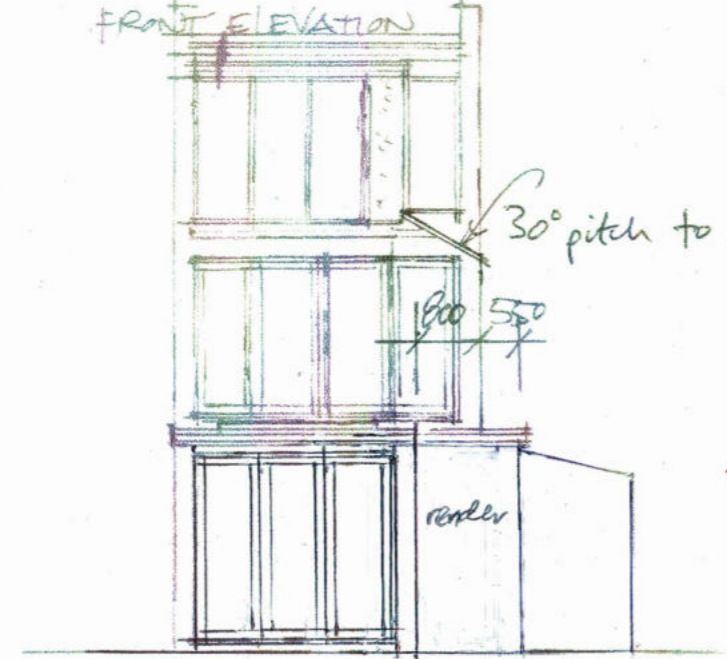
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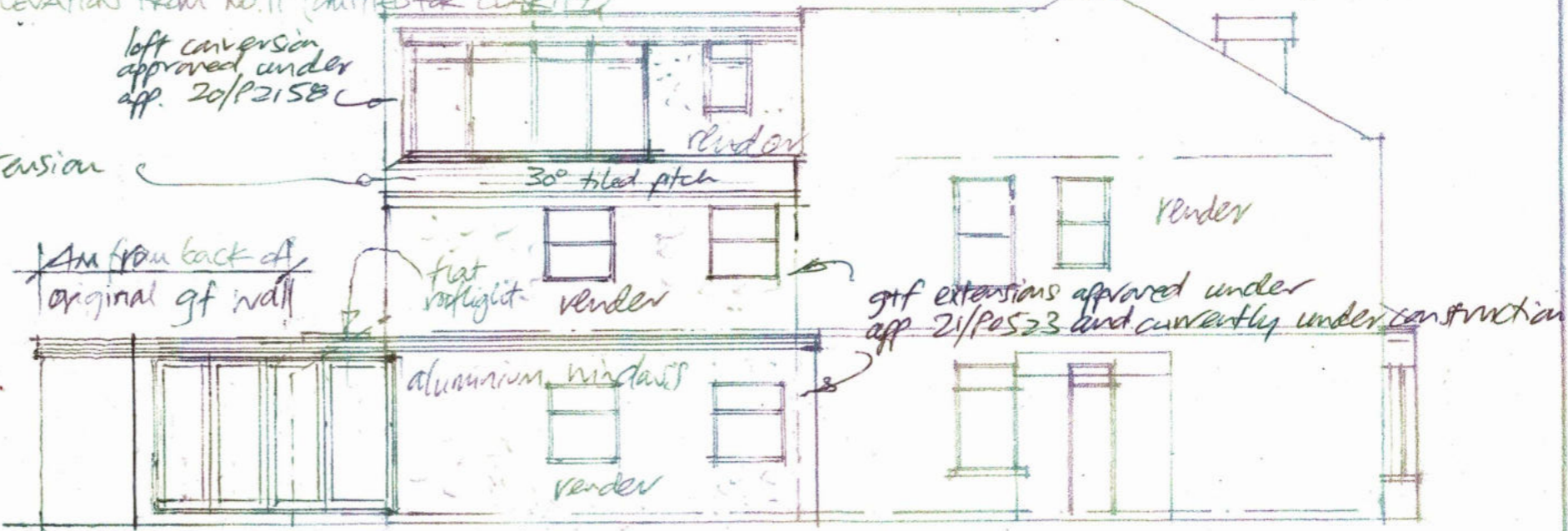
FRONT ELEVATION



SIDE ELEVATION FROM NO. 11 (OMITTED FOR CLARITY)



REAR ELEVATION



SIDE ELEVATION FROM NO. 7

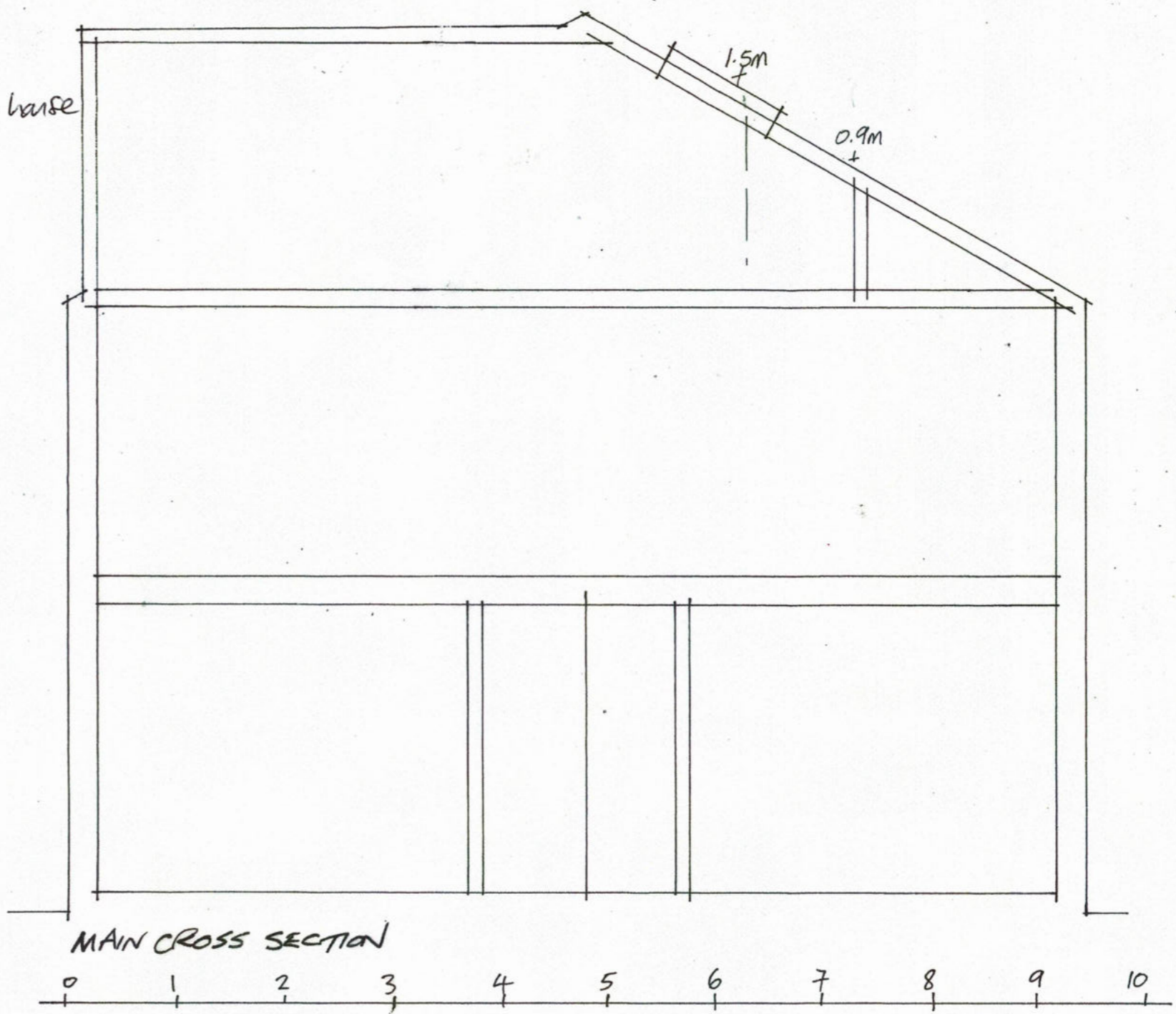
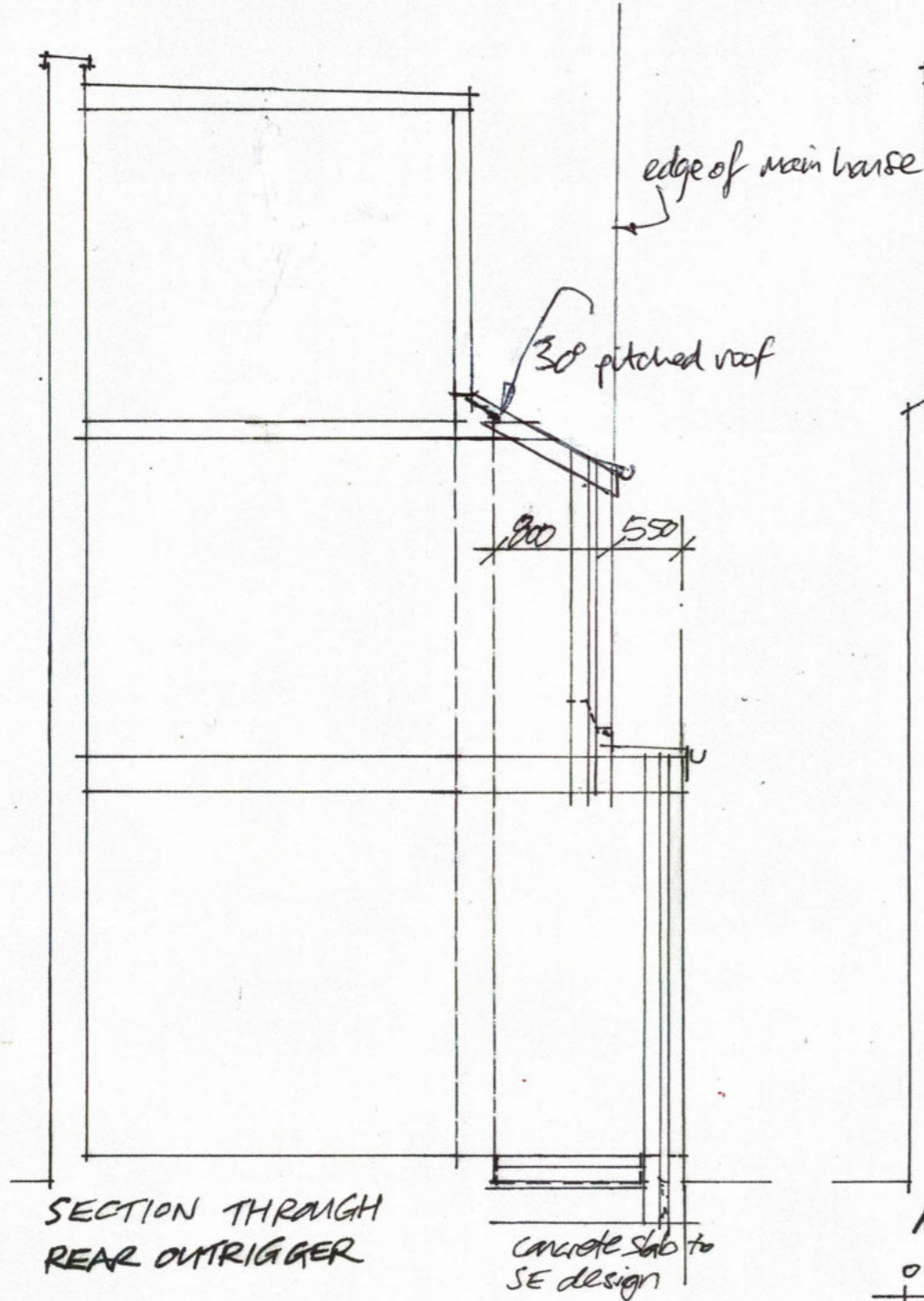


Rev.
ALTERATIONS AT 9 HAWLTON ROAD,
LONDON SW19 1SD
Scale 1:100, Date 2020, Drg. 1394/301
ELEVATIONS AS EXISTING

A3

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ALTERATIONS AT 9 HAMILTON ROAD,
 LONDON SW19 1TD
 Scale 1:50, Date 8/2020, Dwg. 1394/302
 SECTIONS AS EXISTING

A3

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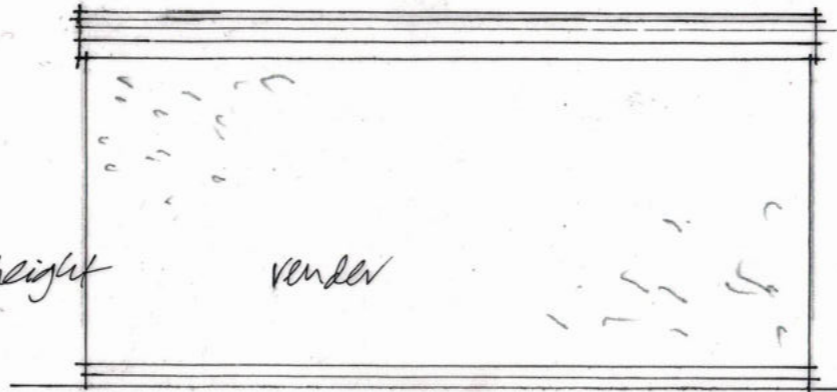
REAR Boundary

walls to be prefabricated,
pre finished insulated panels

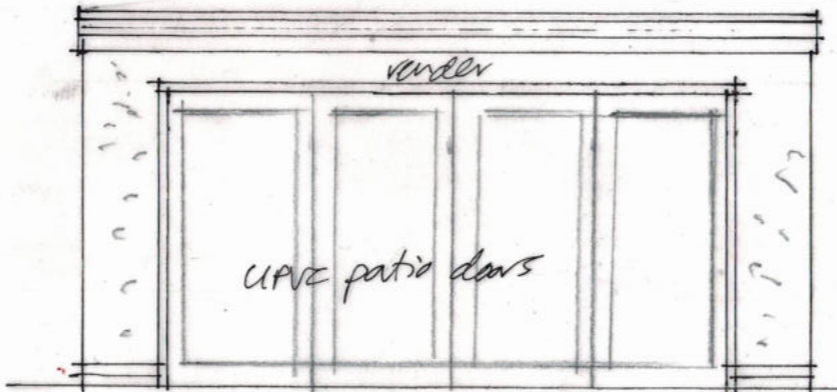
2 no. 1500x1000
flat rooflights ← 2.5m height

GAMES ROOM
4.8 x 6.5m EXTERNAL = 31.2m²
no sleeping accommodation
no kitchen
no bathroom
max. 2.5m height
exempt building Regs.
if less than 30m² internal
area (26.84m²) and
constructed mainly of
non combustible materials

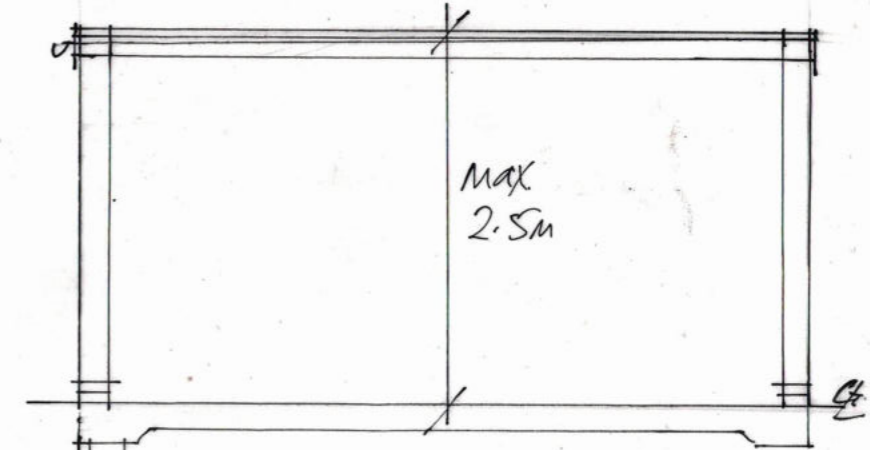
PLAN (see diag. 1394/306 for location) 1:50



REAR ELEVATION 1:50

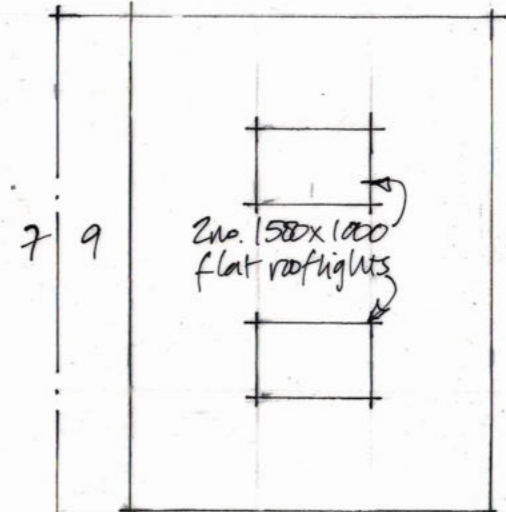


FRONT ELEVATION FROM GARDEN 1:50

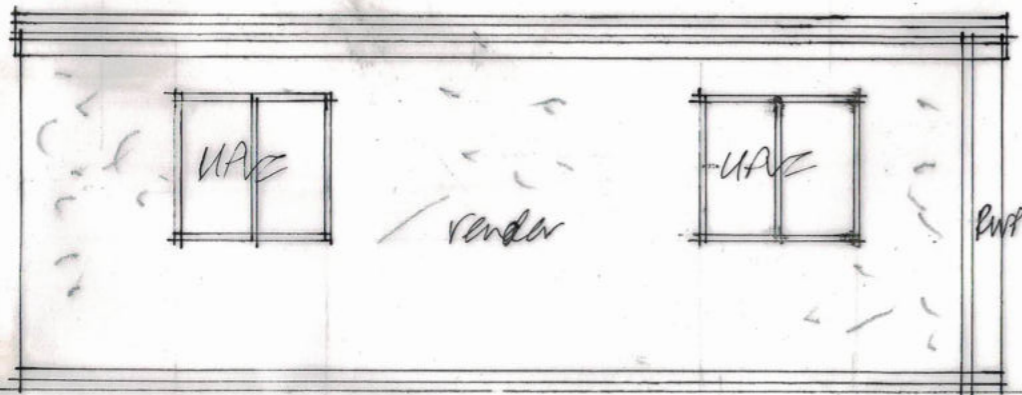


CROSS SECTION ON 1:50

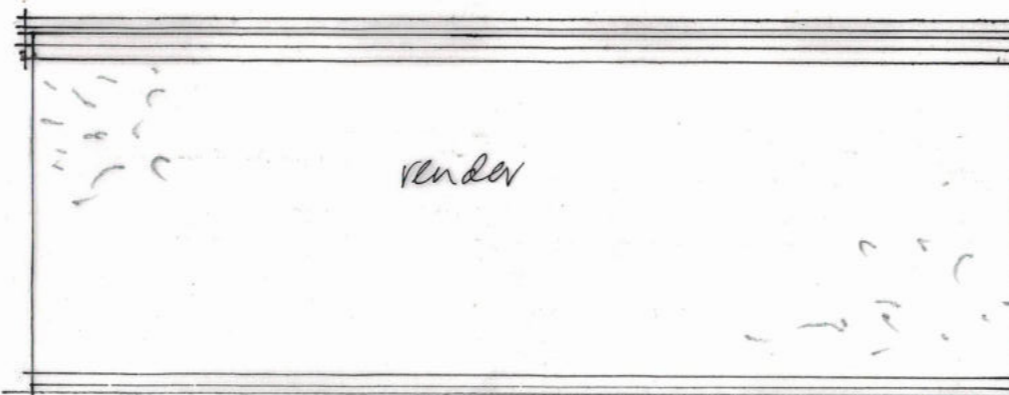
piles if required



ROOF PLAN 1:100



SIDE ELEVATION FROM NO. 7 1:50
Scale 1:100
Scale 1:50



SIDE ELEVATION FROM NO. 11 1:50

ALTERATIONS AT 9 HAMILTON ROAD,
LONDON SW19 1JD
Scale 1:50/100, Date 6/21, Dwg. 1394/307
PLAN, SECTION, ELEVATIONS OF
GAMES ROOM IN REAR GARDEN

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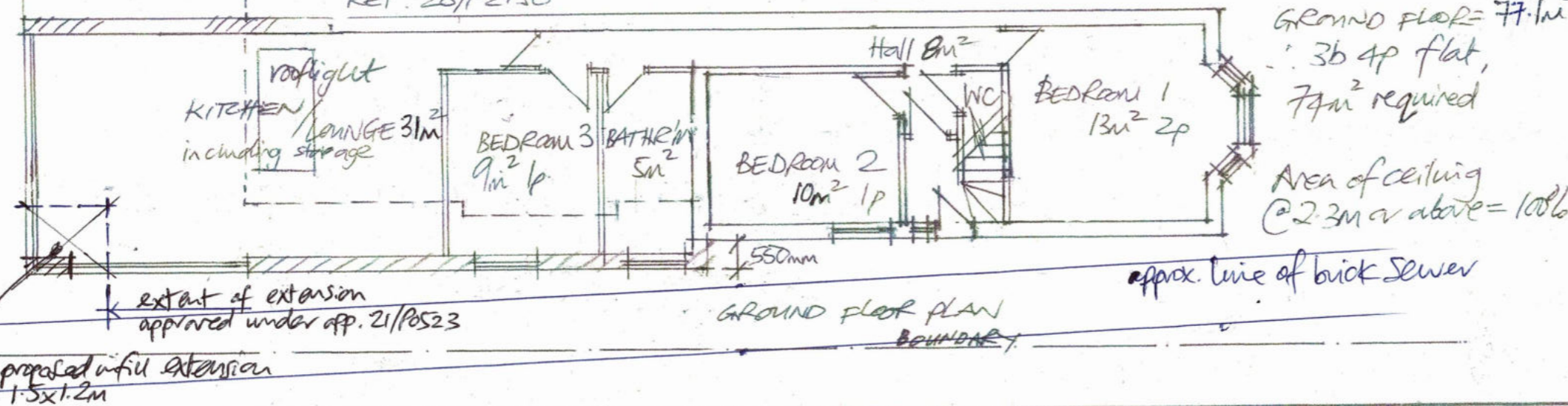
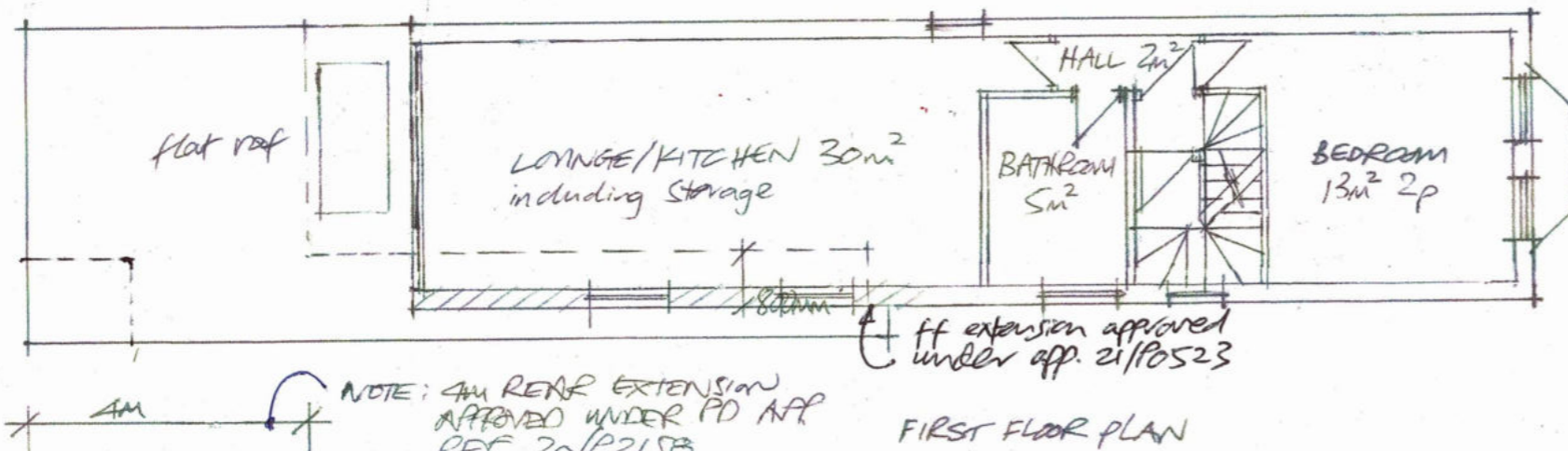
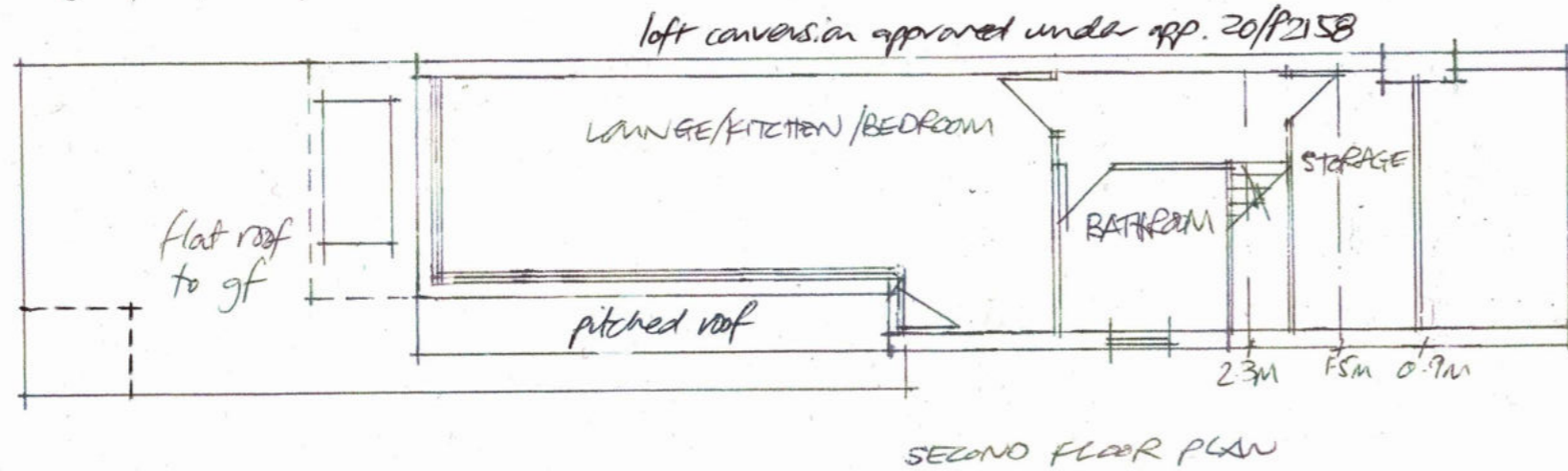
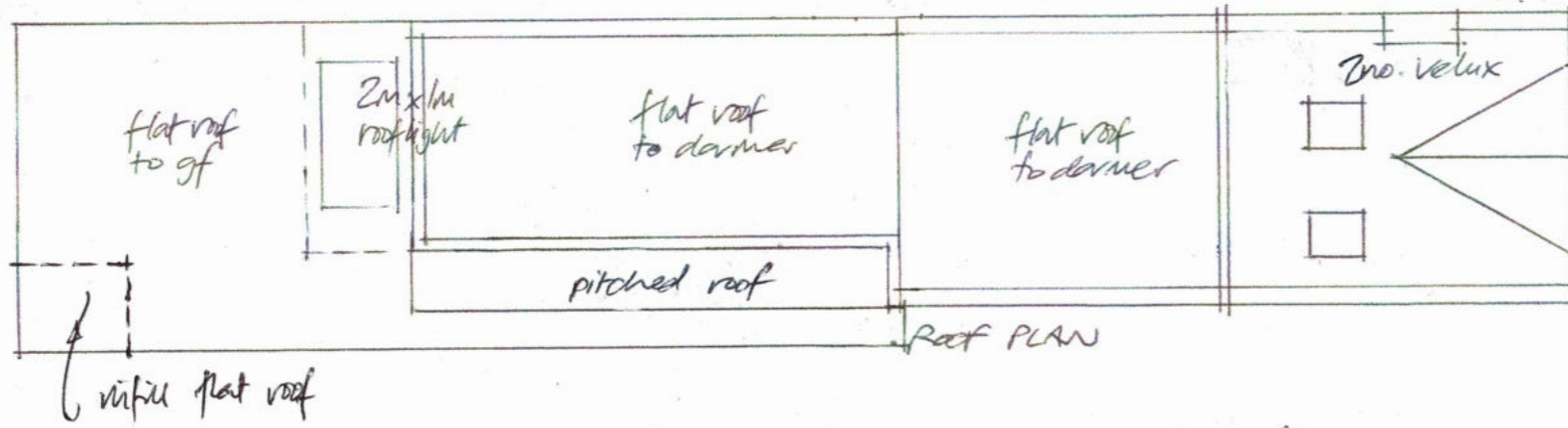
CAN BE SCALED FOR PLANNING ONLY

A3

Boundary

Page 57

Heatherbank M077 0800 0132514

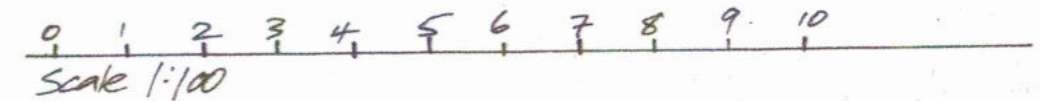


SECOND FLOOR = $38.8m^2$ plus $1.8m^2$ limited headroom, between 0.9 and 1.5m, counted as 50% = $40.6m^2$. studio flat with bathroom, $39m^2$ required minimum

Area of ceiling @ 2.3m = $34.3/40.6 = 78\%$ OK

FIRST FLOOR = $55m^2 - 5m^2$ for stairs/access = $50m^2$, no 1b2p flat, $50m^2$ required

Area of ceiling @ 2.3m = 100%



Plans
ALTERATIONS AT 9 HAMILTON ROAD,
LONDON, SW19 1JD
Scale 1:100, Date 2020, Drg. 1394/303
PLANS AS PROPOSED FOR
FLAT CONVERSION AND GROUND FLOOR
REAR INFILL EXTENSION A3

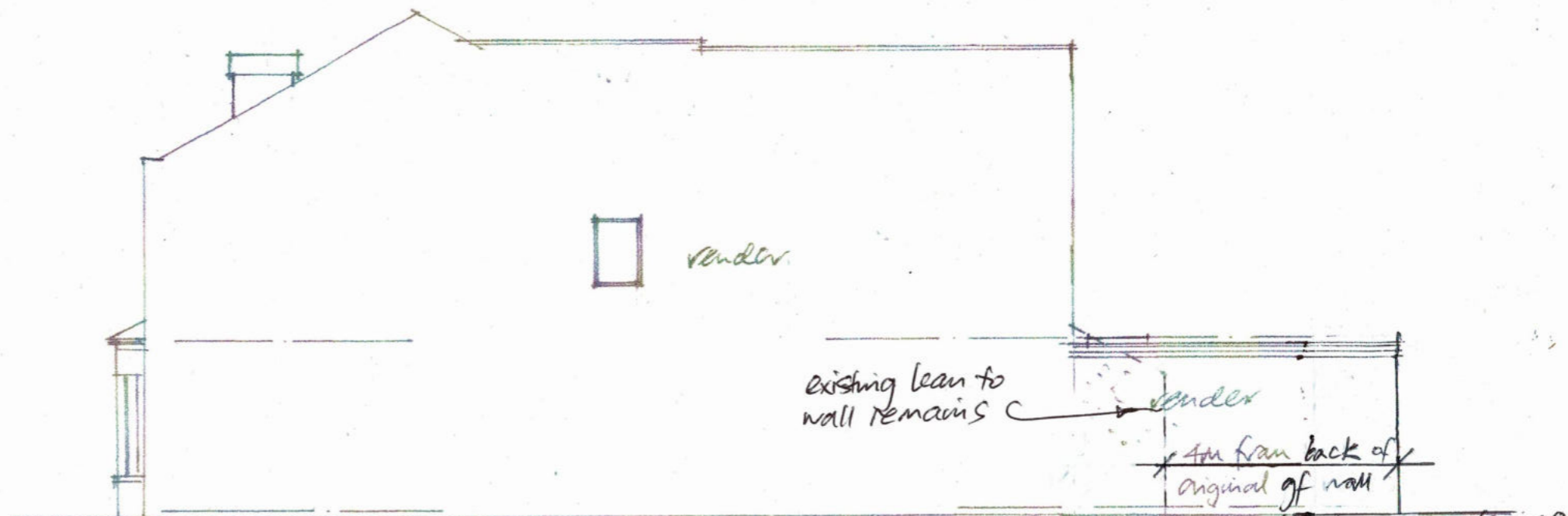
GROUND FLOOR = $77.1m^2$
∴ 3b 4p flat,
 $77m^2$ required

Area of ceiling
@ 2.3m or above = 100%

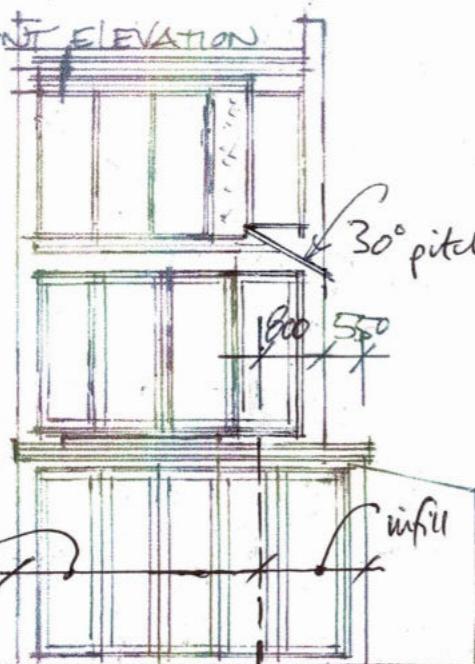
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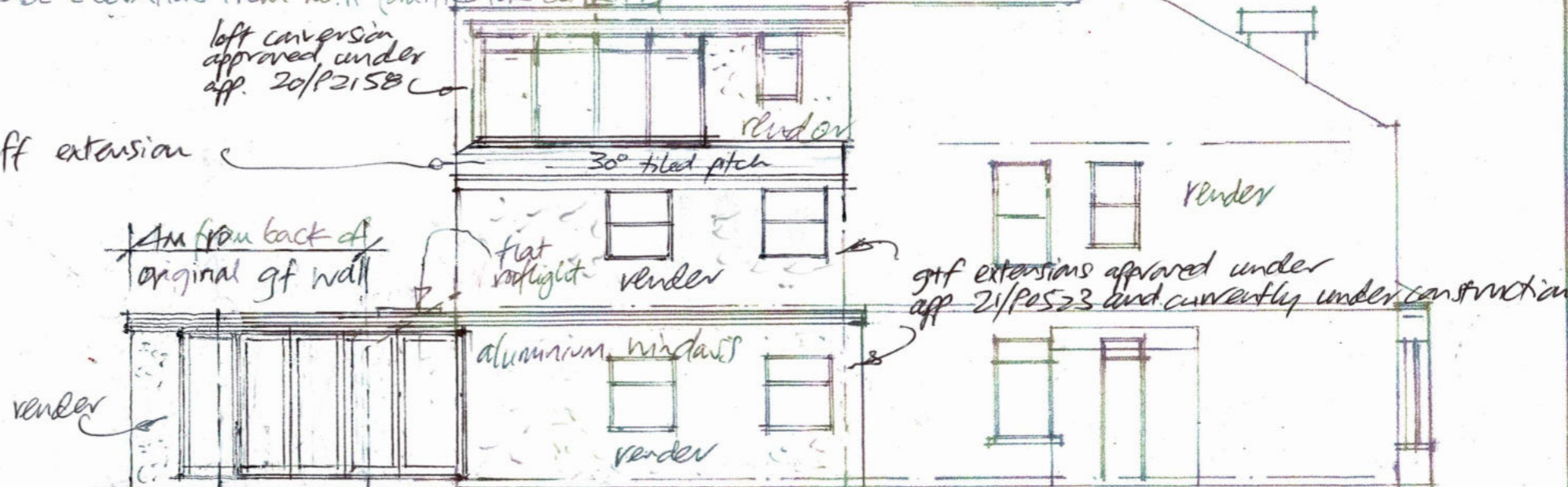
FRONT ELEVATION



SIDE ELEVATION FROM NO. 11 (OMITTED FOR CLARITY)



REAR ELEVATION



SIDE ELEVATION FROM NO. 7

0 1 2 3 4 5 6 7 8 9 10
Scale 1:100

Page 59

approved under app. 20/P2158 and currently under construction

loft conversion approved under app. 20/P2158

30° pitch to side ff extension

4m from back of original of wall

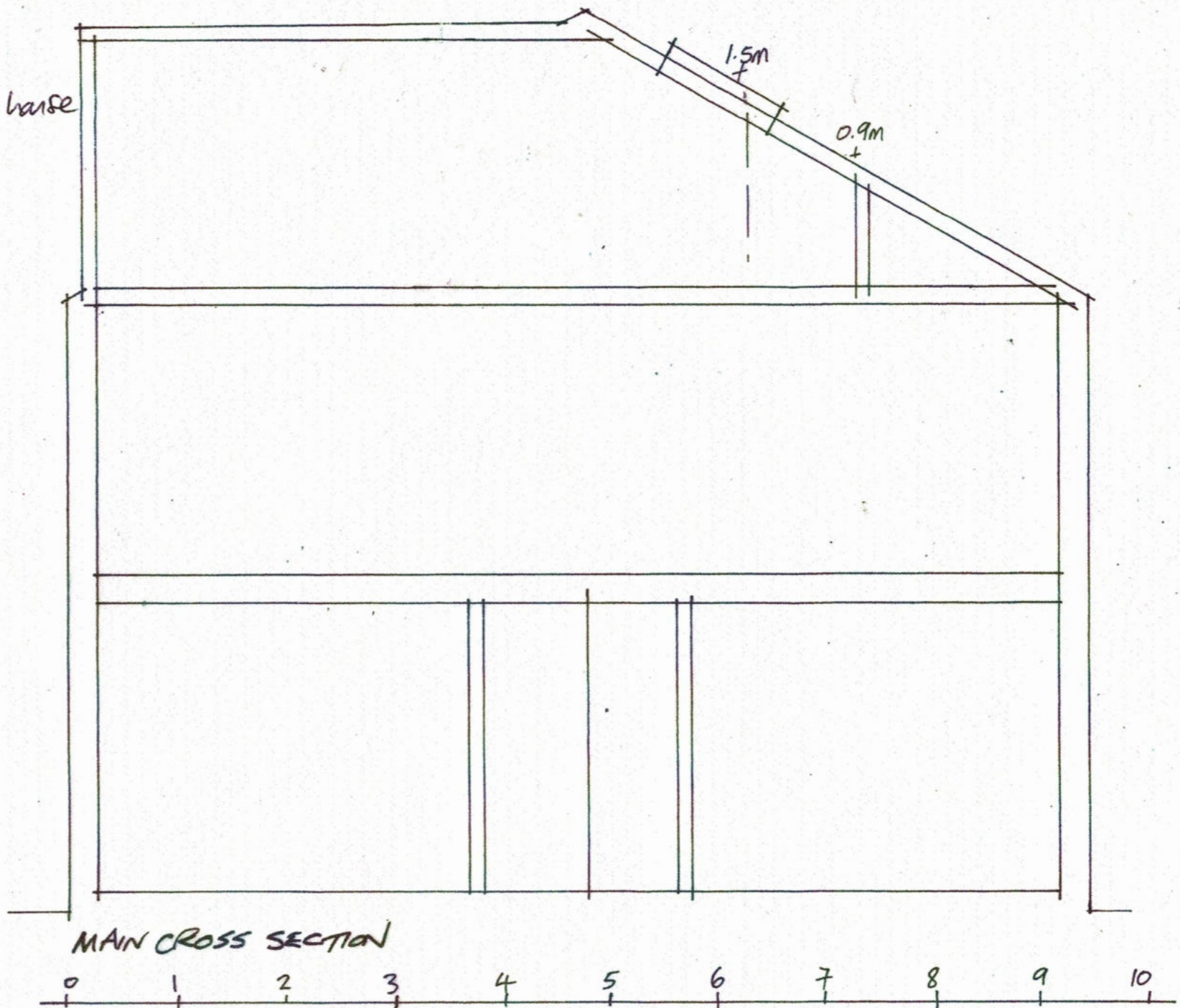
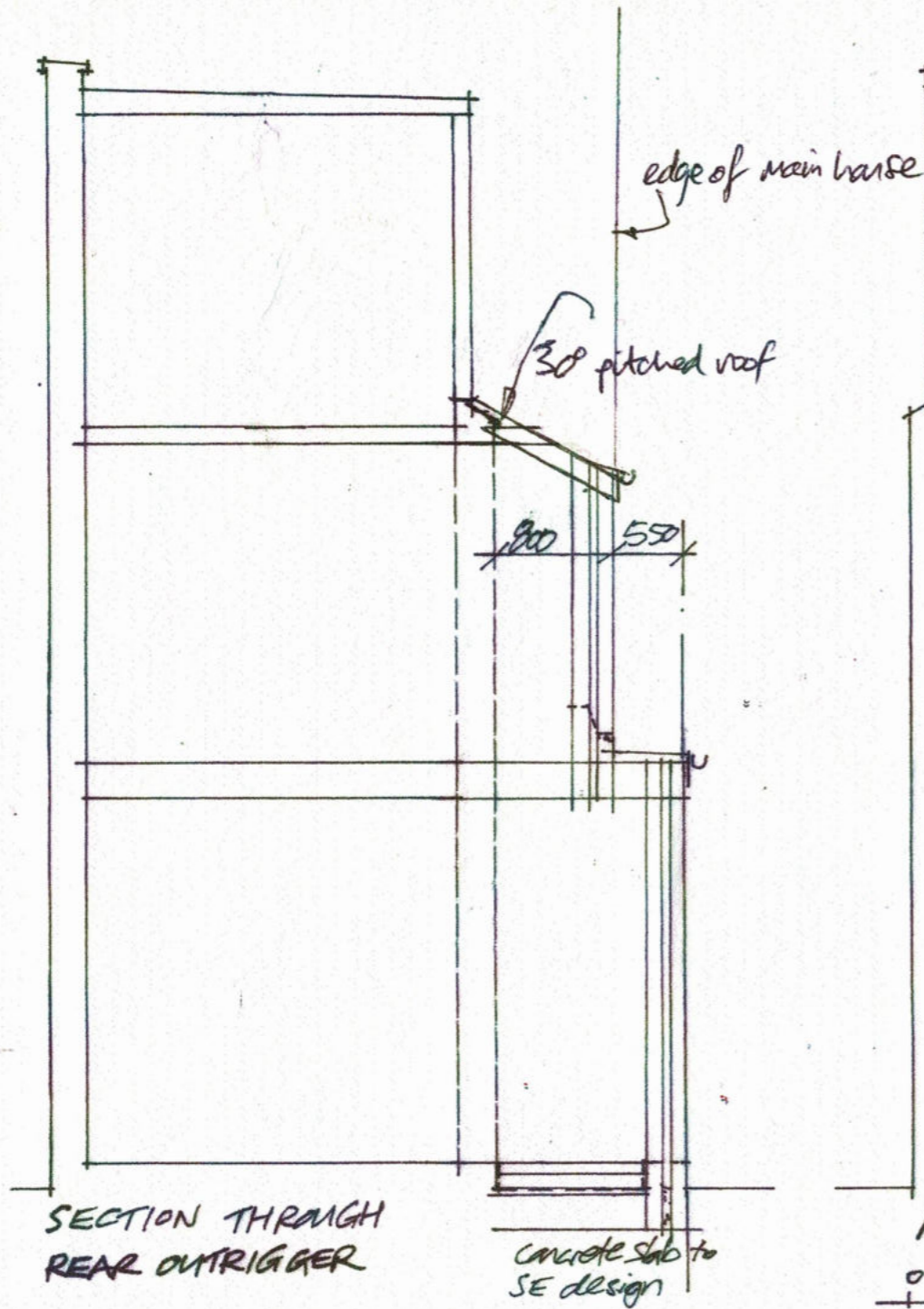
g/f extensions approved under app 21/P0523 and currently under construction

Rev.
ALTERATIONS AT 9 HAWLTON ROAD,
LONDON SW19 1SD
Scale 1:100, Date 2020, Drg. 1394/304
ELEVATIONS AS PROPOSED FOR
FLAT CONVERSION & REAR G/F INFILL EXTENSION **A3**

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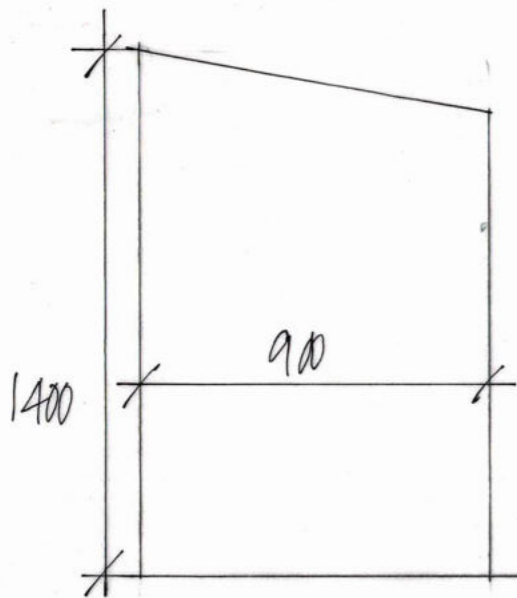


OPERATIONS AT 9 HAMILTON ROAD,
 LONDON SW19 1TD
 Scale 1:50, Date 8/2020, Dwg. 1394/305
 SECTIONS AS PROPOSED -
 no alterations

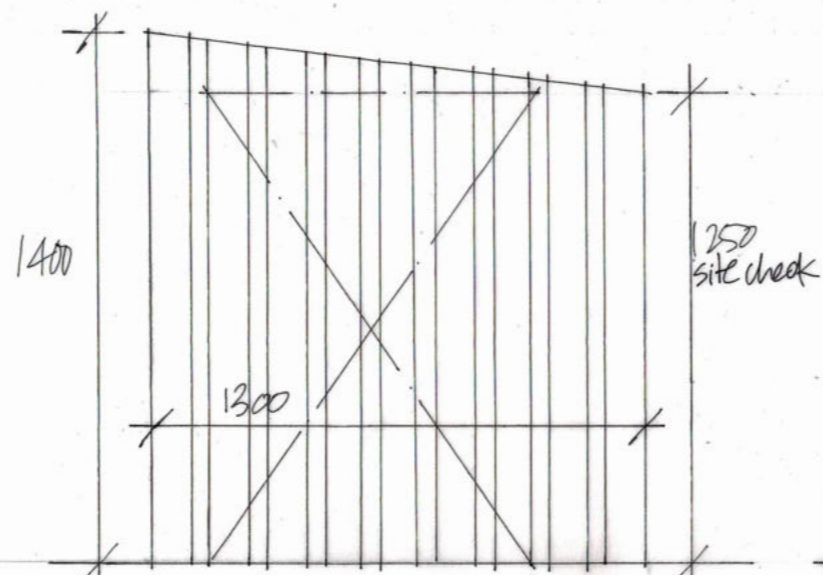
A3

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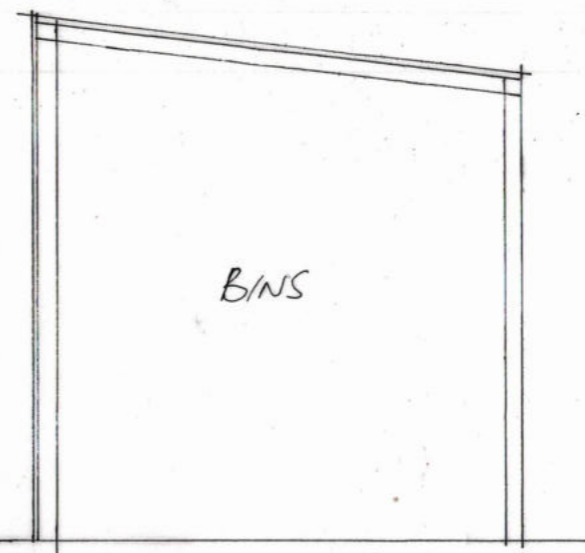
SCALE FOR PLANNING ONLY



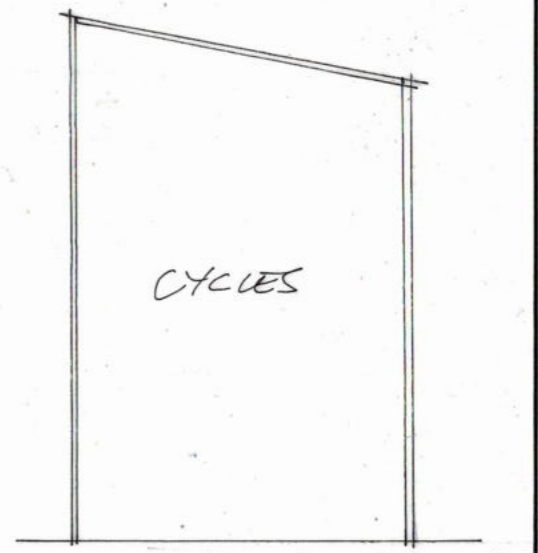
SIDE ELEVATIONS



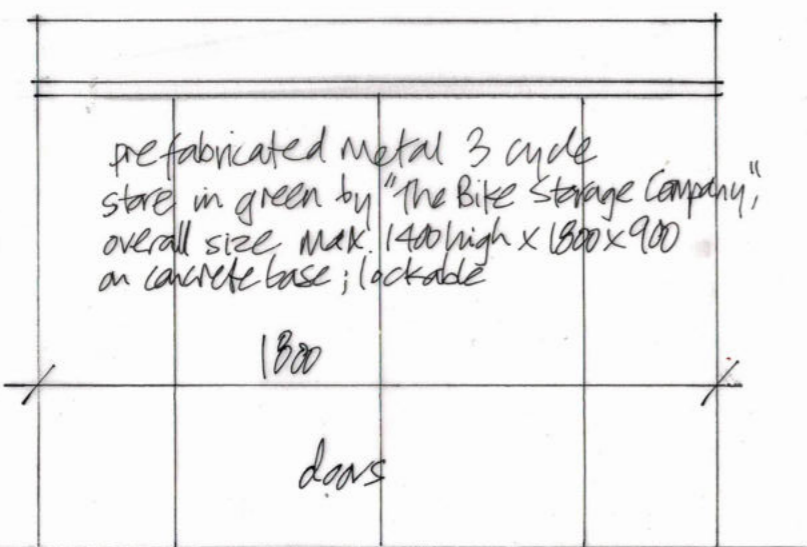
SIDE ELEVATIONS



SECTION THROUGH BINS STORE

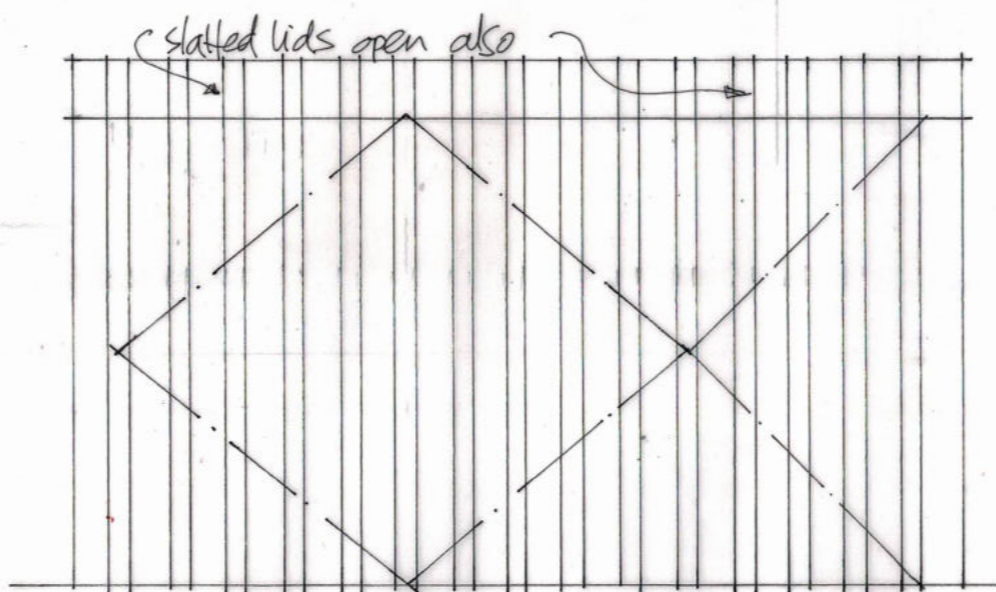


SECTION THROUGH CYCLE STORE



FRONT ELEVATION

pre fabricated metal 3 cycle store in green by "The Brite Storage Company", overall size max. 1400 high x 1800 x 900 on concrete base; lockable

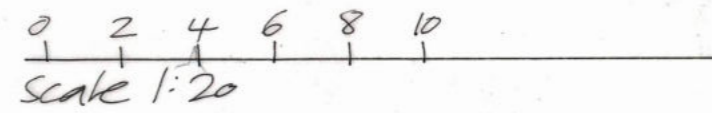


FRONT ELEVATION

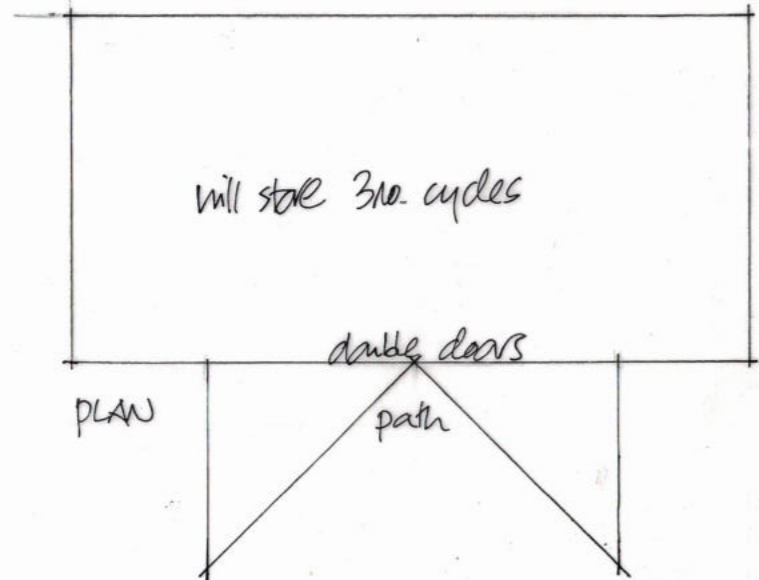
construction:
 100x25mm softwood slats on
 50x25mm sw battens/frame
 with 75x75mm corner posts
 and bracing; all treated/stained green;
 front double opening doors for 660 L bin
 front single opening door for 360L & 23L bins
 double top opening lid for 660L bin
 single top opening lid for 360L bin,
 concrete base, 5mm ventilation gaps between slats

Rights to Light/Party Wall Act/CDM Reg's may apply
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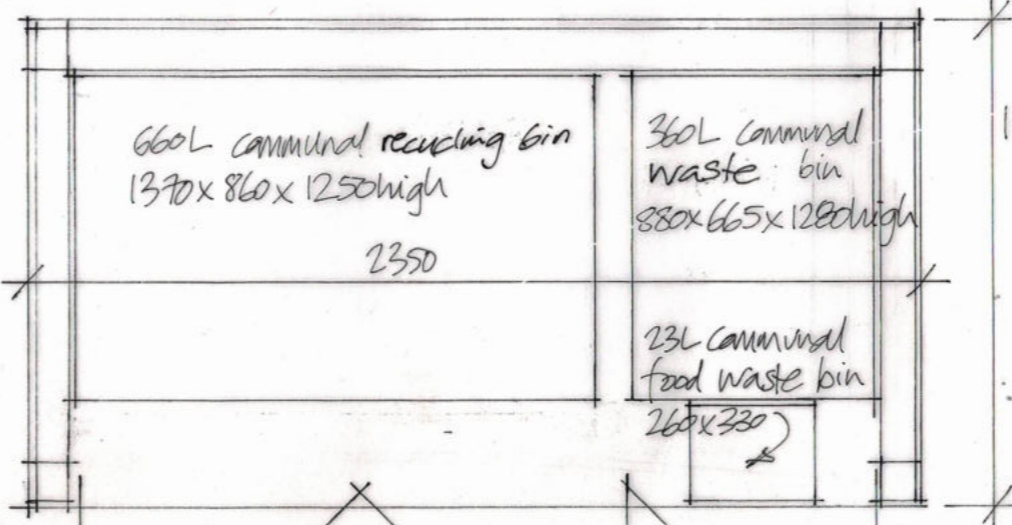
A3



B1/21 Minor
 Revs. R/20 Sections added
 BIN STORAGE & CYCLE STORAGE
 Scale 1:20 Date 2020 Drg. MK1 A/B



PLAN



PLAN

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ANNING APPLICATIONS COMMITTEE
18th August 2022

Item No:

UPRN

APPLICATION NO.

DATE VALID

21/P0400

20/01/2021

Address/Site:

19 Worple Road, Wimbledon, SW19 4JS

(Ward)

Hillside

Proposal:

Demolition of existing structures & redevelopment to provide a new 8-storey (plus basement) building comprising 7330sqm of GIA floorspace, with retail at ground and first floor levels and office above (Class E); landscaping; ground works; associated infrastructure and development

Drawing Nos:

ZZ-XX-DR-A-91-0001(P10), ZZ-XX-DR-A-91-0002(P4),
ZZ-01-DR-A-01-0001(P16), ZZ-GF-DR-A-01-0001(P18),
ZZ-01-DR-A-01-0001(P16), ZZ-02-DR-A-01-0001(P12),
ZZ-03-DR-A-01-0001(P12), ZZ-04-DR-A-01-0001(P12),
ZZ-05-DR-A-01-0001(P12), ZZ-06-DR-A-01-0001(P12),
ZZ-07-DR-A-01-0001(P12), ZZ-RF-DR-A-01-0001(P7),
ZZ-XX-DR-A-02-0001(P10), ZZ-XX-DR-A-02-0002(P9),
ZZ-XX-DR-A-02-0003(P9), ZZ-XX-DR-A-02-0004(P10),
ZZ-XX-DR-A-03-0001(P9), ZZ-XX-DR-A-03-0002(P8),
ZZ-XX-DR-A-03-0003(P8), ZZ-XX-DR-A-03-0004(P9),
ZZ-XX-DR-A-03-0005(P9), 892-PL-002(P02)

Contact Officer:

David Gardener (0208 545 3115)

RECOMMENDATION

GRANT Planning Permission Subject to completion of a S106 Agreement, and conditions

CHECKLIST INFORMATION

- Heads of agreement: S278 for public realm improvements, carbon offset, financial contribution for short stay cycle spaces, financial contribution for on-street disabled parking bay, restriction on size of servicing and delivery vehicles
- Is a screening opinion required: No
- Is an Environmental Statement required: No
- Has an Environmental Impact Assessment been submitted: No

- Press notice: Yes
- Site notice: Yes
- Design Review Panel consulted: No
- Number of neighbours consulted: 129
- External consultations: Transport for London, Metropolitan Police (Designing out crime), Thames Water, Historic England

1. INTRODUCTION

- 1.1 The application has been brought before the Planning Applications Committee due to the nature and number of objections received following public consultation.

2. SITE AND SURROUNDINGS

- 2.1 The application site comprises a 1960s built seven storey Use Class E(g)(i) office building with 5,390sqm gross internal floorspace (GIA), which is located on the north side of Worple Road.
- 2.2 The immediate surrounding area is characterised by buildings of varying heights with building ranging between two and eight storeys in height. The site is bounded by low rise residential buildings to the north, Wimbledon Central, a seven storey residential building, to the southwest, and Swan Court, a five storey office building to the Northeast. Buildings on the opposite side of Worple Road are commercial, comprising the department store, Ely's and food retailer, Sainsbury's.
- 2.3 The application site is not subject to any statutory heritage asset designations although the site sits south and west of the Merton (Wimbledon Hill Road) Conservation Area.
- 2.4 The application site currently comprises 68 off-street car parking spaces, which are accessed from Worple Road. The application site has excellent public transport links (PTAL rating of 6b) being sited in very close proximity to Wimbledon tube, railway and tram station and a number of bus routes. The site is also located in a Controlled Parking Zone (Zone W1), and is subject to a range of parking controls including pay & display bays, blue badge parking bays and taxi only bays. At the site frontage there are three parking bays which allow for a maximum of two hours of parking during the hours of control, comprising 0930-1830 Monday to Saturday and 1000-1600 on Sundays. Servicing from these bays is prohibited between 0830-0930 Monday to Saturday.

3. CURRENT PROPOSAL

- 3.1 The proposal is to demolish the existing building and erect a replacement eight storey building (plus basement) with 7330sqm of GIA floorspace. This would provide an uplift of 1940sqm of floorspace compared to existing. The proposal would comprise 2108sqm of retail (Use Class E(a)) at ground and first floor level, and 5069sqm of office (Use Class E(g)(i)) above. An additional 153sqm,

which comprises some plant and basement access areas, would be shared space. The proposed building would have a maximum height of 29.8m

- 3.2 Facing materials would comprise buff blend brick on the bulk of the front, rear and side elevations with a secondary material of zinc (with red/copper tone) featured on the suspended block over the vehicle access and southwest facing side elevations. Curtain wall glazing would feature at ground and first floor levels on the front elevation, with grey RAL 7024 framing of the curtain wall shopfront. Terraces would be located at level 2 on the southwest side of the building, level 6 on the northeast side, and level 7 at the front.
- 3.3 No off-street car parking is proposed with the existing car park egress from Worple Road retained to enable access to the sub-station at the rear of the site. Servicing & refuse collection will take place on-street. This would involve shared use of the three parking bays directly in front of the site (on the west side of the existing site access) with loading only would be permitted between the hours of 7:00am-8:30am and 7:00pm-9:00pm (Mondays to Saturdays). Between the hours 8:30am-7:00pm (Mondays to Saturdays) and 10:00am-4:00pm (Sundays) the bays would be available for Pay and Display parking only (i.e. no loading), whilst overnight no restrictions would apply.
- 3.4 A total of 92 Long Stay Cycle spaces would be provided at basement level, whilst 16 short stay cycle spaces would be provided outside the front of the building. Shower and changing facilities will be provided at basement level.
- 3.5 The applicant has submitted a proposed landscape strategy, which proposes re-paving the outside the Worple Road frontage of the building in Yorkstone paving. Raised planters and external furniture is proposed for the external terraces.
- 3.6 Amended Plans: Please note that the application has been amended since it was first submitted. The glazing above the office entrance has been removed and replaced by an additional column of buff brickwork, which now wraps around the building corner. The rear of the building has been reduced in height through the reduction in depth of the top two floors. The rear of the building has also been reduced in depth by 2m. The proposed public realm has also been amended with the removal of the proposed paving bands with Yorkstone paving now proposed throughout. The proposed servicing and delivery arrangement has been amended with on-street servicing now proposed instead of off-street. This will incorporate the use of the existing 3 on-street parking bays outside the front of the building.

4. PLANNING HISTORY

The following planning history is relevant:

- 4.1 MER101/85 - Redevelopment of site by the erection of a part 5 storey part 7 storey office building comprising 4770 sqm gross with car parking involving demolition of existing building on site. Refused – 18/04/1985, for the following reason:

“ By reason of its height and massing, the proposed office building would have an adverse effect on the amenities of occupiers of adjoining residential properties in Mansel Road.”

- 4.2 98/P0454 - Recladding of existing elevations of building (excluding ground floor retail unit). Granted - 18/06/1998
- 4.3 99/P1192 - Erection of a part single, part two storey extension at the rear of the existing building (586 sqm) (outline planning permission). Granted - 13/04/2000
- 4.4 03/P2555 - Extension at the rear of the existing building to increase the height from five storeys to seven storeys to the same height as the front of the building. (outline planning permission). Granted - 15/01/2004
- 4.5 03/P2567 - Extension at the rear of the existing building to increase from five storeys to seven storeys to the same height as the front of the building (Application for approval of reserved matters following grant of outline planning permission on appeal in November 2000 (Ref: 99/P1191).
- 4.6 In September 2019 pre-application advice was sought for the demolition of existing building and erection of a nine storey building comprising office floorspace and a new retail premises at ground floor only (LBM Ref: 19/P3442)

5. POLICY CONTEXT

- 5.1 The following policies from the Adopted Sites and Policies Plan and Policies Maps (July 2014):
DM D1 (Urban design and public realm), DM D2 (Design considerations in all developments), DM D3 (Alterations and extensions to existing buildings), DM D4 (Managing heritage assets), DM E1 (Employment areas in Merton), DM E2 (Offices in town centres), DM EP2 (Reducing and mitigating noise), DM F1 (Support for flood risk management), DM F2 (Sustainable urban drainage systems SuDS, wastewater and water infrastructure) DM R1 (Location and scale of development in Merton’s town centres and neighbourhood parades), DM T1 (Support for sustainable transport and active travel), DM T2 (Transport Impacts of development), DM T3 (Car parking and servicing standards), DM T5 (Access to the road network)
- 5.2 The relevant policies in the Adopted Core Strategy (July 2011) are:
CS.6 (Wimbledon Town Centre), CS.7 (Centres), CS.12 (Economic development), CS.14 (Design), CS.15 (Climate Change), CS. 16 (Flood Risk Management), CS.18 (Active Transport), CS.19 (Public Transport), CS.20 (Parking, Servicing and Delivery)
- 5.3 The relevant policies in the London Plan (2021) are:
GG5 (Growing a good economy), GG6 (Increasing efficiency and resilience), SD6 (Town centres and High Streets), SD8 (Town Centre Network), D2 (Infrastructure requirements for sustainable densities), D3 (Optimising site capacity through the design-led approach), D4 (Delivering Good Design), D5

(Inclusive design), D8 (Public Realm), D9 (Tall Buildings), D11 (Safety, security and resilience to emergency), D12 (Fire safety), E1 (Offices), E2 (Providing suitable business space), E3 (Affordable Workspace), E9 (Retail, markets and hot food takeaways), HC1 (Heritage conservation and growth), SI 1 (Improving air quality), SI 2 (Minimising greenhouse gas emissions), SI 3 (Energy infrastructure), SI 4 (Managing heat risk), SI 5 (Water infrastructure), Policy SI 7 (Reducing waste and supporting the circular economy), SI 13 (Sustainable drainage), T4 (Assessing and mitigating transport impacts), T5 (Cycling), T6 (Car parking), T6.2 (Office parking), T6.3 (Retail parking), T6.5 (Non-residential disabled persons parking), T7 (Deliveries, servicing and construction)

- 5.4 National Planning Policy Framework 2021 (NPPF)
- 5.5 National Planning Practice Guidance 2014 (NPPG)
- 5.6 Future Wimbledon Supplementary Planning Document (SPD) (November 2020)
- 5.7 Merton's Tall Buildings Background Paper 2010

6. CONSULTATION

6.1 The application was originally publicised by means of a site notice and individual letters to occupiers of neighbouring properties. In response, 40 letters of objection were received. The letters of objection, which include objections from The Wimbledon Society, Wimbledon E Hillside Residents' Association (WEHRA), Raymond and Mansel Road Residents Association, Wimbledon Central Residents Association Ltd, and Friends of Wimbledon Town Centre, are on the following grounds:

- Loss of daylight/sunlight, inaccuracy of daylight/sunlight report/rights of light impact/No overshadowing report
- Loss of privacy/overlooking
- Excessive height/fails to relate to the height and massing of surrounding buildings/overbearing impact/visually intrusive
- Overdevelopment of site
- Existing tree located behind the site would not screen proposal/impact on conservation area
- Overconcentration of supermarkets in the locality
- Disruption due to noise, dust and debris and inconvenience to pedestrians during construction
- Lack of parking for shopping
- Proposed construction vehicles are of insufficient size to remove required debris during demolition phase
- Congestion from retail shoppers using on-street parking bays and disruption this will cause to pedestrian movement and bus stops. Increased parking pressure on surrounding residential streets
- Impact on safety
- Air quality/odour/noise

- Flood risk
- Employment numbers stated are not a NET gain
Adverse impact on the variety, vitality & viability of retail in Wimbledon's Primary Shopping Area, and other local centres
- Lack of prior notification
- Light pollution
- Lack of sustainability due to demolition of a modern building/no attempt to upgrade existing building/lack of future proofing/increase carbon emissions

6.2 Wimbledon Society

Object to the proposal due to lack of sustainability, excessive height, which does not relate to the height and massing of surrounding buildings, and loss of daylight/sunlight and privacy to neighbouring properties. Also object to the proposed servicing on safety grounds due to heavy goods vehicles entering and exiting the site from Worple Road.

6.3 Wimbledon E Hillside Residents' Association (WEHRA)

Object to the proposal on grounds of sustainability, flood risk, excessive height, moving forward of building line which will impact ability to 'green' the local area and make the pavement area more congested. Also object to the proposed servicing on safety grounds due to heavy goods vehicles entering and exiting the site from Worple Road, and lack of parking for staff or shoppers due to increased congestion and the pressure it will put on parking spaces in the surrounding road network.

6.4 Raymond and Mansel Road Residents Association (RAMRA)

Object to the proposal. The proposal is excessive in terms of height and bulk and would have a detrimental impact on properties located at the rear and at Wimbledon central, including loss of daylight/sunlight, overshadowing of rear gardens, loss of privacy, noise, and visual intrusion. Also object on grounds of lack of sustainability, flood risk and pollution.

6.5 Friends of Wimbledon Town Centre

Object to the proposal on the grounds of loss of daylight/sunlight, privacy, visual intrusion, excessive height, failure to respect the adjacent conservation area, and sustainability. The proposed building also does not relate to surrounding buildings, would have a negative impact on parking locally, and the proposed increase in heavy goods and customer vehicles will have a negative impact on highway safety.

6.6 Wimbledon Central Residents Association Ltd

Object to the proposal concerning loss of daylight/sunlight, rights to light infringement, loss of privacy, sustainability, and unnecessary demolition. There was also lack of any consultation prior to the submission of the application, which has resulted in significant inaccuracies in the application.

6.7 Following amendments to the proposal a further re-consultation was carried out. In response, a further 17 letters of objection, including a letter of objection from the Wimbledon Central Residents Association Ltd were received

reiterating a number of the concerns raised during the previous round of consultation.

6.8 Transport for London

The site of the proposed development is approximately 1km from the A238 Kingston Road which forms part of the Strategic Road Network (SRN). TfL has a duty under the Traffic Management Act 2004 to ensure that any development does not have an adverse impact on the SRN.

6.9 TfL supports the car-free nature of the proposal. It is noted that no Blue Badge (BB) parking is to be provided as part of the proposed development. It is recommended that a contribution is secured from the proposed development to convert an existing on-street parking space to a disabled person parking space should demand arise.

6.10 It is supported that 92 long-stay cycle parking spaces will be provided to the development in excess of London Plan minimum standards. However, the proposed provision of 16 short-stay cycle parking spaces is significantly below the minimum standards set out in Policy T5 of the London Plan where a minimum of 59 short-stay spaces is required and should therefore be provided. All cycle parking should be located in a secure, sheltered and accessible location, and should meet design standards set out in Chapter 8 of the London Cycle Design Standards (LCDS). At least 5% of the cycle parking spaces should be for larger and adapted cycles in line with Chapter 8 of the LCDS.

6.11 After reviewing the revised Deliveries and Servicing Plan (DSP), TfL has the following comments:

- a) TfL understands that servicing will be undertaken from the parking bays in front of the site, in which loading would only be permitted between the hours of 06:30-08:30 and 19:00-21:00. This is supported, subject to approval from Merton Council.
- b) It is understood that most servicing and delivery trips associated with the office will be undertaken via smaller vehicles, which would service the site on-street using existing/modified loading opportunities.
- c) It is supported that refuse collection associated with the office will be undertaken on-street as per the existing situation.
- d) It is understood that the site is expected to receive a net increase of 3 delivery and servicing activities per day, none of which will be in the network peak hours.
- e) It is welcomed that the store manager of the retail unit will liaise with occupiers of the offices to manage the arrival of deliveries and arrange deliveries outside peak periods.

- f) The store manager will encourage tenants to source deliveries from suppliers and operators registered with TfL's Freight Operators Recognition Scheme (FORS). This is also welcomed.
 - g) The DSP should be secured by condition.
- 6.12 It is understood that the Construction Logistics Plan (CLP) and the Travel Plan (TP) remain the same. TfL has no concerns with either the CLP or the TP, subject to approval from Merton Council and bus operations remaining unaffected during construction. The CLP and TP should be secured by condition.
- 6.13 All vehicles should only park/ stop at permitted locations and within the time periods permitted by existing on-street restrictions.
- 6.14 Council's Urban Design Officer (Comments made prior to amendments to the proposal)
- 6.15 Generally I am supportive of this proposal. The architecture is good quality and appropriately contextual, and the height is appropriate and generally in accordance with the Future Wimbledon SPD. It is highly glazed but the brick framing is sufficiently substantial and well detailed to give a good balance to the building. Materials are good quality.
- 6.16 I have two key issues with the proposal. One, is the glazed office entrance which goes the full height of the building, second is the design of the public realm.
- 6.17 The site is quite wide and the street frontage fragmented by service entrances either side. Adding this glazed element fragments the building form and clarity and presence of the building in the street scene. I do not feel it is essential to mark the office entrance in this way, and it adds a verticality that is too thin for the site.
- 6.18 The building is a facade in the street scene and does not need to articulate the corner as if it was on the corner of a street block. It would be appropriate for this element of the building to be incorporated into the brick and glass form of the remainder of the frontage, to make it four bays wide. This would give a better street presence and not emphasise its height too much. The recess on the north side works well. This and the access on the south side serve to provide clear breaks between the adjacent buildings. Currently the main frontage is beginning to get lost among the side gaps and the office glazing, and it shouldn't need to do this.
- 6.19 The building projects further than the existing facade but seems to line up better with the adjacent buildings. The front of the building is let down by a very fragmented approach to the public realm. It is far too fussy and, despite the quality materials, has a dated feel to the design. Separating blocks of pavers with soldier course brick is an example of this. The footway should read as a

single and clear space from building to kerb edge to maximise the sense of width. This will add presence to the building. It is recommended that the whole be re-laid in York stone pavers at a minimum size of 600x450mm in a traditional pattern.

- 6.20 There is also an odd recess/kink in the layout at the entrance to the residential car park. This should be designed out. It is good to see tight radii kerbs for the basement access but the footway needs to be on the level as it crosses it, with small format Yorkstone setts (for strength) to create a seamless and level pedestrian-friendly footway. This should also be proposed for the access to the residential parking (it is in the public highway). Otherwise the setting will be compromised and there will be little quid pro quo for the intensified site use.
- 6.21 Clutter in the footway needs to be kept to a minimum, but half the width is taken up by cycle parking. This needs to be made far more efficient by recessing the spaces into the glazed bays and positioning them at an angle. All other street clutter needs to be identified and rationalised in consultation with the council's highways team.
- 6.22 Council's Transport Planner
- 6.23 Proposed amendments to the delivery and servicing arrangement is acceptable on condition that servicing vehicles are a maximum of 12m in length.
- 6.24 Council's Highway Officer
- 6.25 No objections subject to appropriate conditions including submission of a detailed Construction Logistics Plan.
- 6.26 Council's Climate Change Officer
- 6.27 No objections subject to appropriate conditions and carbon offset financial contribution to be secured by S106 Legal Agreement.
- 6.28 Environmental Health (Noise and Nuisance)
No objections subject to appropriate conditions.
- 6.29 Environmental Health (Air Quality)
No objections subject to appropriate conditions including submission and approval of Construction Management Plan/ Dust Management Plan.
- 6.30 Metropolitan Police (Designing Out Crime Officer)
- 6.31 Have raised some concerns regarding bicycle store, office accommodation accessibility and circulation between the office and retail uses.
- 6.32 Council's Flood Risk Officer
- 6.33 No objections subject to appropriate conditions.

6.34 Thames Water

6.35 Raise no objections regarding impact on waste water, and water networks, sewerage treatment works, and water treatment infrastructure capacity. Have requested a condition requiring the submission of a piling method statement given the works are located within 15m of a strategic sewer.

7. PLANNING CONSIDERATIONS

7.1 Principle of Development

7.2 The Council supports the development of major offices in Wimbledon town centre, which is defined in Policy DM R1 of the Adopted Sites and Policies Plan (July 2014) as offices with more than 1,000sq.m of floorspace. Policy CS.7 of the Core Planning Strategy states that in Wimbledon Town Centre the Council will support high quality offices, especially major development. Policy DM E1 of the Adopted Sites and Policies Plan and Policies Maps (July 2014) states that proposals relating to employment sites will only be supported that (subject to Policy DM E2 and DM E3), retain existing employment land and floor space. The Council will support proposals for the redevelopment of vacant and underused existing employment land and floor space for employment use and proposals for large and major offices in town centres. Policy DM E1 notes that as Wimbledon town centre is tightly bound by residential areas, the possibilities for growth include increasing density on existing sites. This policy states that the council will work with landowners to meet market demand for high quality, well designed large floorplate offices commensurate with Wimbledon's status as a major centre and to take advantage of the internationally recognised Wimbledon 'brand'. The Future Wimbledon SPD (Para. 3.2.1) states that office development providing modern space with large floor plates is limited in the area with demand high and supply limited and as such rents and values are rising. Policy CS 6 of the Core Planning Strategy 2011, seeks to maintain and enhance the retail core of the town centre.

7.3 At a regional level, Policy GG5 of the London Plan (2021) states that to conserve and enhance London's global economic competitiveness and ensure that economic success is shared amongst all Londoners, those involved in planning and development must, among other things, promote the strength and potential of the wider city region, and plan for sufficient employment and industrial space in the right locations to support economic development and regeneration. Policy SD6 also recognises that town centres should be the focus for commercial development beyond the Central Activities Zone (CAZ), and important contributors to the local as well as London-wide economy. Policy E1 seeks to consolidate and, where viable, extend office provision in town centre locations. With regards to retail, Policy E.9 states that a successful, competitive and diverse retail sector, which promotes sustainable access to goods and services for all Londoners, should be supported in line with the wider objectives of this Plan, particularly for town centres.

7.4 At a national level, Paragraph 81 of the NPPF states that 'planning policies and decisions should help create the conditions in which businesses can invest,

expand and adapt. Significant weight should be placed on the need to support economic growth and productivity, taking into account both local business needs and wider opportunities for development'. Therefore significant weight should be placed on the need to support economic growth through the planning system.

7.5 The application site is located in Wimbledon Town centre and has excellent transport links (PTAL rating of 6b), which means it is a highly suitable location for a major commercial (retail/office) development. The proposal would result in a net increase of 1940sqm of GIA overall floorspace, and the applicant has stated in their planning statement that the proposal would generate between 360 – 420 additional jobs, which would help further stimulate the wider Wimbledon economy due to wages being spent locally. It is considered that although there would be a slight decrease in office space to accommodate the retail offering, this is acceptable as the proposed building would re-provide much higher quality office floorspace with well designed large floorplates. The proposal would also extend the retail offer further along this side of Worple Road, which combined with an active frontage would increase the vitality and viability of this part of the town centre.

7.6 Overall, it is considered that the proposal would comply with local, regional and national planning policies, providing a high quality commercial building that is commensurate with Wimbledon's status as a major centre. There is therefore policy support for the proposal in principle.

7.7 Design, Impact on Streetscene and Wider Conservation Area

7.8 Height, Bulk, Massing and Impact on Wider Setting

Policy D9 (Tall Buildings) of the London Plan states that tall buildings are generally those that are substantially taller than their surroundings and cause a significant change to the skyline. Policy D9 states that Boroughs should determine if there are locations where tall buildings may be an appropriate form of development, subject to meeting the other requirements of the Plan, and that any such locations and appropriate tall building heights should be identified on maps in Development Plans.

7.9 Policy D9 of the London Plan states that special attention should be paid to long-range and mid-range views – including the design of the top of the building and the form and proportions of the building. The building should make a positive contribution to the local townscape in terms of legibility, proportions and materiality and the existing and emerging skyline and not adversely affect local or strategic views.

7.10 More specific guidance is outlined in the Tall Buildings Background Paper (2010) which forms part of Merton's Local Development Framework, as an evidence base in support of the Design Policy outlined in the Core Strategy. This states that in Wimbledon Town Centre, tall buildings should contribute to creating a consistent scale of development based on a range of similar but not uniform building heights. These should be determined by reference to surrounding building heights and townscape characteristics.

- 7.11 The Future Wimbledon SPD (November 2020) gives guidance regarding the maximum building heights that would be acceptable on specific sites in Wimbledon Town Centre in terms of both floor count and measured height. The SPD advises that a building of up to eight storeys and a maximum height of 32m can be accommodated on this site. It is considered that given the proposed building would be eight storeys and have a maximum height of approx. 29.8m, that it would comply with the SPD.
- 7.12 The proposed building would be quite prominent from some short and medium range views from within the town centre, and part of the Merton (Wimbledon Hill Road) Conservation Area (mainly from the rear of properties located along Mansel Road). It should be noted that the proposal would not be visible from the main train station square, and important heritage assets such as the Grade II listed Wimbledon Town Hall. In terms of long-range views, it is considered that the proposal would not result in any adverse impact on any protected strategic or local views. The surrounding Wimbledon Town Centre skyline is projected for considerable change, with potential building heights between 8 - 12 storeys identified within the Future Wimbledon SPD on surrounding sites.
- 7.13 The applicant has also provided a number of short and mid-range verified views including looking along Worple Road. It is considered that these views demonstrate that the building is not excessive in terms of its height and size and responds well to the surrounding townscape. The proposed building has a well defined top, middle and base, with the top floor set back reducing its bulk and massing when viewed from the street. The rear of the building has also been reduced with the top two floors reduced in depth, which as a result reduces the mass of the building when viewed from the Conservation Area at the rear.
- 7.14 Design
It is considered that the proposed building would have a well defined top, middle and base, and its design approach is supported by officers. The architecture is high quality and appropriately contextual, whilst the brick framing is sufficiently substantial and well detailed to give a good balance to the building. The proposed materials which comprise buff brickwork, which wraps around the building corner and zinc (with red/copper tone), which is featured on the suspended block over the vehicle access and southwest facing side elevations, are good quality. It is also considered that the recess on the north side of the building works well, as this and the access on the south side serve to provide a clear break between the proposed building and adjacent buildings. The top floor is also set back from the front of the building to further reduce its bulk and massing when viewed from the street.
- 7.15 Heritage
Paragraph 197 of the NPPF states that, in determining proposals affecting heritage assets, account should be taken of: the desirability of sustaining and enhancing their significance; the positive contribution that they can make to sustainable communities and the desirability that new development should make a positive contribution to local character and distinctiveness. London Plan

Policy HC1 requires that development affecting heritage assets should conserve their significance by being sympathetic to their form, scale, materials and architectural detail and that the cumulative impacts from incremental change from development on heritage assets and their setting should be actively managed and that development proposals should avoid harm. Policy DM D4 of the Adopted Sites and Policies Plan and Policies Maps (July 2014) states that all development proposals associated with the borough's heritage assets or their setting will be expected to demonstrate, within a Heritage Statement, how the proposal conserves and where appropriate enhances the significance of the asset in terms of its individual architectural or historic interest and its setting. There are no designated or non-designated heritage assets within the site boundary, therefore no heritage assets will be directly affected through development on the site.

7.16 The Merton (Wimbledon Hill Road) Conservation Area abuts the rear of the application site, with the rear elevations and gardens of properties located on the south side of Mansel Road directly facing the proposal. Although the proposal would be visible from the public realm within the Conservation Area, this would be limited with views restricted to between properties along Mansel Road and from the east end of Worple Road and southern end of Wimbledon Hill Road, which are also in the Conservation Area. It is however considered that the proposal would have an acceptable impact on the Conservation Area, with the top floor set back from the front and rear to reduce its bulk and massing. It is considered that the building is a high quality design and the applicant has submitted a heritage statement and verified views demonstrating how the building will integrate well within the surrounding townscape and conserves the setting of these heritage assets.

7.17 Public Realm

The streetscape at the front of the building is largely formed of an eclectic mix of hard materials and paving types. Red block-work defines the property boundary, whilst beyond this, within the adoptable public pavement space, mixed quality buff paving slabs with block paving details are used. It is considered that the overall paving strategy creates a patchwork of materials and clutters the street. This leads to a poor streetscape experience for pedestrians. A simpler, decluttered pavement would provide a stronger front entrance experience for the new building and aid accessibility for users of the street.

7.18 The applicant has submitted a proposed landscape strategy showing that the Worple Road frontage (both private and public highway) will be paved in Yorkstone. The proposed public realm improvements are supported with the proposed paving modernising, simplifying and decluttering the streetscape. Please note that the proposed public realm improvement are indicative at this stage and subject to approval by the Council's Highways team. The proposed public realm works will however be secured via a S278 Legal Agreement, requiring them to be completed prior to first occupation of the building.

7.19 Residential Amenity

- 7.20 Policy DM D2 of the Adopted Sites and Policies Plan and Policies Maps (July 2014) states that proposals for development will be required to ensure provision of appropriate levels of sunlight and daylight, quality of living conditions, amenity space and privacy, to both proposed and adjoining buildings and gardens. Development should also protect new and existing development from visual intrusion.
- 7.21 Self-contained residential flats are located at Wimbledon Central (Nos. 21 – 33 Worple Road), which bounds the southwest of the site, whilst residential dwellings located on Mansel Road are located to the rear of the site (Nos 10 – 14 Bound the site). Wimbledon Central is a maximum of eight storeys with three six storey wings located at the rear. Commercial units are located at ground floor level with residential located above. The residential properties with windows and balconies/terraces on the northeast facing flank elevation that would be most impacted by the proposal as this element directly faces the proposed building.
- 7.22 The applicant has submitted a daylight and sunlight report which assesses the impact of the proposed development on its surroundings with regards to daylight and sunlight availability to habitable rooms. In this instance the Vertical Sky Component Test (VSC) and Daylight Distribution Test has been applied. The VSC is a measure of the amount of diffuse daylight reaching a window. The BRE advises that where daylight and sunlight is reduced by less than 20% the impact would be negligible, where the reduction is between 20 – 35% the impact is considered to be minor, 35 – 50% the impact is considered to be moderate, and more than 50% the impact is considered to be major. The Daylight Distribution Test is an indication of how good the distribution of daylight is in a room. The BRE guidelines recommends that for an existing room to receive adequate daylight distribution, 80% of the working plane should have a view of the sky.
- 7.22 Wimbledon Central (Nos. 21 – 33 Worple Road)
The VSC test demonstrates that 30 of the 80 windows tested would retain a minimum of 80% of their former value, which means the impact on these windows would be negligible, whilst a further 20 windows would retain between 20 – 35% of their former value, which means the impact would be minor. From the remaining 30 windows, 25 would retain 35 – 50% of their former value, which means there would be a moderate impact on these windows, whilst 5 would lose in excess of 50% of their former value, which means there would be a severe impact on these windows. With regards to the five windows where there would be a severe impact (50+% reduction), it appears that these windows either serve kitchen or balcony windows to the living room. It is considered that the impact on these windows is acceptable in this instance given the rooms which the balconies serve also appear to feature 2 further windows located in the rear elevation, which would not be impacted by the proposal, whilst kitchens given their use are not considered to require the same level of daylight/sunlight as a living room. With regards to the 25 windows where there is a moderate impact (35 – 50% reduction) and 20 windows where there would be a minor impact (20 -35% reduction), these windows also appear to serve similar rooms, with the addition of some bedrooms, which again are

considered not to have the same requirements as a living room given these are generally used at night. It should be noted that 10 windows where there is a moderate reduction in daylight/sunlight are located on a single conservatory at sixth floor level. The impact on this residential unit is considered acceptable given a conservatory is considered to be a secondary living area, with the front (southwest) facing elevation not impacted by the proposal.

- 7.23 A total of 44 rooms were assessed for daylight distribution. This demonstrated that 24 rooms will enjoy levels of daylight distribution that meet the BRE guidance, which includes all of the main living areas within the properties to the rear. Again, similar to the VSC results the rooms that didn't meet BRE guidelines were bedrooms, kitchens and bathrooms, which are not afforded the same level of protection as a living room. Overall, it is considered that given the VSC and daylight distribution results that on balance the impact on daylight/sunlight levels is considered acceptable.
- 7.24 It is considered that the proposal would have an acceptable impact on privacy levels with regards to Wimbledon Central. It should be noted that there are terraces proposed at levels 2, 6 and 7. The terrace at level 6 would be located on the northeast side of the building so will not overlook this building. The terrace at level 2 would be located on the southwest side of the building, whilst the terrace at level 7 is located at the front of the building. A condition will be attached requiring the terrace at level 2 is fully enclosed, and the southwest facing side of the terrace at level 7 is enclosed by a 1.8m high privacy screen to prevent any overlooking of residential units at Wimbledon Central. There is an approx. 20m gap between the rear element of the proposed building, and Wimbledon Central. This element is approximately the same distance to Wimbledon Central when compared to existing so there would be little increase in privacy loss compared to existing up to level 4. Levels 5 – 7 are taller than existing so a condition will be attached requiring windows in the southwest elevation are obscure glazed below 1.8m internal floor height to avoid any additional overlooking.
- 7.25 The proposed building is between approx. 3.7m and 10.1m taller than the existing building, which means it will be more prominent when viewed from Wimbledon Central. It is however considered the building would not be visually intrusive or overbearing given the approx. 20m gap between the rear element of the building above level 2, and the rear wing of Wimbledon Central. The front section of the proposed building would be located approx. 6.2m from the terrace of the level 6 residential unit at Wimbledon Central. It is considered that although this would block views to the northeast from this unit, it would have little impact on views to the southeast, which is considered acceptable as it is considered that it is not realistic or reasonable to expect uninterrupted views in three directions from a roof terrace in a town centre location. The proposed building is also considered to be a much higher quality design than existing, with high quality materials, so the building is an improvement in this respect.
- 7.26 Nos. 2 – 14 Mansel Road
The VSC test demonstrates that all windows tested at Nos. 2 – 14 Mansel Road would retain a minimum of 80% of their former value and as such the impact on

daylight/sunlight levels at these properties would be negligible. The daylight distribution test also demonstrated that all habitable rooms will enjoy daylight distribution that meets the BRE guidance. The proposal would therefore have an acceptable impact on daylight/sunlight levels at these properties.

- 7.27 It is also considered that the proposal would not be visually intrusive or result in an unacceptable level of privacy loss. Since the application was first submitted, the top two floors have been reduced in height, which means the rear of the building is only approx. 3.7m higher than the existing building, whilst the rear elevation has been pulled back a further 2m to an average of approx. 5.8m from the rear boundary compared to the existing approx. 7.7m. It is considered that although there would be some additional impact when viewed from the Mansel Road properties, it would not be excessive, and as such is considered acceptable. A condition will be attached requiring windows in the rear elevation are obscure glazed below 1.8m internal floor height to prevent any overlooking.

7.28 Parking and Traffic

- 7.29 Policy T6 of the London Plan states that Car-free development should be the starting point for all development proposals in places that are (or are planned to be) well connected by public transport. Car-free development has no general parking but disabled persons parking for Blue Badge holders should be provided as set out in Policy T6 .5 on Non-residential disabled persons parking. With regards to Office parking Policy T6.2 states that in well-connected parts of outer London, including town centres, in close proximity to stations and in Opportunity Areas, office developments are encouraged to be car-free. Adequate provision should also be made for efficient deliveries and servicing and emergency access. With regards to cycle parking, Policy T5 states that this should be designed and laid out in accordance with the guidance contained in the London Cycling Design Standards, and that development proposals should demonstrate how cycle parking facilities will cater for larger cycles, including adapted cycles for disabled people.

- 7.30 At a local level Policy CS.18 promotes active transport and encourages design that provides attractive, safe, covered cycle storage, cycle parking and other facilities (such as showers, bike cages and lockers). Policy CS.20 of the Core Planning Strategy states that the Council will require developers to demonstrate that their development will not adversely affect pedestrian and cycle movements, safety, the convenience of local residents or the quality of bus movement and/or facilities; on-street parking and traffic management. This is endorsed in Policies DM T1 and DM T3 of the 2014 Sites and Policies Plan. Developments should also incorporate adequate facilities for servicing to ensure loading and unloading activities do not have an adverse impact on the public highway.

- 7.31 The application site is well connected and has excellent public transport links (PTAL rating of 6b). The site is served by rail services from Wimbledon station and a number of bus routes run along Worple Road. The proposal does not include any car parking for employees (net loss of 68 spaces), and this is

considered acceptable given the sites highly accessible location. Given the site is within a Controlled Parking Zone (W1) and has a PTAL rating of 6b, the proposal is to be 'permit free' preventing future employees from obtaining parking permits. Policy CS.20 of the Core Planning Strategy states that the Council will support permit free developments in areas within CPZ's benefiting from good access to public transport (PTAL 4-6).

- 7.32 Policy T6.5 states that standards for non-residential disabled persons parking are based on a percentage of the total number of parking bays, however, all non-residential elements should still provide access to at least one on or off-street disabled persons parking bay. The development provides no Blue Badge (BB) parking however this is considered acceptable in this instance given the constraints of the site. It is however recommended that a financial contribution is secured to convert an existing on-street parking space to a disabled person parking space should demand arise in future.
- 7.33 The application as originally submitted proposed to service the development off-street. However, this was considered unacceptable by officers due to the difficulty and disruption of manoeuvring heavy goods vehicles into the site from Worple Road. After discussions with officers it is now proposed to service the site on-street. This would involve shared use of the three parking bays directly in front of the site (on the west side of the existing site access) with loading only being permitted between the hours of 7:00am-8:30am and 7:00pm-9:00pm (Mondays to Saturdays). Between the hours 8:30am-7:00pm (Mondays to Saturdays) and 10:00am-4:00pm (Sundays) the bays would be available for Pay and Display parking only (i.e. no loading); whilst overnight no restrictions would apply. The applicant has submitted a Transport Assessment that states the office use is likely to generate a similar number of servicing trips as existing, whilst the retail element would likely generate 3 daily trips, which means overall there would only be a small increase in servicing trips. The applicant has submitted an updated Servicing and Delivery Plan which includes swept path analysis that demonstrates that a 12m vehicle can safely enter and exit the proposed on-street parking area in forward gear. Servicing and delivery vehicles will be restricted to 12m in size and this will be secured by S106 Agreement.
- 7.34 The proposed building would provide 7330sqm of GIA floorspace comprising 2108sqm of retail (Use Class E(a)) at ground and first floor level, and 5069sqm of office (Use Class E(g)(i)) above. An additional 153sqm, which comprises some plant and basement access areas, would be shared space. This means the proposal should provide a total of 78 long stay cycle spaces and 57 short stay cycle spaces to comply with London Plan Policy T.5. The proposed development would comply with this policy, providing a total of 92 long stay cycle spaces at basement level, which are accessed using a power assisted wheel track. Although the proposed number of short stay cycle spaces (16) does not comply with policy, it is considered that the proposed provision is acceptable in this instance given the lack of available space outside the front of the building. However, given there is a shortfall, the applicant will be required to provide a financial contribution of £12,300 (41 x £300 per short stay cycle space) for short stay cycle provision in the local area. The cycle storage is also

secure, covered and other facilities such as showers and lockers are provided. A condition, will be attached requiring full details of the cycle parking proposed, which shall include a requirement that at least 5% of the cycle parking spaces are for larger and adapted cycles.

7.35 The Council's Transport and Highways officers have assessed the application and consider it acceptable subject to appropriate conditions.

7.36 **Sustainability and Energy**

7.37 The application includes an Energy Sustainability Statement indicating that the development is targeting a BREEAM rating of Excellent for the Offices and Very Good for the retail element which meets Merton's minimum requirements. The GLA's Guidance on preparing energy assessments and the new London Plan require all major non-domestic schemes to achieve at least a 15% improvement against Building Regulations through energy efficiency alone. Based on the energy statement provided, the proposed development achieves this target with a 20.74% improvement through energy efficiency alone. District heating networks have been discounted due to the lack of an existing or planned district heating network in the vicinity of the site. The applicant has confirmed that provisions for connection to future District Heating Schemes will be provided. This will be secured via condition.

7.38 A carbon offset financial contribution of £104,439, which unless agreed in writing should be paid upon commencement of the development. The Council's Climate Change Officer has assessed the application and has confirmed that the application would comply with policies on climate change and water usage. Appropriate conditions are recommended.

7.39 **Basement Construction and Flood Risk**

7.40 The existing building comprises a basement area, which extends up to the highway edge. The proposed building would feature a larger basement area, which extends to the rear of the site. With regards to flood risk, the applicant has provided a drainage report. The report includes an in-depth assessment of the risk of flooding and provides adequate mitigation measures including attenuation tanks, blue roof, and permeable paving, which will help prevent run-off rates exceeding capacity. The site lies within Flood Zone 1 wherein principle the proposed development (including basement) is acceptable. Thames Water and the Council's Flood Engineer have assess the proposal and raise no objections subject to conditions.

7.41 The submitted Basement Impact Assessment documents also demonstrate that the proposed basement works can be undertaken safely without adversely affecting the surrounding built and natural environment. A condition will be attached requiring the submission of detailed demolition, design and construction method statements.

7.42 **Trees**

Policy DM O2 of the Adopted Sites and Policies Plan and Policies Maps (July 2014) states that development will only be permitted if it will not damage or destroy any tree which has significant amenity value, is in a conservation area or is subject to a Tree Preservation Order (TPO). Policy G 7 of the London Plan states that development proposals should ensure that, wherever possible, existing trees of value are retained.

- 7.43 There are three trees located immediately to the rear of the site in the rear gardens of Mansel Road. One tree has been given a 'B' category rating whilst the other two have been given a 'C' category rating. The applicant has submitted an Arboricultural Implications Assessment and Tree Protection Plan, which demonstrates that the proposal would not have a detrimental impact on these trees on condition that the recommended tree protection measures are put in place during construction works.

7.44 Air Quality

- 7.45 The applicant has submitted an Air Quality Assessment (AQA) report which considers the potential impacts on local air quality associated with the construction and operation of the proposed development.

- 7.46 The proposed development has the potential to expose future users to elevated pollution levels and impact existing air quality in the vicinity of the site during operation. Dispersion modelling was therefore undertaken using ADMS-Roads in order to predict pollutant concentrations as a result of emissions from the local highway network. Results were subsequently verified using local monitoring data.

- 7.47 Impacts on NO₂ and PM₁₀ concentrations as a result of operational phase road vehicle exhaust emissions were predicted to be negligible at all sensitive receptor locations. The results of the dispersion modelling assessment indicated NO₂ concentrations were below the relevant 1-hour mean AQO at the proposed retail space.

- 7.48 The Proposed Development is considered to be air quality neutral in regard to both building emissions and transport emissions in line with the LPG Air Quality Neutral Consultation Draft. The assessment has defined appropriate mitigation measures to reduce the level of dust, which may temporarily occur during demolition and construction activities. The Council's Environmental Health Officer has assessed the application and raises no objections subject to conditions including submission of Construction Management Plan/ Dust Management Plan.

7.49 Fire Safety

- 7.50 The applicant has submitted a fire statement which includes the following:
- Assessment of the building's means of escape requirements, demonstrating adequate arrangements are in place to provide warning and evacuate the building safely.

- The internal fire spread requirements of the building, including structural fire protection, compartmentation and surface spread of flame;
- Assessment of external fire spread;
- Provision of adequate access and facilities for the fire service; and,
- That appropriate operational fire safety management arrangements will be in place.

8. ENVIRONMENTAL IMPACT ASSESSMENT

- 8.1 The application does not constitute Schedule 1 or Schedule 2 development. Accordingly, there are no requirements in terms of EIA submission.

9. LOCAL FINANCIAL CONSIDERATIONS

- 9.1 The proposal would result in a net gain in gross floor space and as such will be liable to pay a Community Infrastructure Levy (CIL). The funds will be spent on the Crossrail project, with the remainder spent on strategic infrastructure and neighbourhood projects.

10. CONCLUSION

- 10.1 No.19 Worple Road is located in Wimbledon Town centre and has excellent transport links (PTAL rating of 6b), which means it is a highly suitable location for a major commercial development. The proposal would provide an enlarged, modernised and highly sustainable commercial (office/retail) building with well designed large floorplates commensurate with Wimbledon's status as a major centre. It is considered that the proposal would respect its context in terms of its scale and massing, would be of a high quality design which contributes to local distinctiveness. New major office floorspace proposals are encouraged within Wimbledon Town Centre and the proposal would be compliant with policy. The high quality design is such that officers are satisfied that it would not be a visually harmful building and would be commensurate with the desires for intensification of development in the town centre as set out in the Future Wimbledon SPD. The impact on residential amenity, transport and highways, sustainability, and flood risk, is considered to be acceptable subject to the imposition of suitable conditions. It is therefore recommended that planning permission be granted subject to the conditions and heads of terms set out below.

RECOMMENDATION

GRANT PLANNING PERMISSION subject to the completion of a S106 agreement covering the following heads of terms:

- 1) S278 agreement to be entered into for public realm improvements
- 2) Carbon Offset Financial Contribution (£104,439)
- 3) Financial contribution for cycle parking in the local vicinity (£12,300)
- 4) Financial contribution for conversion of on-street parking bay to disabled bay (To be confirmed)
- 5) Permit free
- 6) Restriction of delivery and service vehicle size
- 7) Paying the Council's costs for monitoring the travel plan over five years (£2000)
- 8) Paying the Council's legal and professional costs in drafting, completing and monitoring the legal agreement.

And subject to the following conditions:

1. A.1 (Commencement of Development)
2. A.7 (Approved plans)
3. B.1 (External Materials to be Approved)
4. B.4 (Details of Surface Treatment)
5. B.5 (Details of wall/fences)
6. No external windows and doors shall be installed until detailed drawings at 1:20 scale of all external windows and doors, including materials, set back within the opening, finishes and method of opening have been submitted to and approved by the local planning authority. Only the approved details shall be used in the development hereby permitted.

Reason: To ensure a satisfactory standard of development in accordance with the following Development Plan policies for Merton: policies 7.5 and 7.6 of the London Plan 2016, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D1 and D2 of Merton's Sites and Policies Plan 2014.

7. Before the development hereby permitted is first occupied, the windows in the southwest and rear facing elevations at level 5 and above shall be glazed with obscured glass and shall be maintained as such thereafter.

Reason: To safeguard the amenities and privacy of the occupiers of adjoining properties and to comply with the following Development Plan policies for Merton: policies D3 and D4 of the London Plan 2021, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D2 and D3 of Merton's Sites and Policies Plan 2014.

8. The offices shall not be occupied until a scheme of details of screening of the balconies at levels 2 and 7 have been submitted for approval to the Local Planning Authority. The development shall not be occupied until the scheme has been approved and implemented in its approved form and those details shall thereafter be retained for use at all times from the date of first occupation.

Reason: To safeguard the amenities and privacy of the occupiers of adjoining properties and to comply with the following Development Plan policies for Merton: policies D3 and D4 of the London Plan 2021, policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D2 and D3 of Merton's Sites and Policies Plan 2014.

9. C.6 (Refuse and Recycling - Details to be submitted)
10. D.11 (Construction Times)
11. H.6 (Cycle Parking – Details to be Submitted)
12. H.8 (Travel Plan)
13. Development shall not commence until a working method statement relating to the demolition and construction phase has been submitted to and approved in writing by the Local Planning Authority to accommodate:
 - (i) Parking of vehicles of site workers and visitors;
 - (ii) Loading and unloading of plant and materials;
 - (iii) Storage of construction plant and materials;
 - (iv) Wheel cleaning facilities
 - (v) Control of dust, smell and other effluvia;
 - (vi) Control of surface water run-off.

No development shall be carried out except in full accordance with the approved method statement.

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies T4 and T7 of the London Plan 2021, policy CS20 of Merton's Core Planning Strategy 2011 and policy DM T2 of Merton's Sites and Policies Plan 2014.

14. The development shall comply with the approved Delivery and Servicing Plan (Ref: liwimb/1909057) dated 10th June 2022 (as amended by email received 26th July 2022). The approved measures shall be maintained, in accordance with the Plan, for the duration of the use, unless the prior written approval of the Local Planning Authority is obtained to any variation.

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies T4 and T7 of the London Plan 2021, policy CS20 of Merton's Core Planning Strategy 2011 and policies DM T2, T3 and T5 of Merton's Sites and Policies Plan 2014.

15. Prior to the commencement of the development hereby permitted, a Construction Logistics Plan shall be submitted to and approved in writing by the Local Planning Authority. The approved measures shall be implemented prior to the first occupation of the development hereby permitted and shall be so maintained for the duration of the use, unless the prior written approval of the Local Planning Authority is first obtained to any variation.

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies T4 and T7 of the London Plan 2021, policy CS20 of Merton's Core Planning Strategy 2011 and policy DM T2 of Merton's Sites and Policies Plan 2014.

16. Prior to the commencement of development, including demolition, a detailed Demolition and Construction Environmental Management Plan (DCEMP) shall be submitted to and approved in writing by the Local Planning Authority. The DCEMP shall include:

- a) An Air quality management plan that identifies the steps and procedures that will be implemented to minimise the creation and impact of dust and other air emissions resulting from the site preparation, demolition, and groundwork and construction phases of the development. To include continuous dust monitoring.

- b) Construction environmental management plan that identifies the steps and procedures that will be implemented to minimise the creation and impact of noise, vibration, dust and other air emissions resulting from the site preparation, demolition, and groundwork and construction phases of the development.

- c) The development shall not be implemented other than in accordance with the approved scheme, unless previously agreed in writing by the Local Planning Authority.

Reason: To ensure the development does not raise local environment impacts and pollution.

17. All Non-Road Mobile Machinery (NRMM) of net power of 37kW and up to and including 560kW used during the course of the demolition, site preparation and construction phases shall comply with the emission standards set out in chapter 7 of the GLA's supplementary planning guidance "Control of Dust and Emissions During Construction and Demolition" dated July 2014 (SPG), or subsequent guidance. Unless it complies with the standards set out in the SPG, no NRMM shall be on site, at any time, whether in use or not, without the prior written consent of the local planning authority. The developer shall keep an up to date list of all NRMM used during the demolition, site preparation and construction phases of the development on the online register at <https://nrmm.london/>

Reason: To manage and prevent further deterioration of existing low quality air across London in accordance with London Plan policies GG3 and SI1, and NPPF 181.

18. F.1 (Landscaping/Planting Scheme)

19. The building shall not be occupied until details of public realm improvement works have been submitted and approved by the Local Planning Authority. The works shall be carried out prior to first occupation of the approved building and shall be permanently retained thereafter.

Reason: To achieve a high quality public realm in accordance with the following Development Plan policies for Merton: policy CS14 of Merton's Core Planning Strategy 2011 and policies DM D1 and D2 of Merton's Sites and Policies Plan 2014.

20. The development hereby permitted shall incorporate security measures to minimise the risk of crime and to meet the specific security needs of the development in accordance with Secured by Design. Details of these measures shall be submitted to and approved in writing by the local planning authority prior to first occupation of the development and shall be implemented in accordance with the approved details prior to occupation.

Reason: In order to achieve the principles and objectives of Secured by Design to improve community safety and crime prevention in accordance with Policy: Chapters 01B & 01C Merton New Local Plan, Policy D11 London Plan, Section 17 Crime and Disorder Act 1988 and National Planning Policy Framework (NPPF).

21. Prior to occupation a Secured by Design final certificate or its equivalent from the South West Designing Out Crime office shall be submitted to and approved by the Local Planning Authority.

Reason: In order to achieve the principles and objectives of Secured by Design to provide a safer environment for future residents and visitors to the site and reduce the fear of crime in accordance with Policy: Chapters 01B & 01C Merton New Local Plan, Policy D11 London Plan, Section 17 Crime and Disorder Act 1988 and National Planning Policy Framework (NPPF).

22. No development shall occur until a site investigation is undertaken to consider the potential for contaminated-land, and then if necessary, a detailed remediation scheme described to make the site suitable for, intended use by removing unacceptable risks to sensitive receptors, is submitted to and approved by the Local Planning Authority prior to commencement of works.

Reason: To protect the health of future users of the site in accordance with policy DM EP4 of Merton's sites and policies plan 2014.

23. Prior to first occupation, the remediation shall be completed and a verification report, produced on completion of the remediation, shall be submitted to the approval of the LPA.

Reason: To protect the health of future users of the site in accordance with policy DM EP4 of Merton's sites and policies plan 2014.

24. The recommendations as specified in the Acoustics Consultants Ltd, Noise Assessment Report, Issue No.9, dated 19th July 2022 shall be implemented as a minimum standard. A post construction noise survey shall be conducted and remedial measures implemented should be submitted criteria fail to be achieved, first being agreed by the LPA.

Reason: To protect the health of future users of the site in accordance with policy DM EP2 of Merton's sites and policies plan 2014.

25. Loading and unloading is only permitted between the hours of 07:00-08:30 and 19:00-21:00 Mondays to Saturdays, and not on Sundays or Bank holidays.

Reason: To safeguard the amenities of the area and the occupiers of neighbouring properties and ensure compliance with the following Development Plan policies for Merton: policies D14 and T7 of the London Plan 2021 and policy DM EP2 of Merton's Sites and Polices Plan 2014.

26. Unless otherwise agreed in writing by the Local Planning Authority, no part of the development hereby approved shall be used or occupied until evidence demonstrating that the development has achieved CO2 reductions in accordance with those outlined in the energy statement (dated 19th July 2022), has been submitted to and acknowledged in writing by the Local Planning Authority.

Reason: To ensure that the development achieves a high standard of sustainability and makes efficient use of resources and to comply the following Development Plan policies for Merton: policy SI2 of the London Plan 2021 and policy CS15 of Merton's Core Planning Strategy 2011.

27. No development shall commence until the applicant submits to, and has secured written approval from, the Local Planning Authority evidence demonstrating that the development has been designed to enable connection of the site to an existing or future district heating network, in accordance with the Technical Standards of the London Heat Network Manual (2014).

Reason: To demonstrate that the site heat network has been designed to link all building uses on site (domestic and non-domestic), and to demonstrate that sufficient space has been allocated in the plant room for future connection to wider district heating, in accordance with London Plan policies SI2 and SI3.

28. In order to demonstrate compliance with the 'be seen' post-construction monitoring requirement of Policy SI 2 of the London Plan, the legal Owner shall at all times and all in all respects comply with the energy monitoring requirements set out in points a, b and c below. In the case of non-compliance the legal Owner shall upon written notice from the Local Planning Authority immediately take all steps reasonably required to remedy non-compliance.

- a) Prior to commencement of development hereby permitted, the Owner is required to submit to the GLA accurate and verified estimates of the 'be seen' energy performance indicators, as outlined in Chapter 3 'Planning stage' of the GLA 'Be seen' energy monitoring guidance document, for the consented development. This should be submitted to the GLA's monitoring portal in accordance with the 'Be seen' energy monitoring guidance.
- b) Once the as-built design has been completed (upon commencement of RIBA Stage 6) and prior to the building(s) being occupied (or handed over to a new legal owner, if applicable), the legal Owner is required to provide updated accurate and verified estimates of the 'be seen' energy performance indicators for each reportable unit of the development, as per the methodology outlined in Chapter 4 'As-built stage' of the GLA 'Be seen' energy monitoring guidance. All data and supporting evidence should be uploaded to the GLA's monitoring portal. The owner should also confirm that suitable monitoring devices have been installed and maintained for the monitoring of the in-use energy performance indicators, as outlined in Chapter 5 'In-use stage' of the GLA 'Be seen' energy monitoring guidance document.
- c) Upon completion of the first year of occupation following the end of the defects liability period (DLP) and for the following four years, the legal Owner is required to provide accurate and verified annual in-use energy performance data for all relevant indicators under each reportable unit of the development as per the methodology outlined in Chapter 5 'In-use stage' of the GLA 'Be seen' energy monitoring guidance document. All data and supporting evidence should be uploaded to the GLA's monitoring portal. This condition will be satisfied after the legal Owner has reported on all relevant indicators included in Chapter 5 'In-use stage' of the GLA 'Be Seen' energy monitoring guidance document for at least five years.

Reason: In order to ensure that actual operational energy performance is minimised and demonstrate compliance with the 'be seen' post-construction monitoring requirement of Policy SI 2 of the London Plan.

29. Unless otherwise agreed in writing by the Local Planning Authority, a Post-Construction Review Certificate issued by the Building Research Establishment or other equivalent assessors confirming that the offices and the retail unit have achieved a BREEAM rating of not less than the standards equivalent to 'Excellent' and 'Very Good' respectively shall be submitted to and acknowledged in writing by the Local Planning Authority within 6 months of first occupation.

Reason: To ensure that the development achieves a high standard of sustainability and makes efficient use of resources and to comply the following Development Plan policies for Merton: policy CS15 of Merton's Core Planning Strategy 2011.

30. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any Order revoking and re-enacting that Order with or without modification), floor level 2 and above of the development shall be used for office use (Use Class E(g)(i)) and for no other purpose, without planning permission first obtained from the Local Planning Authority.

Reason: To ensure that there is an adequate supply of suitable sites and premises in locations that optimise opportunities and co-locational advantages for offices and minimise negative effects on other users and to comply with the following Development Plan policies for Merton: Policy DM E2 of Merton's Sites and Policies Plan 2014 and policy CS12 of the Core Planning Strategy 2011.

31. No development shall commence prior to submission and approval by the Local Planning Authority of a Basement Construction Statement comprising the following:

- a) Ground Movement Analysis (Vertical and Horizontal) including any heave or settlement analysis, and Damage Category Assessment with detailed calculations.
- b) Site Specific Ground Investigation Report with borehole logs and an interpretative report with recommendations for the foundation type and design
- c) Approval in Principle in accordance with CG 300 (DMRB) signed off by LBM Highways.
- d) Detailed Demolition Method Statement submitted by the Contractor responsible for the demolition of the existing property.
- e) Detailed Construction Method Statement produced by the respective Contractors responsible for the sheet piling, excavation and construction of the permanent retaining wall. This shall be reviewed and agreed by the Structural Engineer designing the basement.
- f) Design calculations of the temporary works supporting the highway and adjoining properties to facilitate excavation.
- g) Detail design calculations of the permanent retaining wall retaining the highway has to be submitted. The calculations shall be carried out in accordance with Eurocodes. We recommend assuming full hydrostatic pressure to ground level and using a highway surcharge of 20 KN/m² for the design of the retaining wall supporting the highway.
- h) Temporary retaining wall drawings such as pile plan, section and construction sequence.
- i) Permanent retaining wall drawings including plan, section and construction sequence.

j) Movement monitoring report produced by specialist surveyors appointed to install monitoring gauges to detect any movement of the highway/neighbouring properties from start to completion of the project works. The report should include the proposed locations of the horizontal and vertical movement monitoring, frequency of monitoring, trigger levels, and the actions required for different trigger alarms.

32. No piling shall take place until a Piling Method Statement (detailing the depth and type of piling to be undertaken and the methodology by which such piling will be carried out, including measures to prevent and minimise the potential for damage to subsurface sewerage infrastructure, and the programme for the works) has been submitted to and approved in writing by the local planning authority in consultation with Thames Water. Any piling must be undertaken in accordance with the terms of the approved piling method statement

Reason: The proposed works will be in close proximity to underground sewerage utility infrastructure. Piling has the potential to significantly impact / cause failure of local underground sewerage utility infrastructure.

33. Prior to the commencement of the construction phase (excluding demolition), detailed plans and sections demonstrating that the threshold levels for all access points (including basements) are set at a minimum of 300mm above the existing road levels, shall be submitted to and approved in writing by the local planning authority.

Reason: To reduce the risk of surface and foul water flooding to the proposed development and future users, and ensure surface water and foul flood risk does not increase offsite in accordance with Merton's policies CS16, DMF2 and the London Plan policy SI 13.

34. Prior to the commencement of the construction phase (excluding demolition), a detailed scheme for the provision of surface and foul water drainage shall be submitted to and approved in writing by the local planning authority for the development. The drainage scheme will dispose of surface water by means of a sustainable drainage system (SuDS) based on the 100yr plus 40% climate change event, at the agreed runoff rate (no more than 2l/s) in accordance with drainage hierarchy contained within the London Plan Policy (SI 13 and SPG) and the advice contained within the National SuDS Standards. A maintenance plan should also be included to ensure that they last the lifetime of the development.

Reason: To reduce the risk of surface and foul water flooding to the proposed development and future users, and ensure surface water and foul flood risk does not increase offsite in accordance with Merton's policies CS16, DMF2 and the London Plan policy SI 13

35. Condition: Prior to the commencement of the construction phase (excluding demolition), the applicant shall submit a detailed proposal on how drainage and groundwater will be managed and mitigated during and post construction (permanent phase), for example through the implementation of passive

drainage measures around the basement structure, waterproofing and drainage.

Reason: To reduce the risk of surface and foul water flooding to the proposed development and future users and ensure surface water and foul flood risk does not increase offsite in accordance with Merton's policies CS16, DMF2 and the London Plan policy SI 13.

36. Condition: Prior to the commencement of the construction phase (excluding demolition), the detailed design and specification for the blue roofs shall be submitted to and approved in writing by the Local Planning Authority. The design shall be carried out as approved, retained and maintained by the applicant in perpetuity thereafter.

Reason: To reduce the risk of surface and foul water flooding to the proposed development and future users and ensure surface water and foul flood risk does not increase offsite in accordance with Merton's policies CS16, DMF2 and the London Plan policy SI 13.

37. The retail use (Use Class E(a)) hereby permitted shall operate only between the hours of 0700 to 2300 Monday to Saturday, and 1000 to 1700 on Sunday and Bank Holidays.

Reason: To safeguard the amenities of surrounding area and to ensure compliance with the following Development Plan policies for Merton: policies D4 and D14 of the London Plan 2021, policy CS7 of Merton's Core Planning Strategy 2011 and policy DM EP2 of Merton's Sites and Policies Plan 2014.

38. No development [including demolition] pursuant to this consent shall commence until the tree protection measures set out in TREE SURVEY, ARBORICULTURAL IMPACT ASSESSMENT & TREE PROTECTION PLAN Rev:3 dated 24th August 2021 have been implemented on site and in accordance with the recommendations and guidance set out in BS 5837:2012. The details and measures as approved shall be retained and maintained, until the completion of all site operations.

Reason: To protect and safeguard the existing retained trees in accordance with the following Development Plan policies for Merton: policy G7 of the London Plan 2021, policy CS13 of Merton's Core Planning Strategy 2011 and policy O2 of Merton's Sites and Policies Plan 2014.

39. Site supervision: The requirements of condition 38 shall include the retention of an arboricultural expert to supervise, monitor and report to the LPA not less than monthly the status of all tree works and tree protection measures throughout the course of the demolition and construction period. At the conclusion of the construction period the arboricultural expert shall submit to the LPA a satisfactory completion statement to demonstrate compliance with the approved protection measures.

Reason: To protect and safeguard the existing retained trees in accordance with the following Development Plan policies for Merton: policy G7 of the London Plan 2021, policy CS13 of Merton's Core Planning Strategy 2011 and policy DMO2 of Merton's Sites and Policies Plan 2014.

40. Details of the proposed external plant, ventilation and extraction equipment shall be submitted to and approved in writing by the Local Planning Authority prior to occupation of the building hereby permitted. The development shall be carried out in accordance with the approved details”.

Reason: To safeguard the amenities of surrounding area and to ensure compliance with the following Development Plan policies for Merton: policies D4 and D14 of the London Plan 2021, policy CS7 of Merton's Core Planning Strategy 2011 and policy DM EP2 of Merton's Sites and Policies Plan 2014.

41. INFORMATIVE: Carbon emissions evidence requirements for Post Construction stage assessments must provide:
- Detailed documentary evidence confirming the Target Emission Rate (TER), Building Emission Rate (BER) and percentage improvement of BER over TER based on 'As Built' BRUKL outputs and bespoke model outputs; AND
 - A copy of the Building Regulations Output Document from the approved software and the bespoke modelling outputs based on the agreed bespoke modelling methodology. The output documents must be based on the 'as built' stage of analysis and must account for any changes to the specification during construction.
- AND, where the developer has used SAP 10 conversion factors:
- The completed Carbon Emissions Reporting Spreadsheet based on the 'As Built' BRUKL outputs.
- AND, where applicable:
- MCS certificates and photos of all installed renewable technologies.
42. INF9 (Works on the Public Highway)
43. INF12 (Works affecting the public highway)
44. INFORMATIVE: Thames Water will aim to provide customers with a minimum pressure of 10m head (approx. 1 bar) and a flow rate of 9m/minute at the point where it leaves Thames water pipes. The developer should take account of this minimum pressure in the design of the proposed development.
45. INFORMATIVE: No surface water runoff should discharge onto the public highway including the public footway or highway. When it is proposed to connect to a public sewer, the site drainage should be separate and combined at the final manhole nearest the boundary. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer

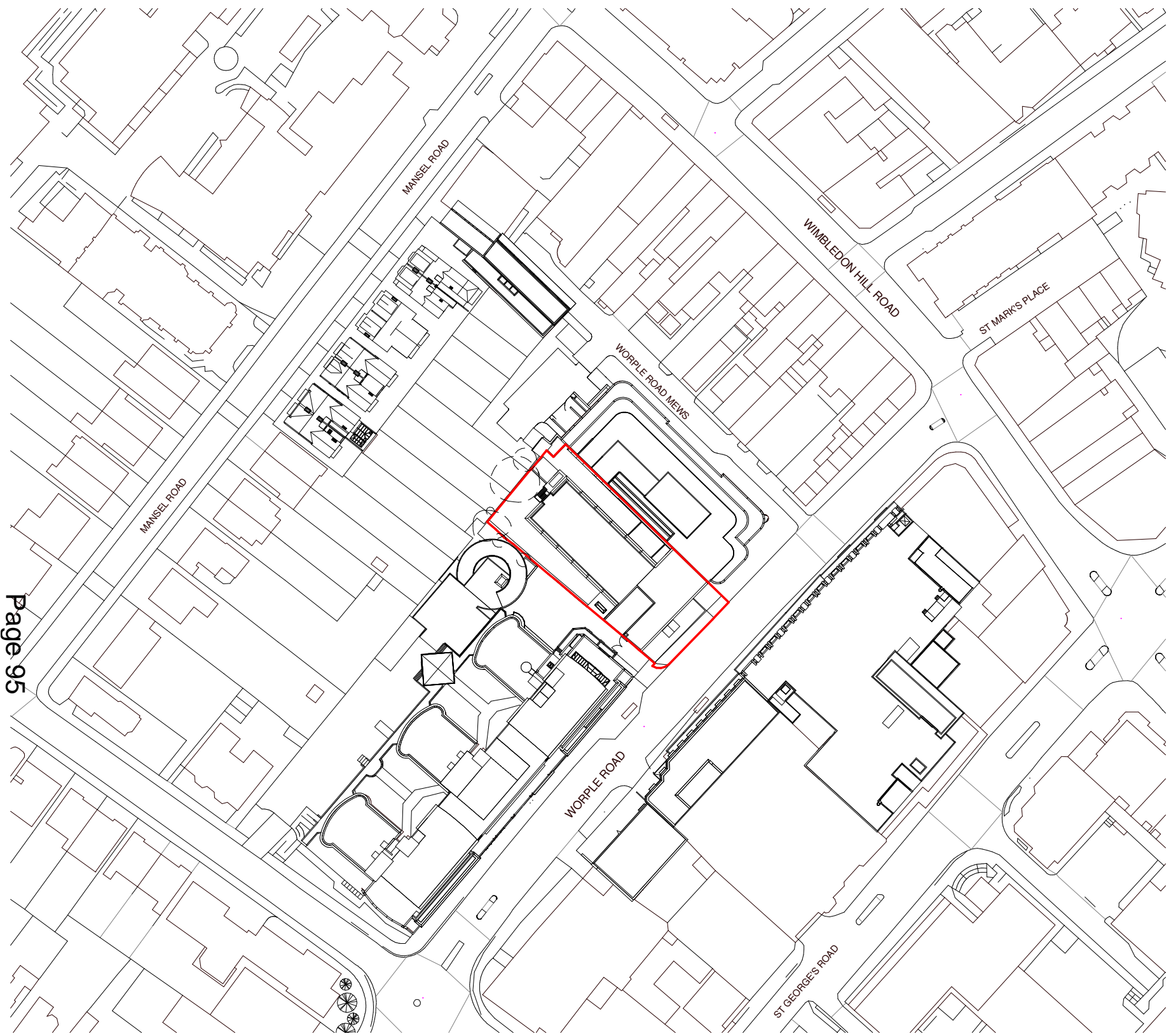
Services will be required (contact no. 0845 850 2777). No waste material, including concrete, mortar, grout, plaster, fats, oils and chemicals shall be washed down on the highway or disposed of into the highway drainage system.

NORTHGATE SE GIS Print Template



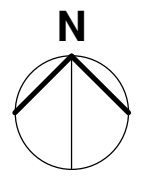
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Revision	Description	Author	Checker	Date
P1	Planning Submission	LC	PB	17.11.20



Application Boundary

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Site Location Plan

1 : 1250

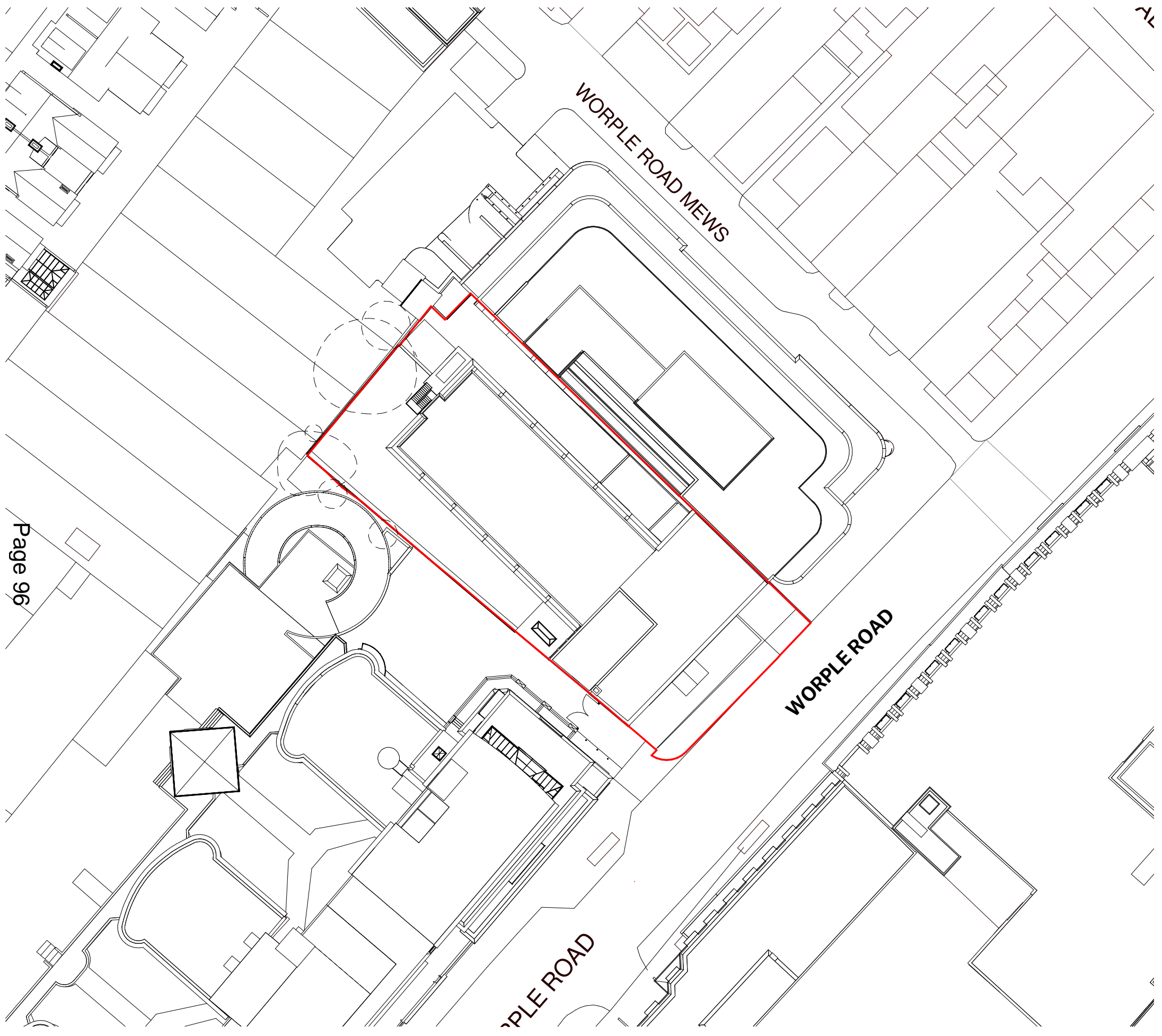
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Project				
07758 - 19 Worple Road				
Drawing Title				
Site Location Plan				
Project No.	Drawing No.	Status	Revision	Scale at A3
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File Reference: 07758-SPACE-ZZ-XX-DR-A-91-1001				

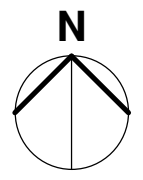


Revision	Description	Author	Checker	Date
P1	Planning Submission	LC	PB	17.11.20



 Application Boundary

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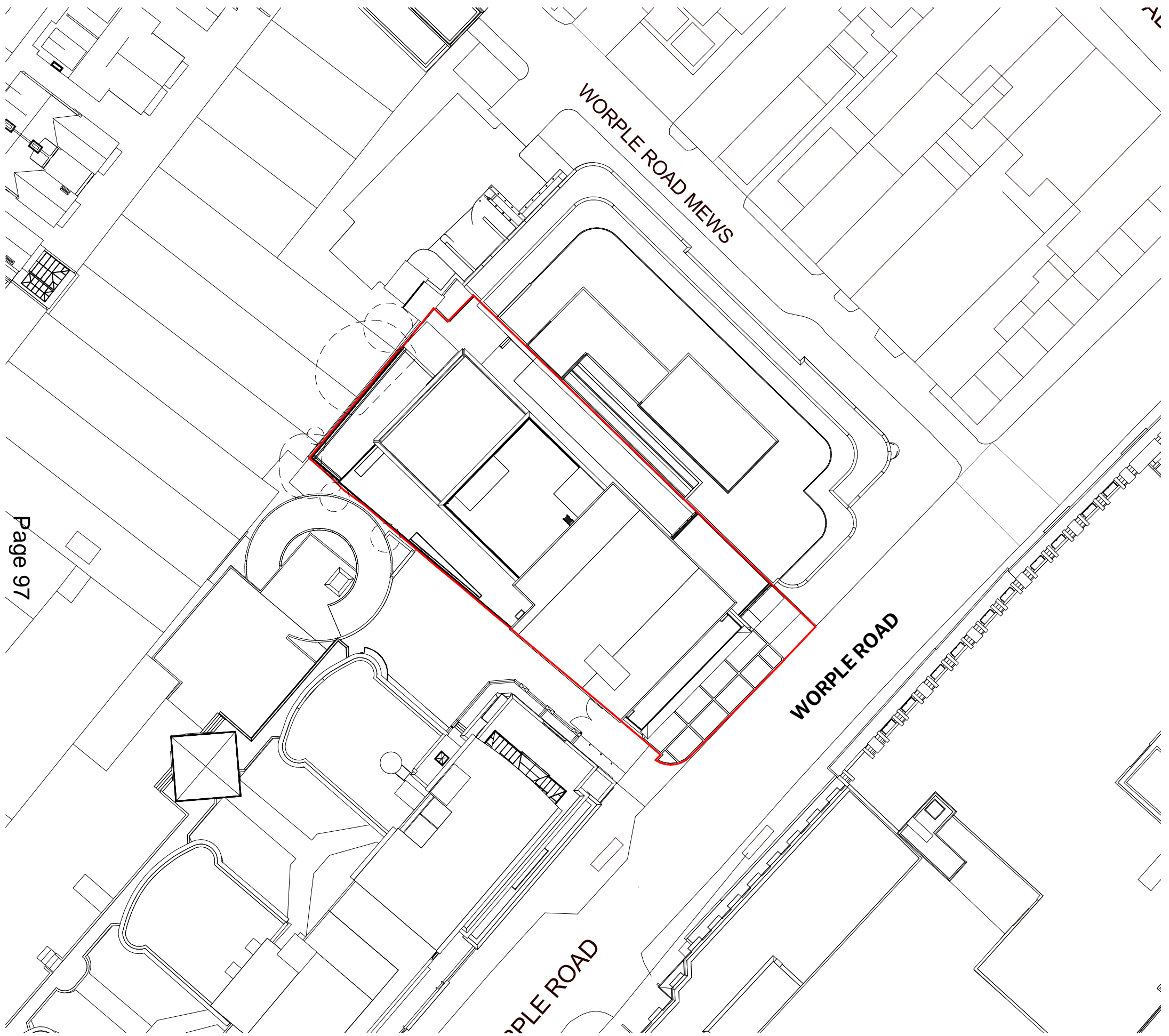
Site Plan - Existing

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07758 - 19 Worple Road				
Drawing Title				
Site Plan - Existing				
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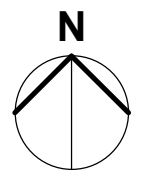
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Revision	Description	Author	Checker	Date
P1	First issue, issued for Board review	LC	PB	02.07.20
P2	Red line boundary amended	LC	PB	15.07.20
P3	Red line boundary amended to include pavement	LC	PB	18.08.20
P4	Drawing amended to highlight extent of ownership	LC	PB	26.08.20
P5	Design freeze issue	LC	PB	15.09.20
P6	Draft Planning Submission	LC	PB	23.09.20
P7	Planning Submission	LC	PB	17.11.20
P8	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21

Application Boundary

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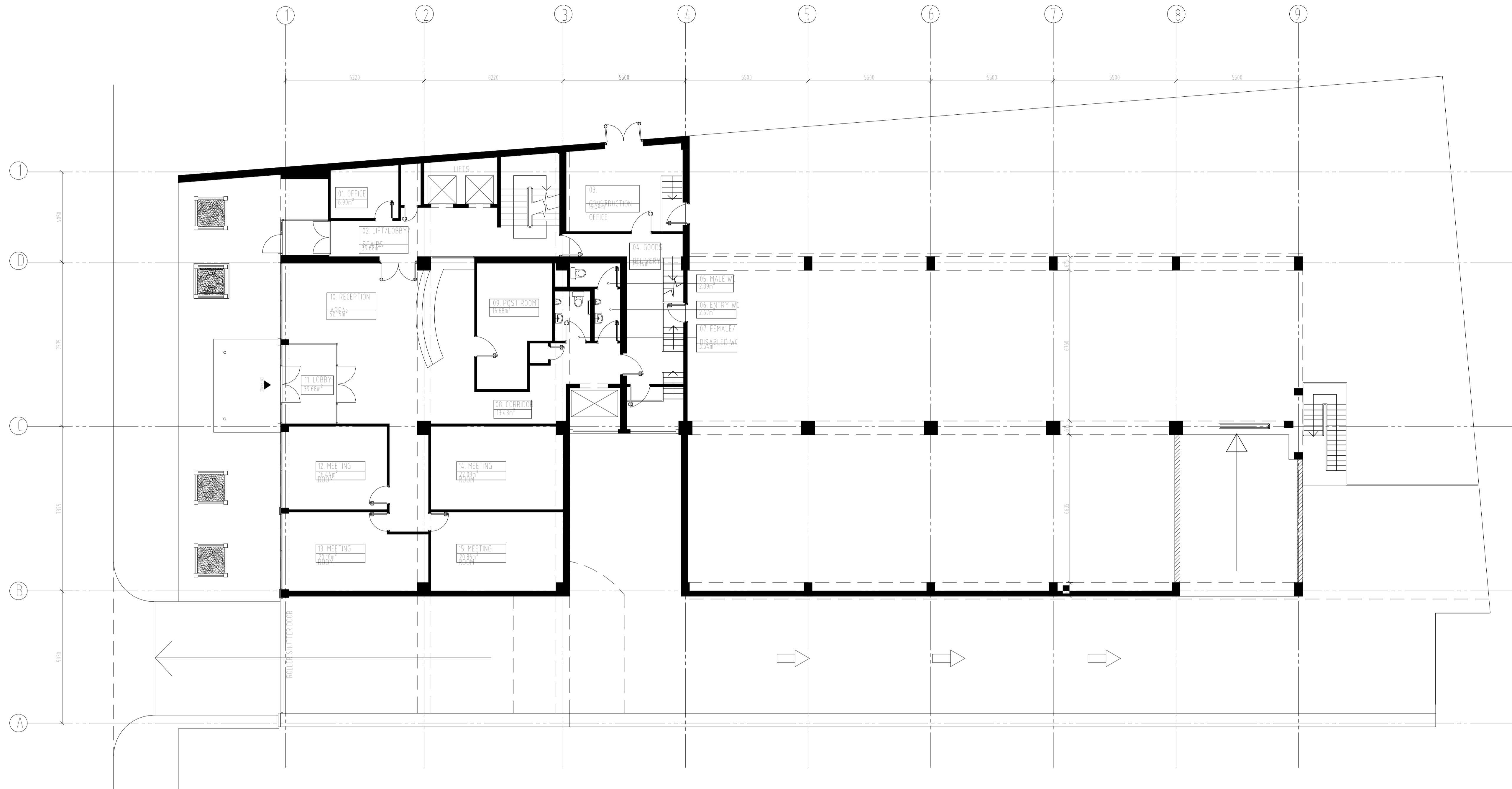
Site Plan - Proposed

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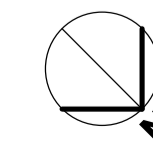
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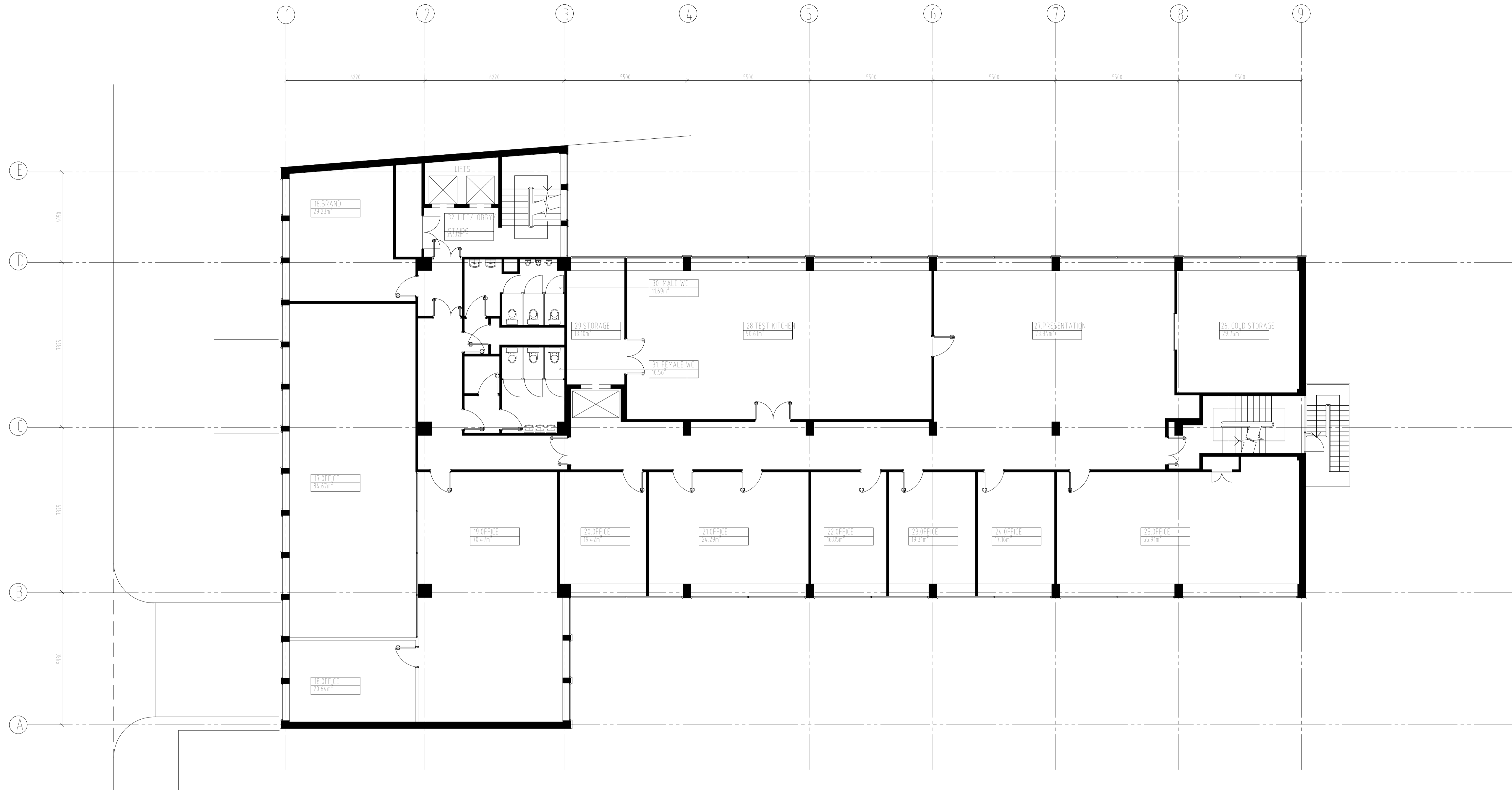
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Project 07758 - 19 Worple Road				
Drawing Title General Arrangement Plan - Existing - Level 00				
Project No. 07758	Drawing No. ZZ-GF-DR-A-01-0051	Status P1	Revision P1	Scale at A1 1:100
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Revision	Description	Author	Checker	Date
P1	Planning Submission	LC	PB	17.11.20

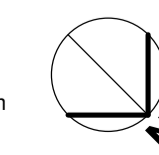


GA Plan - Existing - Level 01
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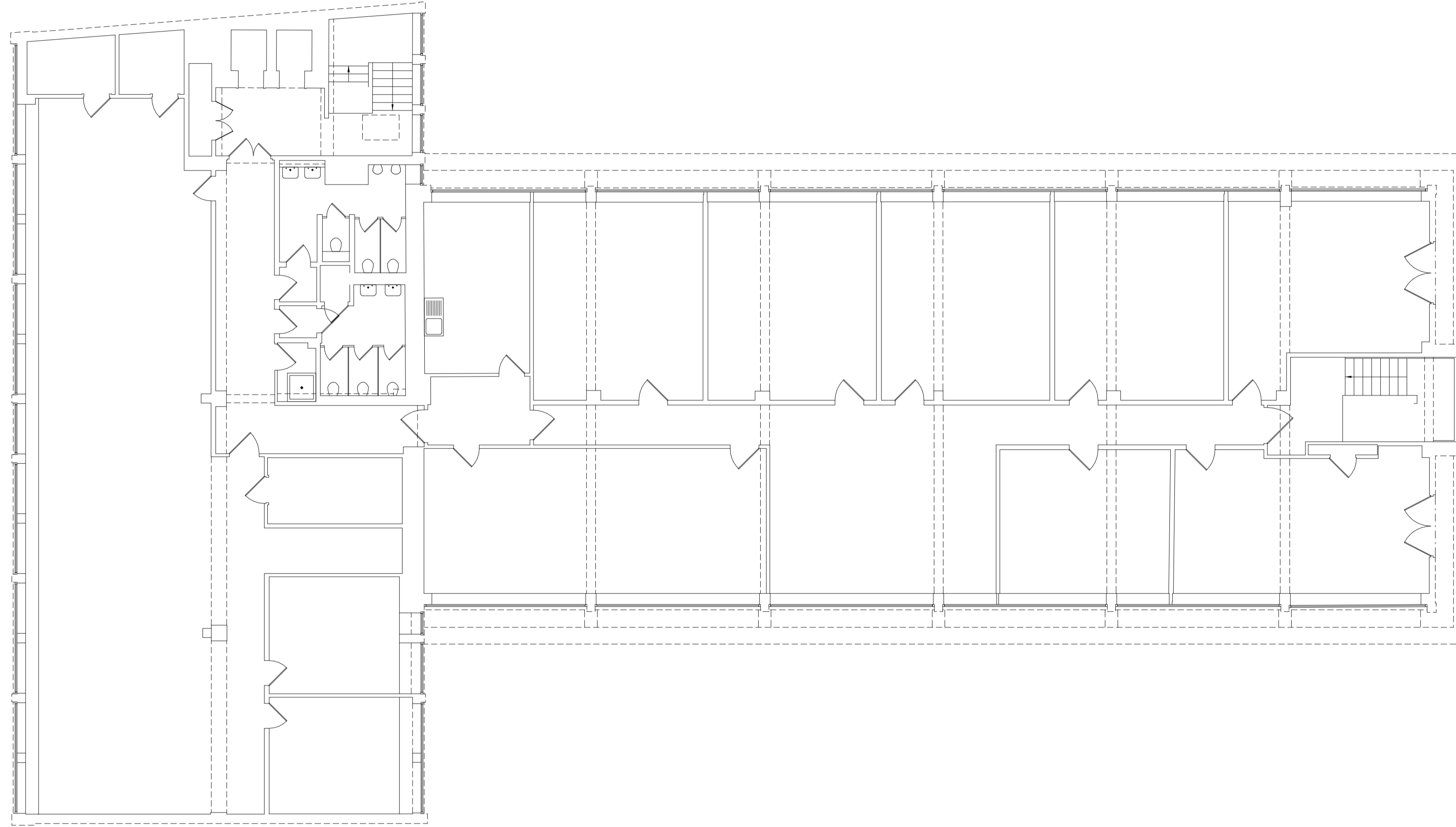
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Revision	Description	Author	Checker	Date
P1	Planning Submission	LC	PB	17.11.20



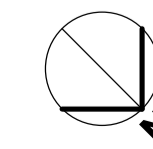
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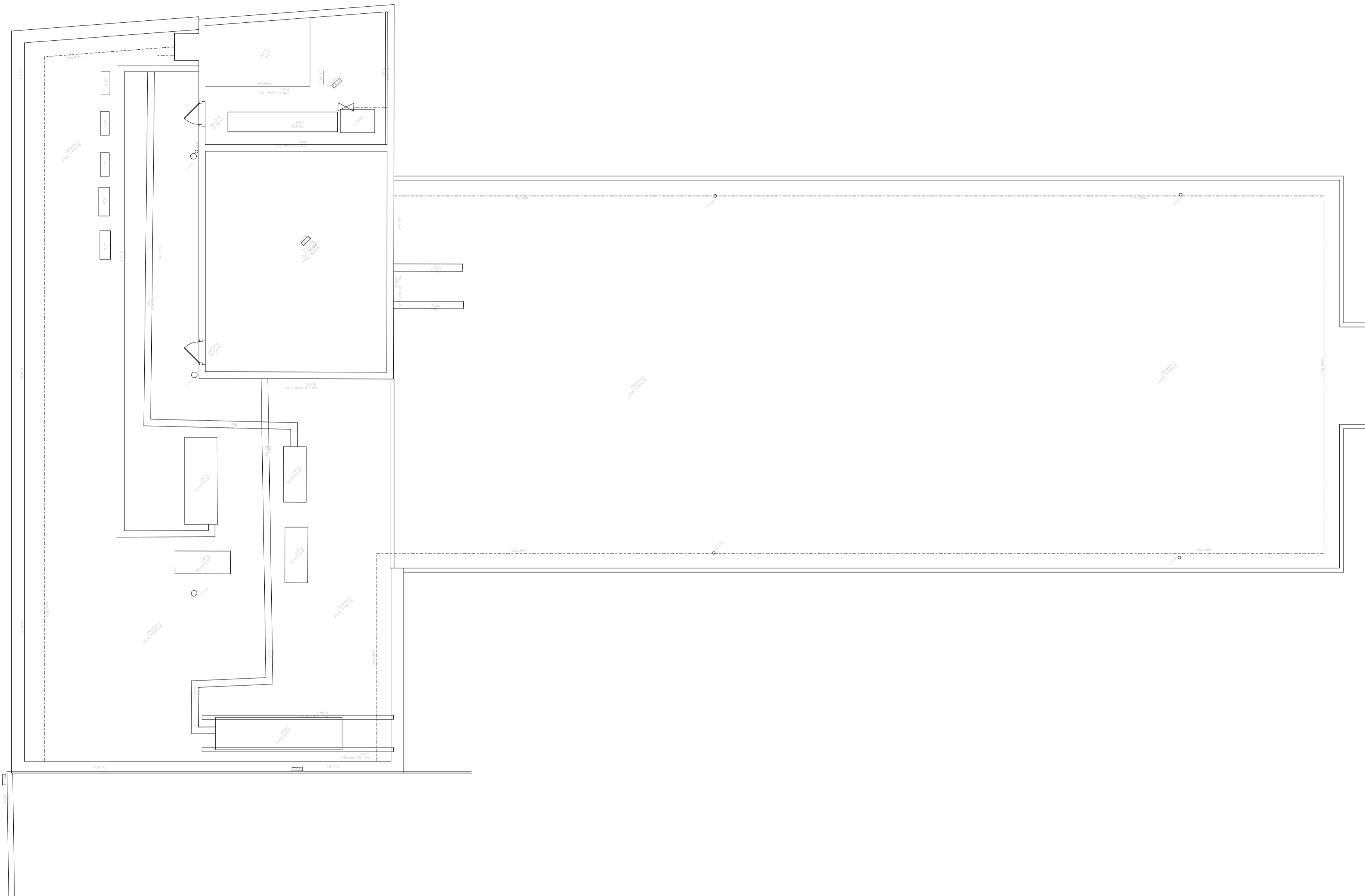
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Revision	Description	Author	Checker	Date
P1	Planning Submission	LC	PB	17.11.20



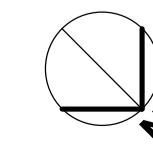
GA Plan - Existing - Roof Plan

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Project 07758 - 19 Worple Road				
Drawing Title General Arrangement Plan - Existing - Roof Plan				
Project No. 07758	Drawing No. ZZ-RF-DR-A-01-0051	Status P1	Revision P1	Scale at A1 1:100
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Revision	Description	Author	Checker	Date
P1	Planning Submission	LC	PB	17.11.20

Revision	Description	Author	Checker	Date
P1	First issue of planning drawings, structural grid amended, ancillary accommodation/core developed	LC	PB	29.06.20
P2	General design development, issued for Board review	LC	PB	02.07.20
P3	Red line boundary amended	LC	PB	15.07.20
P4	Design freeze issue	LC	PB	07.08.20
P5	Final draft for comment prior to submission	LC	PB	26.08.20
P6	General amendments made	LC	PB	15.09.20
P7	Draft Planning Submission	LC	PB	23.09.20
P8	Travellator repositioned, revised service and cycle access, revised escape strategy and basement layout	LC	PB	11.11.20
P9	Planning Submission	LC	PB	17.11.20
P10	Planning Submission - amended following further client comments prior to submission	LC	PB	07.01.21
P11	Layout revised to suit natural smoke ventilation strategy.	AW	DW	02.01.21
P12	Layout revised following consultation with client, consultants and specialist manufacturers.	AW	DW	08.03.21
P13	GA Plan general revision including Lidl store layout amendments and changes to core area of offices.	GCJ	DW	19.03.21
P14	Internal door and room references added.	AW	GCJ	13.05.21
P15	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P16	Reissued for Planning	GCJ	DW	19.08.21
P17	Revised following client comments received 06.09.21	AW	DW	14.09.21
P18	Paving to main entrance. Refer to Landscape proposals	AW	DW	16.09.21



- External levels to rear as existing
- External retail plant
- Existing sub station
- Escape from office accommodation above. Door swing beyond 90 degrees for escape route.
- Full Sluice
- Lodge lifts (national spec)
- 2400mm Clear Sectional Door
- Glazed automatic door to cycle entrance
- Armo barriers to edge
- Power assisted wheel track
- Store travellator. Min 3300mm clear width in structure required.
- Edge protection bollards
- Line of security gate and cycle access.

General Arrangement
 Note:
 Coordination with structural, mechanical and electrical consultants ongoing.
 Floor areas subject to location of structural walls and columns - To be confirmed.
 Previous Planning Issue Rev P1



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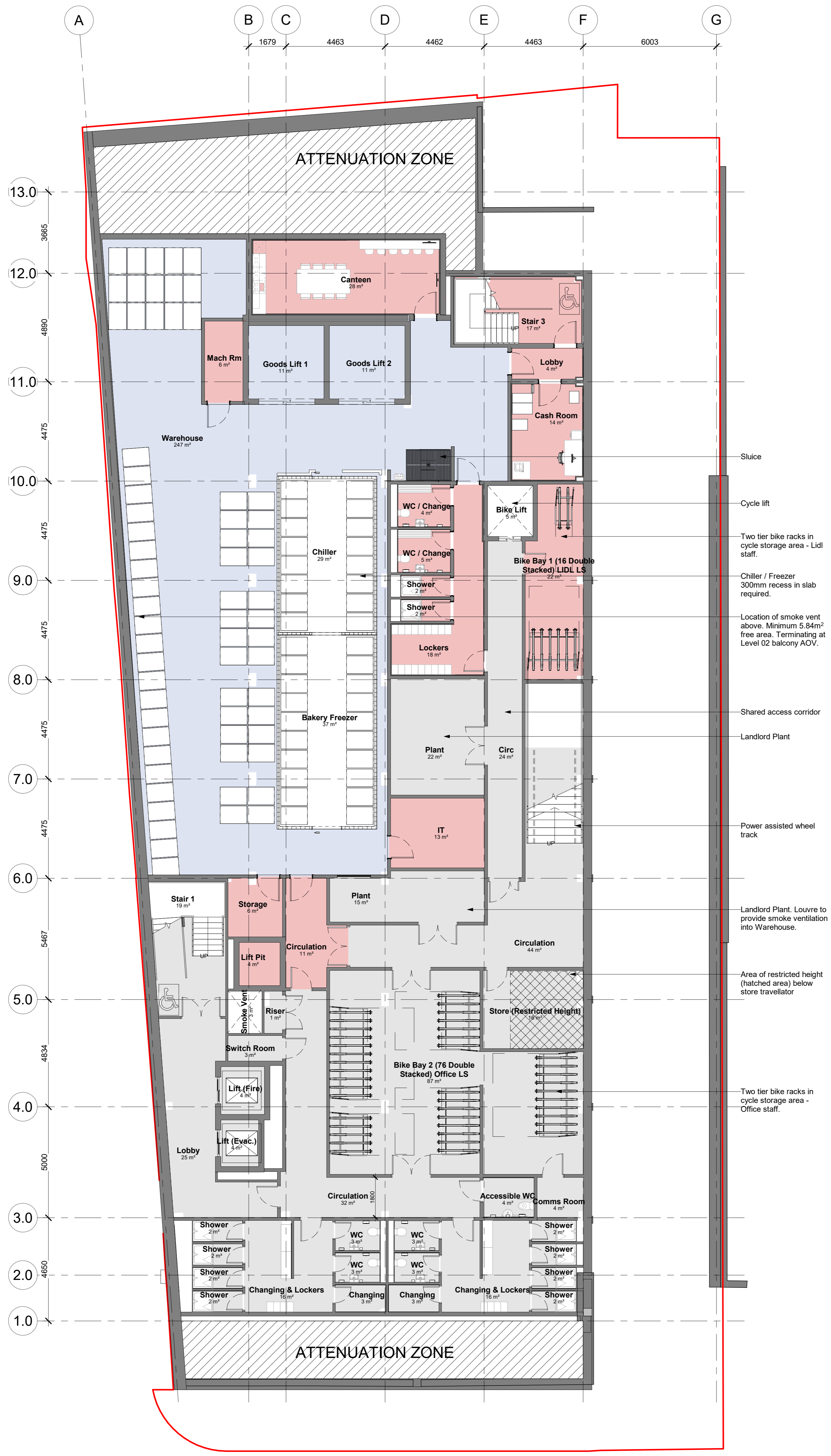
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Project No. 07758	Drawing No. ZZ-GF-DR-A-01-0001	Status S3	Revision P18	Scale at A1 1:100
File Reference: 07758-SPACE-ZZ-GF-DR-A-01-0001				

WORPLE ROAD



Revision	Description	Author	Checker	Date
P1	First issue of planning drawings, structural grid amended, ancillary accommodation/core developed	LC	PB	29.06.20
P2	General design development, issued for Board review	LC	PB	02.07.20
P3	Red line boundary amended	LC	PB	15.07.20
P4	Design freeze issue	LC	PB	07.08.20
P5	Final draft for comment prior to submission	LC	PB	26.08.20
P6	General amendments made	LC	PB	15.09.20
P7	Draft Planning Submission	LC	PB	23.09.20
P8	Travellator repositioned, revised service and cycle access, revised escape strategy and basement layout	LC	PB	11.11.20
P9	Planning Submission	LC	PB	17.11.20
P10	Layout revised to suit natural smoke ventilation strategy, as confirmed by DDA 12.05.21.	AW	DW	02.01.21
P11	Layout revised following consultation with client, consultants and specialist manufacturers.	AW	DW	08.03.21
P12	GA Plan general revision including Lidl store layout amendments and changes to core area of offices.	GCJ	DW	19.03.21
P13	Internal door and room references added. Plant B-027 to be landlord space, as confirmed by DDA 12.05.21.	AW	GCJ	13.05.21
P14	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P15	Revised for Planning	GCJ	DW	19.08.21
P16	Revised following client comments received 06.09.21	AW	DW	14.09.21



Sluice

Cycle lift

Two tier bike racks in cycle storage area - Lidl staff.

Chiller / Freezer 300mm recess in slab required.

Location of smoke vent above. Minimum 5.84m² free area. Terminating at Level 02 balcony AOV.

Shared access corridor

Landlord Plant

Power assisted wheel track

Landlord Plant. Louvre to provide smoke ventilation into Warehouse.

Area of restricted height (hatched area) below store travellator

Two tier bike racks in cycle storage area - Office staff.

Retail Bike Numbers
Sales 1169m2
Long Stay 1169/175 = 7
Short Stay (750/20) + (419/150) = 41
Actual

Long Stay 16
Short Stay 8

Office Bike Numbers
GIA 3882m2
Long Stay 1 per 75m² = 52
Short Stay 1 per 500m² = 8
Actual

Long Stay 76
Short Stay 8

General Arrangement

Note:
Coordination with structural, mechanical and electrical consultants ongoing.
Floor areas subject to location of structural walls and columns - To be confirmed.
Previous Planning Issue Rev P1

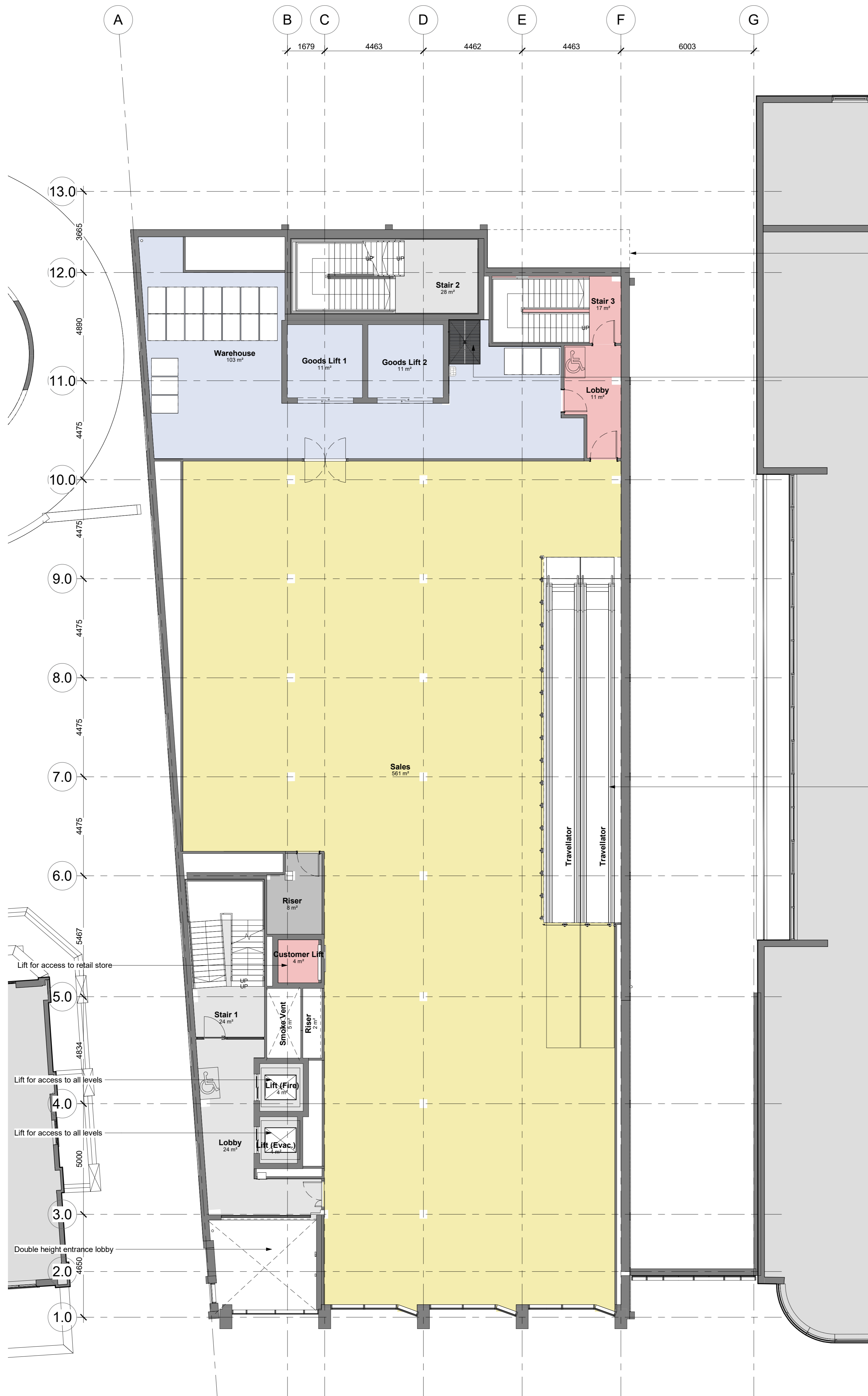
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Project 07758 - 19 Worple Road				
Drawing Title General Arrangement Plan - Proposed - Level -01				
Project No. 07758	Drawing No. ZZ-B1-DR-A-01-0001	Status S3	Revision P16	Scale at A1 1:100
File Reference: 07758-SPACE-ZZ-B1-DR-A-01-0001				



Revision	Description	Author	Checker	Date
P1	First issue of planning drawings, structural grid amended, ancillary accommodation/core developed	LC	PB	29.06.20
P2	General design development, issued for Board review	LC	PB	02.07.20
P3	Design freeze issue	LC	PB	07.08.20
P4	Final draft for comment prior to submission	LC	PB	26.08.20
P5	General amendments made	LC	PB	15.09.20
P6	Draft Planning Submission	LC	PB	23.09.20
P7	Travellator repositioned, revised service and cycle access, revised escape strategy and basement layout	LC	PB	11.11.20
P8	Planning Submission	LC	PB	17.11.20
P9	Layout revised to suit natural smoke ventilation strategy.	AW	DW	02.01.21
P10	Layout revised following consultation with client, consultants and specialist manufacturers.	AW	DW	08.03.21
P11	GA Plan general revision including Lidl store layout amendments and changes to core area of offices.	GCJ	DW	19.03.21
P12	Internal door and room references added.	AW	GCJ	13.05.21
P13	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P14	Revision P14 to match Conject	AW	GCJ	06.08.21
P15	Revised for Planning	GCJ	DW	19.08.21
P16	Revised following client comments received 06.09.21	AW	DW	14.09.21

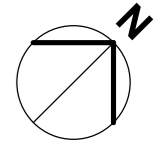


Cantlevered floor over.

Full sluice

Store travellator. Min 3300mm clear width in structure required.

General Arrangement
 Note:
 Coordination with structural, mechanical and electrical consultants ongoing.
 Floor areas subject to location of structural walls and columns - To be confirmed.
 Previous Planning Issue Rev P1



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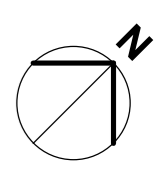
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File Reference: 07758-SPACE-ZZ-01-DR-A-01-0001				



Revision	Description	Author	Checker	Date
P1	First issue of planning drawings, structural grid amended, ancillary accommodation/core developed	LC	PB	29.06.20
P2	General design development, issued for Board review	LC	PB	02.07.20
P3	Design freeze issue	LC	PB	07.08.20
P4	Final draft for comment prior to submission	LC	PB	26.08.20
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P7	Planning Submission	LC	PB	17.11.20
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P10	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P11	Reissued for Planning	GCJ	DW	19.08.21
P12	Revised following client comments received 06.09.21	AW	DW	14.09.21



General Arrangement
 Note:
 Coordination with structural, mechanical and electrical consultants ongoing.
 Floor areas subject to location of structural walls and columns - To be confirmed.
 Previous Planning Issue Rev P1



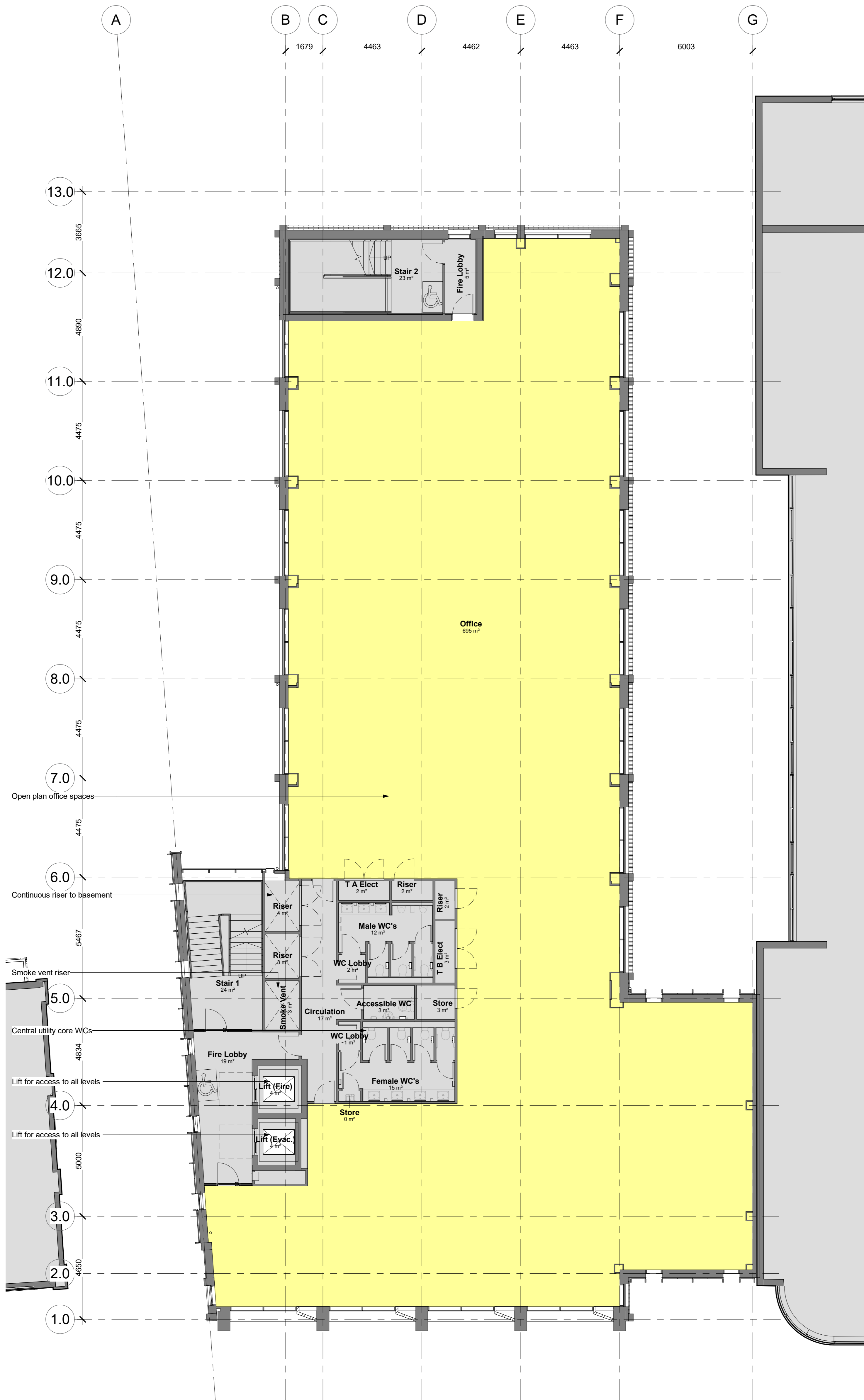
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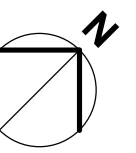
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File Reference: 07758-SPACE-ZZ-02-DR-A-01-0001				



Revision	Description	Author	Checker	Date
P1	First issue of planning drawings, structural grid amended, ancillary accommodation/core developed	LC	PB	29.06.20
P2	General design development, issued for Board review	LC	PB	02.07.20
P3	Design freeze issue	LC	PB	07.08.20
P4	Final draft for comment prior to submission	LC	PB	26.08.20
P5	General amendments made	LC	PB	15.09.20
P6	Draft Planning Submission	LC	PB	23.09.20
P7	Planning Submission	LC	PB	17.11.20
P8	GA Plan general revision including Lidl store layout amendments and changes to core area of offices.	GCJ	DW	19.03.21
P9	Internal door and room references added.	AW	GCJ	13.05.21
P10	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P11	Reissued for Planning	GCJ	DW	19.08.21
P12	Revised following client comments received 06.09.21	AW	DW	14.09.21



General Arrangement
 Note:
 Coordination with structural, mechanical and electrical consultants ongoing.
 Floor areas subject to location of structural walls and columns - To be confirmed.
 Previous Planning Issue Rev P1



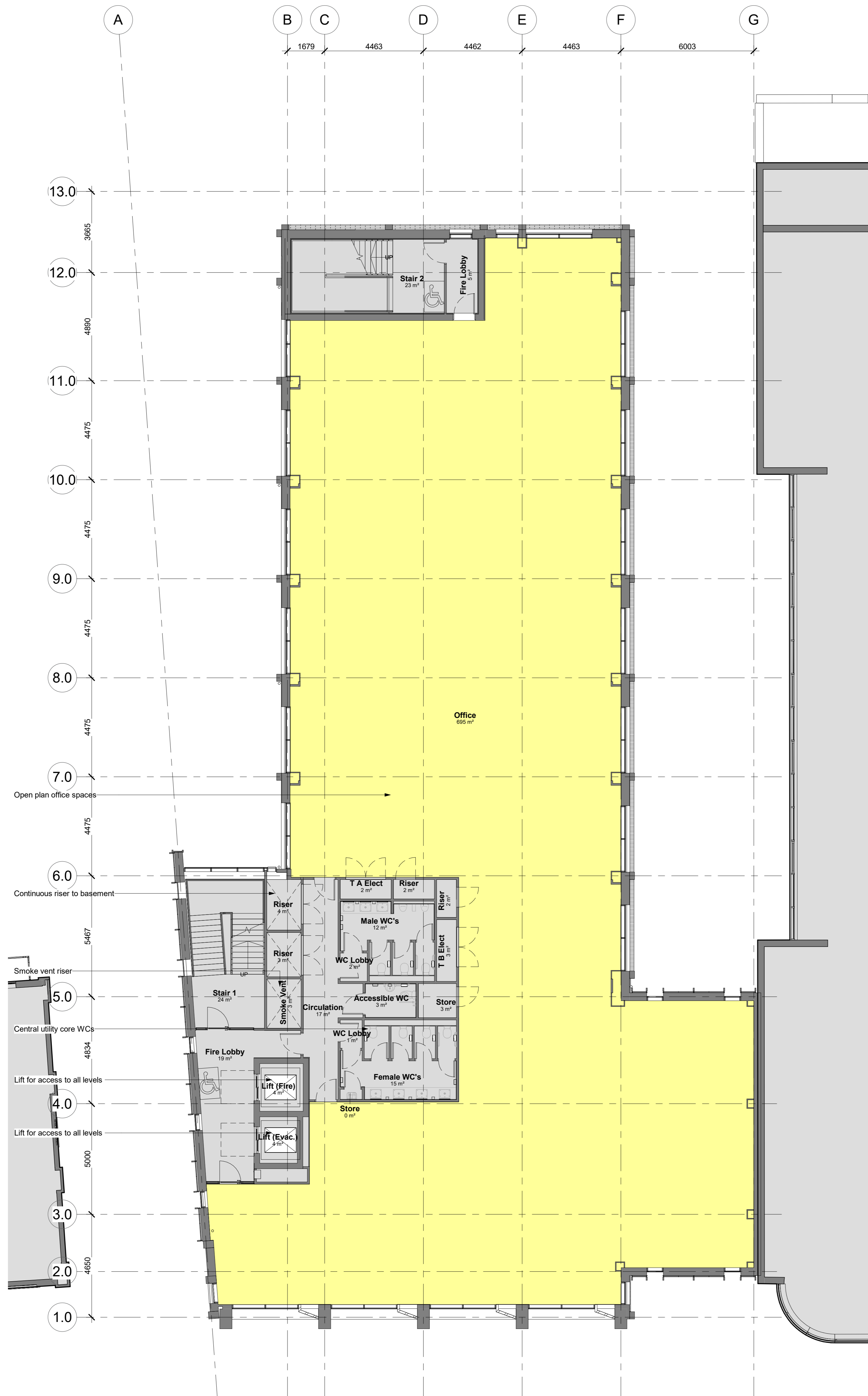
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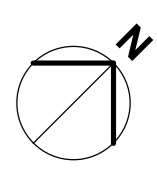
Project 07758 - 19 Worple Road				
Drawing Title General Arrangement Plan - Proposed - Level 03				
Project No. 07758	Drawing No. ZZ-03-DR-A-01-0001	Status S3	Revision P12	Scale at A1 1:100
File Reference: 07758-SPACE-ZZ-03-DR-A-01-0001				



Revision	Description	Author	Checker	Date
P1	First issue of planning drawings, structural grid amended, ancillary accommodation/core developed	LC	PB	29.06.20
P2	General design development, issued for Board review	LC	PB	02.07.20
P3	Design freeze issue	LC	PB	07.08.20
P4	Final draft for comment prior to submission	LC	PB	26.08.20
P5	General amendments made	LC	PB	15.09.20
P6	Draft Planning Submission	LC	PB	23.09.20
P7	Planning Submission	LC	PB	17.11.20
P8	GA Plan general revision including Lidl store layout amendments and changes to core area of offices.	GCJ	DW	19.03.21
P9	Internal door and room references added.	AW	GCJ	13.05.21
P10	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P11	Reissued for Planning	GCJ	DW	19.08.21
P12	Revised following client comments received 06.09.21	AW	DW	14.09.21



General Arrangement
 Note:
 Coordination with structural, mechanical and electrical consultants ongoing.
 Floor areas subject to location of structural walls and columns - To be confirmed.
 Previous Planning Issue Rev P1



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Project 07758 - 19 Worpole Road				
Drawing Title General Arrangement Plan - Proposed - Level 04				
Project No. 07758	Drawing No. ZZ-04-DR-A-01-0001	Status S3	Revision P12	Scale at A1 1:100
File Reference: 07758-SPACE-ZZ-04-DR-A-01-0001				



Revision	Description	Author	Checker	Date
P1	First issue of planning drawings, structural grid amended, ancillary accommodation/core developed	LC	PB	29.06.20
P2	General design development, issued for Board review	LC	PB	02.07.20
P3	Design freeze issue	LC	PB	07.08.20
P4	Final draft for comment prior to submission	LC	PB	26.08.20
P5	General amendments made	LC	PB	15.09.20
P6	Draft Planning Submission	LC	PB	23.09.20
P7	Planning Submission	LC	PB	17.11.20
P8	GA Plan general revision including Lidl store layout amendments and changes to core area of offices.	GCJ	DW	19.03.21
P9	Internal door and room references added.	AW	GCJ	13.05.21
P10	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P11	Reissued for Planning	GCJ	DW	19.08.21
P12	Revised following client comments received 06.09.21	AW	DW	14.09.21



General Arrangement
 Note:
 Coordination with structural, mechanical and electrical consultants ongoing.
 Floor areas subject to location of structural walls and columns - To be confirmed.
 Previous Planning Issue Rev P1

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Project 07758 - 19 Worpole Road				
Drawing Title General Arrangement Plan - Proposed - Level 05				
Project No. 07758	Drawing No. ZZ-05-DR-A-01-0001	Status S3	Revision P12	Scale at A1 1:100
File Reference: 07758-SPACE-ZZ-05-DR-A-01-0001				



Revision	Description	Author	Checker	Date
P1	First issue of planning drawings, structural grid amended, ancillary accommodation/core developed	LC	PB	29.06.20
P2	General design development, issued for Board review	LC	PB	02.07.20
P3	Design freeze issue	LC	PB	07.08.20
P4	Final draft for comment prior to submission	LC	PB	26.08.20
P5	General amendments made	LC	PB	15.09.20
P6	Draft Planning Submission	LC	PB	23.09.20
P7	Planning Submission	LC	PB	17.11.20
P8	GA Plan general revision including Lidl store layout amendments and changes to core area of offices.	GCJ	DW	19.03.21
P9	Internal door and room references added.	AW	GCJ	13.05.21
P10	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P11	Reissued for Planning	GCJ	DW	19.08.21
P12	Revised following client comments received 06.09.21	AW	DW	14.09.21



General Arrangement
 Note:
 Coordination with structural, mechanical and electrical consultants ongoing.
 Floor areas subject to location of structural walls and columns - To be confirmed.
 Previous Planning Issue Rev P1

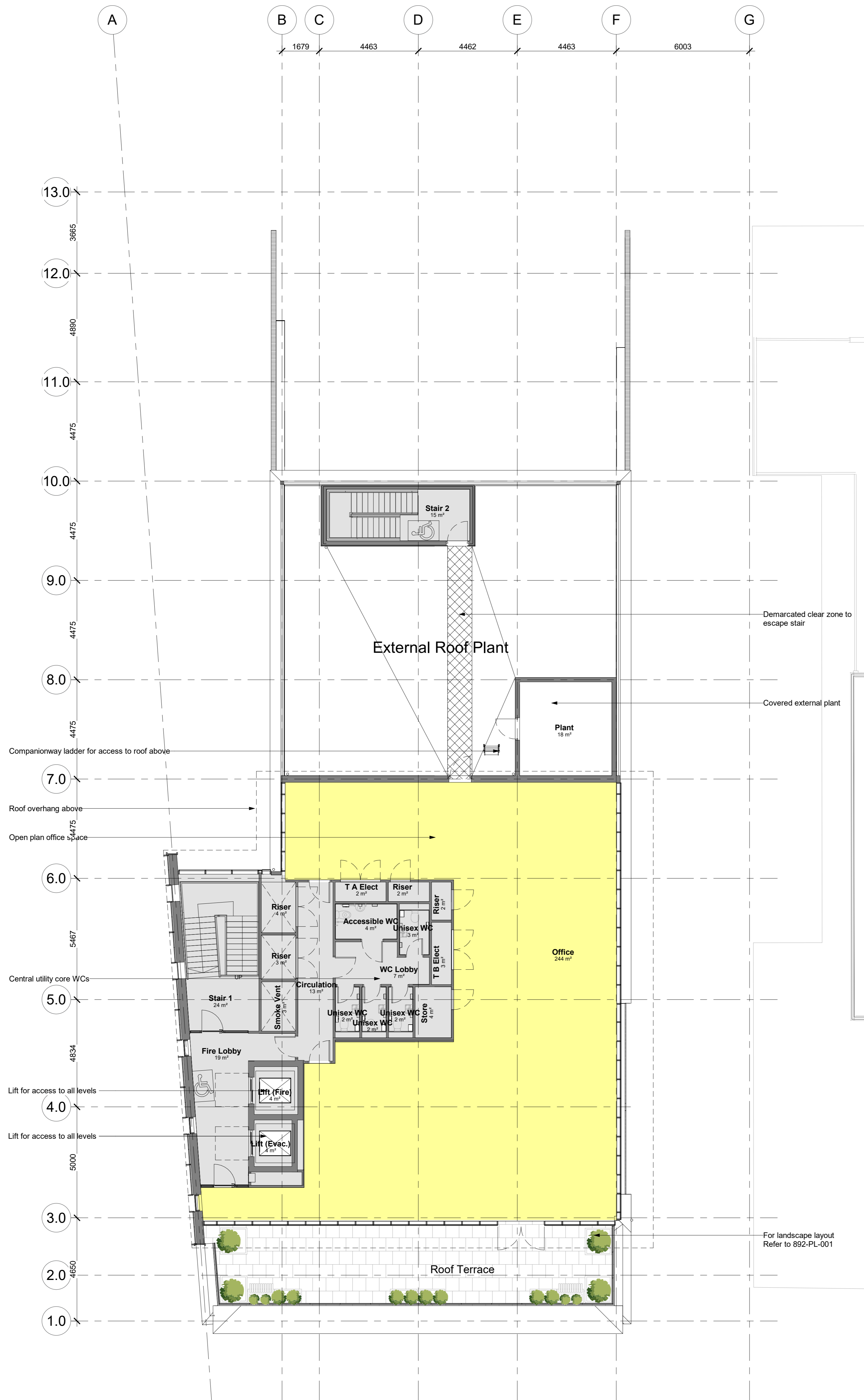
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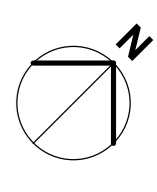
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Project No. 07758	Drawing No. ZZ-06-DR-A-01-0001	Status S3	Revision P12	Scale at A1 1:100
File Reference: 07758-SPACE-ZZ-06-DR-A-01-0001				



Revision	Description	Author	Checker	Date
P1	First issue of planning drawings, structural grid amended, ancillary accommodation/core developed	LC	PB	29.06.20
P2	General design development, issued for Board review	LC	PB	02.07.20
P3	Design freeze issue	LC	PB	07.08.20
P4	Final draft for comment prior to submission	LC	PB	26.08.20
P5	General amendments made	LC	PB	15.09.20
P6	Draft Planning Submission	LC	PB	23.09.20
P7	Planning Submission	LC	PB	17.11.20
P8	GA Plan general revision including Lidl store layout amendments and changes to core area of offices.	GCJ	DW	19.03.21
P9	Internal door and room references added.	AW	GCJ	13.05.21
P10	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P11	Reissued for Planning	GCJ	DW	19.08.21
P12	Revised following client comments received 06.09.21	AW	DW	14.09.21



General Arrangement
 Note:
 Coordination with structural, mechanical and electrical consultants ongoing.
 Floor areas subject to location of structural walls and columns - To be confirmed.
 Previous Planning Issue Rev P1



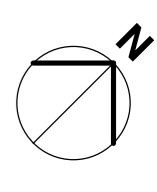
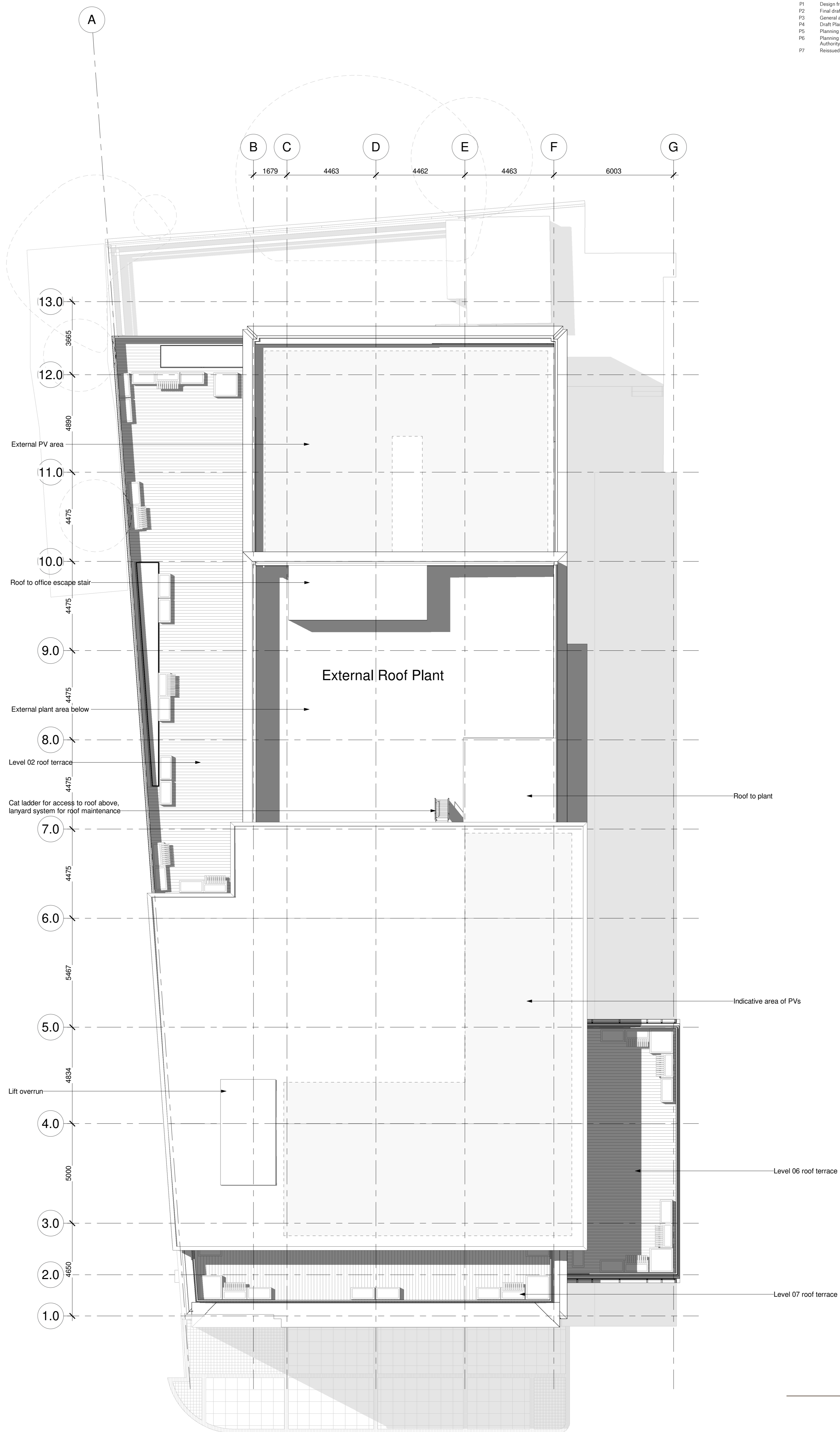
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Project 07758 - 19 Worple Road				
Drawing Title General Arrangement Plan - Proposed - Level 07				
Project No. 07758	Drawing No. ZZ-07-DR-A-01-0001	Status S3	Revision P12	Scale at A1 1:100
File Reference: 07758-SPACE-ZZ-07-DR-A-01-0001				



Revision	Description	Author	Checker	Date
P1	Design freeze issue	LC	PB	07.08.20
P2	Final draft for comment prior to submission	LC	PB	26.08.20
P3	General amendments made	LC	PB	15.09.20
P4	Draft Planning Submission	LC	PB	23.09.20
P5	Planning Submission	LC	PB	17.11.20
P6	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P7	Reissued for Planning	GCJ	DW	19.08.21



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Project 07758 - 19 Worple Road				
Drawing Title General Arrangement Plan - Proposed - Roof Plan				
Project No. 07758	Drawing No. ZZ-RF-DR-A-01-0001	Status S3	Revision P7	Scale at A1 1:100
File Reference: 07758-SPACE-ZZ-RF-DR-A-01-0001				





General Arrangement Elevation - Existing - NE

1 : 100

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Project				
07758 - 19 Worple Road				
Drawing Title				
General Arrangement Elevation - Existing - NE				
Project No.	Drawing No.	Status	Revision	Scale at A1
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File Reference: 07758-SPACE-ZZ-XX-DR-A-02-0054				





General Arrangement Elevation - Existing - NW

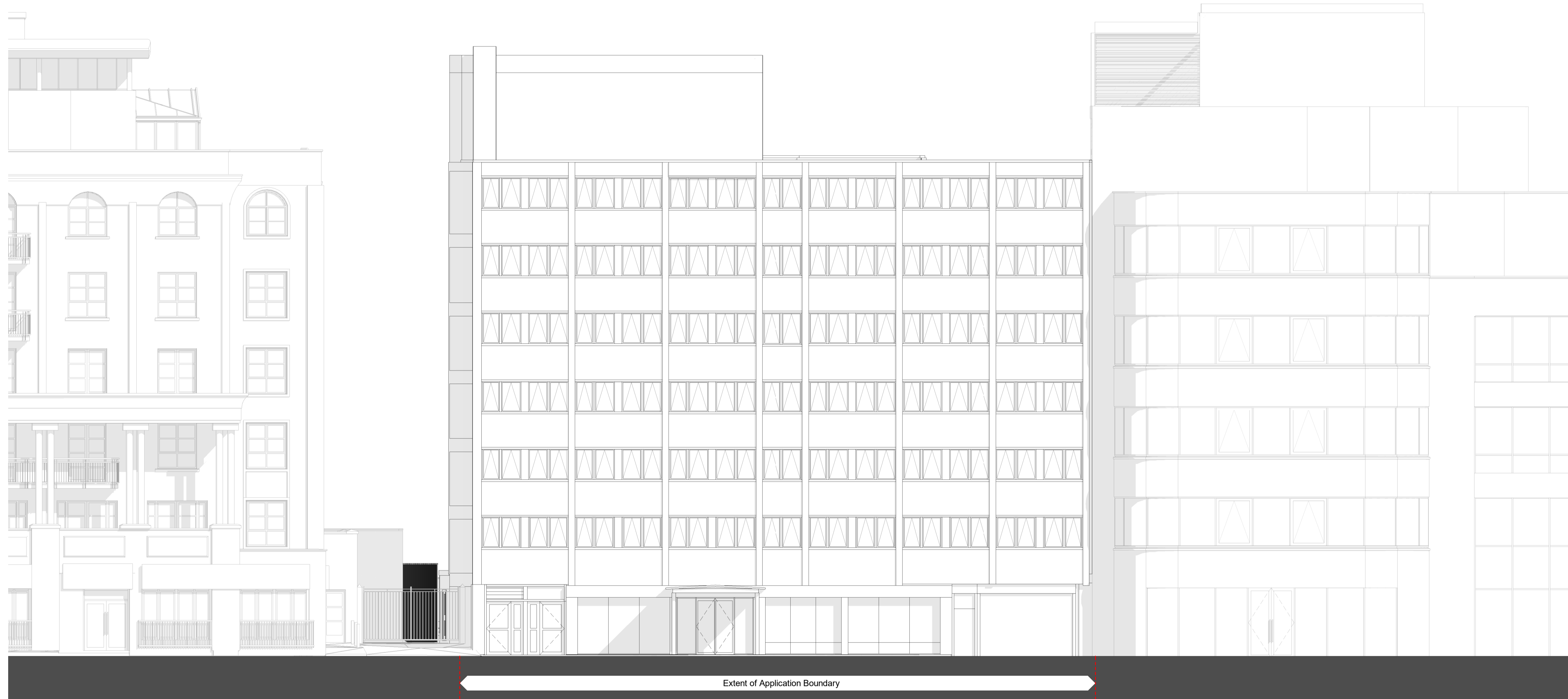
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Project				
07758 - 19 Worple Road				
Drawing Title				
General Arrangement Elevation - Existing - NW				
Project No.	Drawing No.	Status	Revision	Scale at A1
07758	ZZ-XX-DR-A-02-0053	P1	P1	1:100
File Reference: 07758-SPACE-ZZ-XX-DR-A-02-0053				





GA Elevation - Existing - SE (Worple Road)

1 : 100

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Project				
07758 - 19 Worple Road				
Drawing Title				
General Arrangement Elevation - Existing - SE (Worple Road)				
Project No.	Drawing No.	Status	Revision	Scale at A1
07758	ZZ-XX-DR-A-02-0051		P1	1 : 100
File Reference: 07758-SPACE-ZZ-XX-DR-A-02-0051				





General Arrangement Elevation - Existing - SW

1 : 100

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Project				
07758 - 19 Worple Road				
Drawing Title				
General Arrangement Elevation - Existing - SW				
Project No.	Drawing No.	Status	Revision	Scale at A1
07758	ZZ-XX-DR-A-02-0052		P1	1:100
File Reference: 07758-SPACE-ZZ-XX-DR-A-02-0052				



Revision	Description	Author	Checker	Date
P1	First issue of planning drawings	LC	PB	29.06.20
P2	General design development, issued for Board review	LC	PB	02.07.20
P3	Design freeze issue	LC	PB	07.08.20
P4	Final draft for comment prior to submission	LC	PB	26.08.20
P5	General amendments made	LC	PB	15.09.20
P6	Draft Planning Submission	LC	PB	23.09.20
P7	Planning Submission	LC	PB	17.11.20
P8	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P9	Reissued for Planning	GCI	DW	19.08.21
P10	Sectional and Delivery door colour revised to grey, at client request.	AW	DW	22.09.21



Page 116
29860
(Dimension to top of parapet)
(Dimension to lift overrun)

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Project 07758 - 19 Worple Road				
Drawing Title General Arrangement Elevation - Proposed - NE				
Project No. 07758	Drawing No. ZZ-XX-DR-A-02-0004	Status P10	Revision 1	Scale at A1 1:100
File Reference: 07758-SPACE-ZZ-XX-DR-A-02-0004				

This drawing is Copyright - Check all dimensions on site - Inform originator of any discrepancies - Work to figured dimensions only - Do not scale

Revision	Description	Author	Checker	Date
P1	First issue of planning drawings	LC	PB	29.06.20
P2	General design development, issued for Board review	LC	PB	02.07.20
P3	Design freeze issue	LC	PB	07.08.20
P4	Final draft for comment prior to submission	LC	PB	26.08.20
P5	General amendments made	LC	PB	15.09.20
P6	Draft Planning Submission	LC	PB	23.09.20
P7	Planning Submission	LC	PB	17.11.20
P8	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P9	Reissued for Planning	GCI	DW	19.08.21



(unrun) to lift over (Dimension) / 00862
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Project				
07758 - 19 Worple Road				
Drawing Title				
General Arrangement Elevation - Proposed - NW				
Project No.	Drawing No.	Status	Revision	Scale at A1
07758	ZZ-XX-DR-A-02-0003		P9	1:100
File Reference: 07758-SPACE-ZZ-XX-DR-A-02-0003				



Revision	Description	Author	Checker	Date
P1	First issue of planning drawings	LC	PB	29.06.20
P2	General design development, issued for Board review	LC	PB	02.07.20
P3	Design freeze issue	LC	PB	07.08.20
P4	Final draft for comment prior to submission	LC	PB	26.08.20
P5	General amendments made	LC	PB	15.09.20
P6	Draft Planning Submission	LC	PB	23.09.20
P7	Planning Submission	LC	PB	17.11.20
P8	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P9	Reissued for Planning	GCI	DW	19.08.21
P10	Lidl Store curtain glazing frames revised from RAL 5010 to RAL 7024, in accordance with client request.	AW	DW	08.09.21



Front Elevation
1 : 100



Street Scene
1 : 500

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Project 07758 - 19 Worples Road				
Drawing Title General Arrangement Elevation - Proposed - SE (Worples Road)				
Project No. 07758	Drawing No. ZZ-XX-DR-A-02-0001	Status P10	Revision As indicated	Scale at A1 As indicated
File Reference: 07758-SPACE-ZZ-XX-DR-A-02-0001				



Revision	Description	Author	Checker	Date
P1	First issue	LC	PB	10.06.20
P2	General design development, issued for Board review	LC	PB	02.07.20
P3	Design freeze issue	LC	PB	07.08.20
P4	Final draft for comment prior to submission	LC	PB	26.08.20
P5	General amendments made	LC	PB	15.09.20
P6	Draft Planning Submission	LC	PB	23.09.20
P7	Planning Submission	LC	PB	17.11.20
P8	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P9	Reissued for Planning	GCI	DW	19.08.21



06862
 Page 119
 (total dimension to top of parapet)

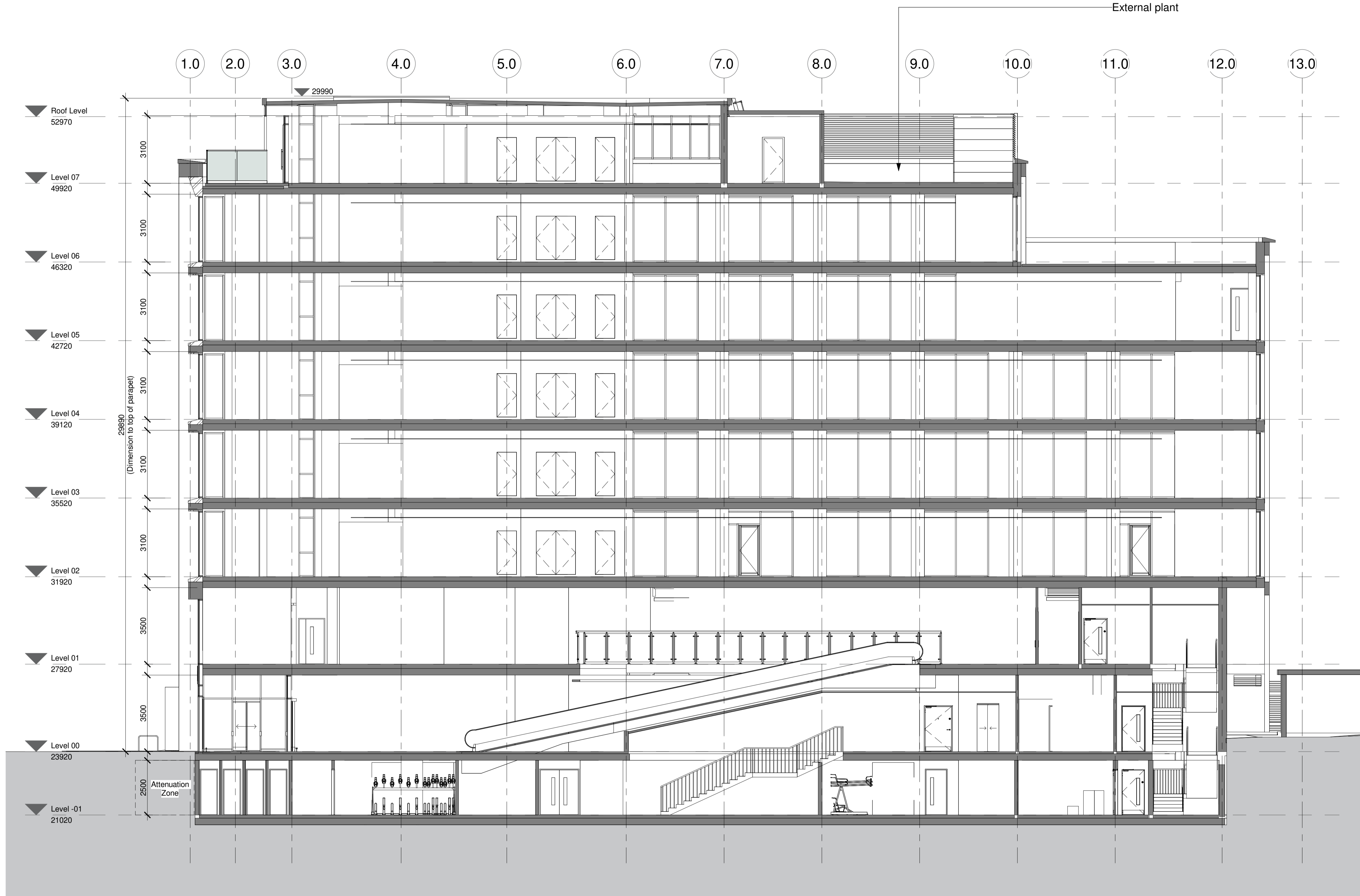
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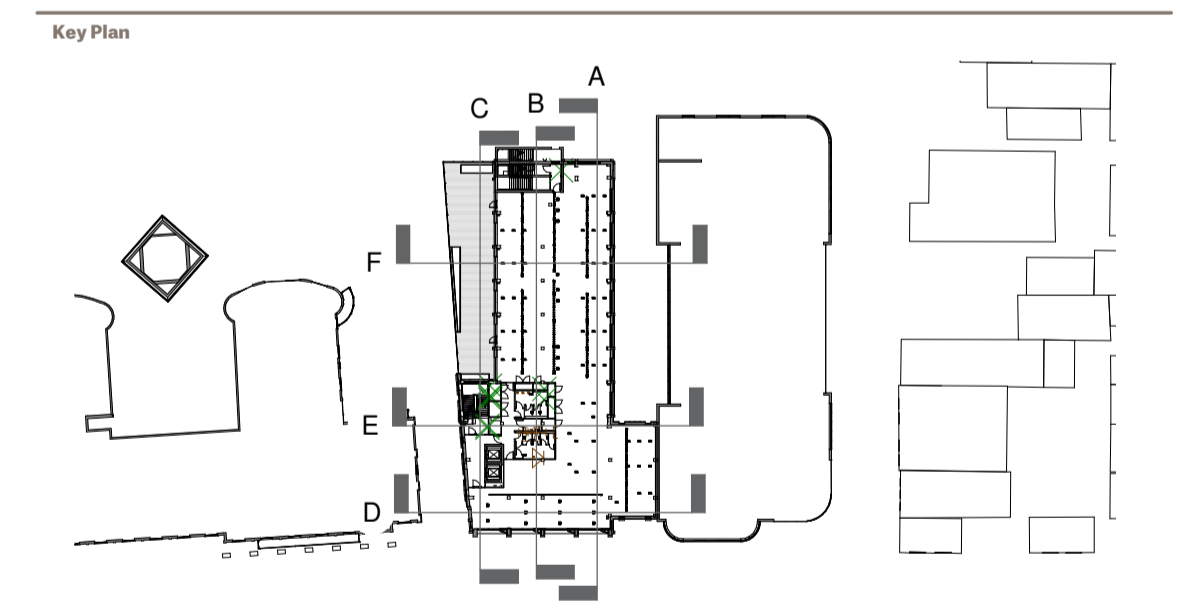
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Drawing Title				
General Arrangement Elevation - Proposed - SW				
Project No.	Drawing No.	Status	Revision	Scale at A1
07758	ZZ-XX-DR-A-02-0002		P9	1:100
File Reference: 07758-SPACE-XX-DR-A-02-0002				



Revision	Description	Author	Checker	Date
P1	First issue of planning drawings	LC	PB	29.06.20
P2	General design development, issued for Board review	LC	PB	02.07.20
P3	Design freeze issue	LC	PB	07.08.20
P4	Final draft for comment prior to submission	LC	PB	26.08.20
P5	General amendments made	LC	PB	15.09.20
P6	Draft Planning Submission	LC	PB	23.09.20
P7	Planning Submission	LC	PB	17.11.20
P8	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P9	Reissued for Planning	GCI	DW	19.08.21



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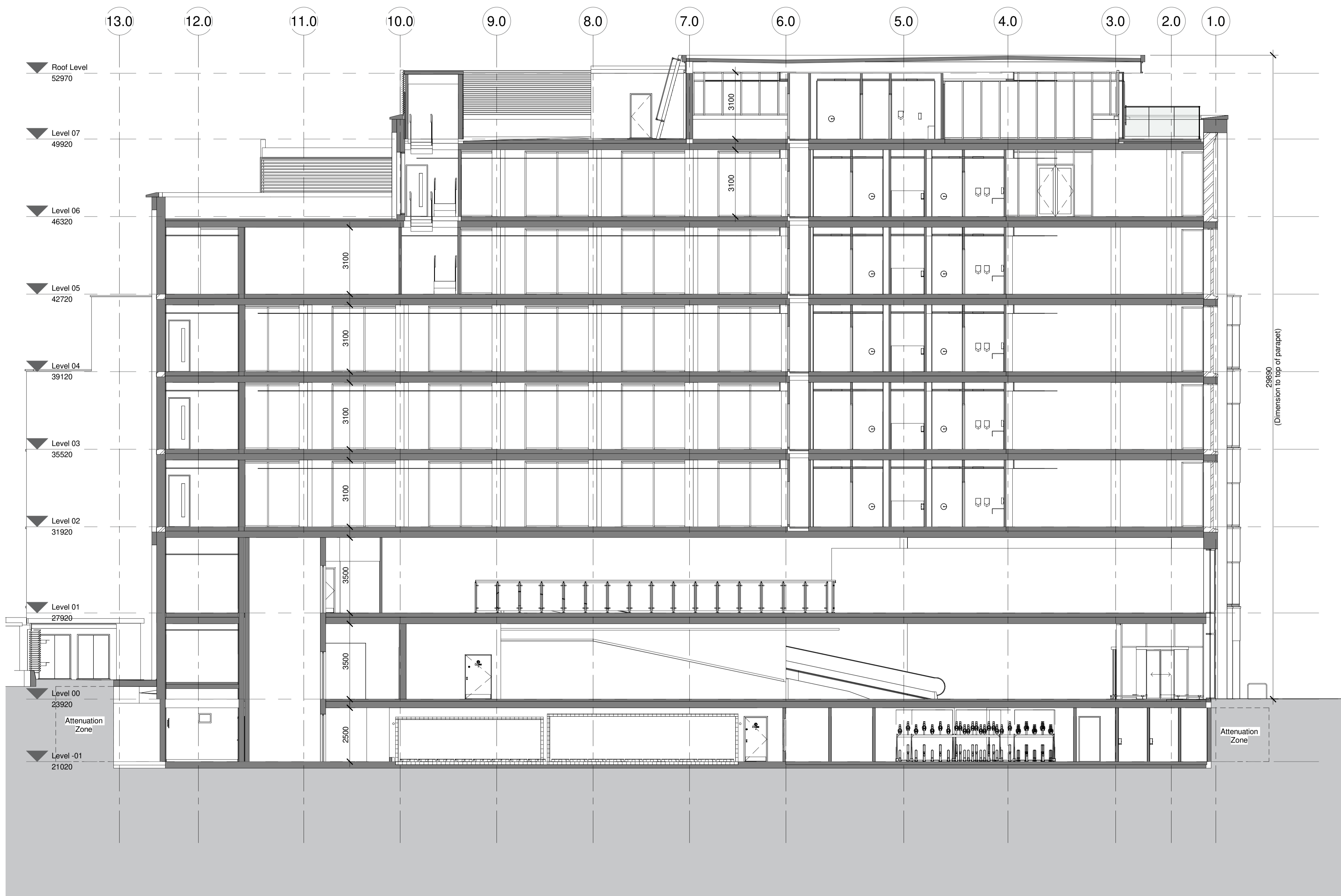
Section A
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Project 07758 - 19 Worple Road				
Drawing Title General Arrangement Sections - Proposed - Sheet 1				
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File Reference: 07758-SPACE-ZZ-XX-DR-A-03-0001				

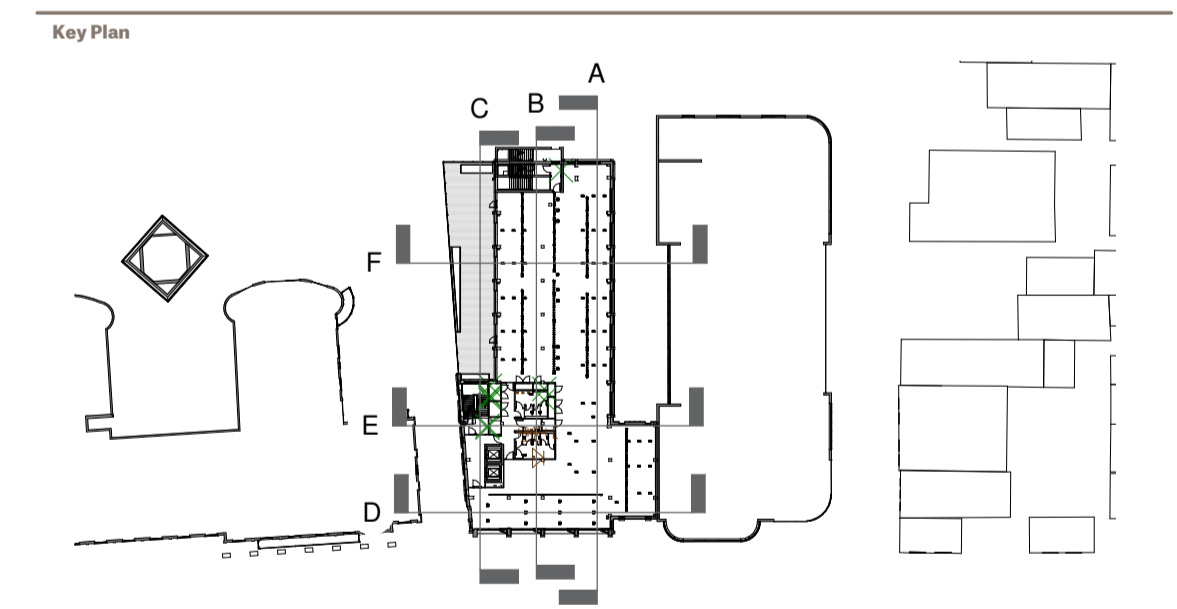


Revision	Description	Author	Checker	Date
P1	First issue, issued for Board review	LC	PB	02.07.20
P2	Design freeze issue	LC	PB	07.08.20
P3	Final draft for comment prior to submission	LC	PB	26.08.20
P4	General amendments made	LC	PB	15.09.20
P5	Draft Planning Submission	LC	PB	23.09.20
P6	Planning Submission	LC	PB	17.11.20
P7	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P8	Reissued for Planning	GCJ	DW	19.08.21

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Section B
1 : 100



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Project 07758 - 19 Worple Road				
Drawing Title General Arrangement Sections - Proposed - Sheet 2				
Project No. 07758	Drawing No. ZZ-XX-DR-A-03-0002	Status P8	Revision P8	Scale at A1 As indicated
File Reference: 07758-SPACE-ZZ-XX-DR-A-03-0002				

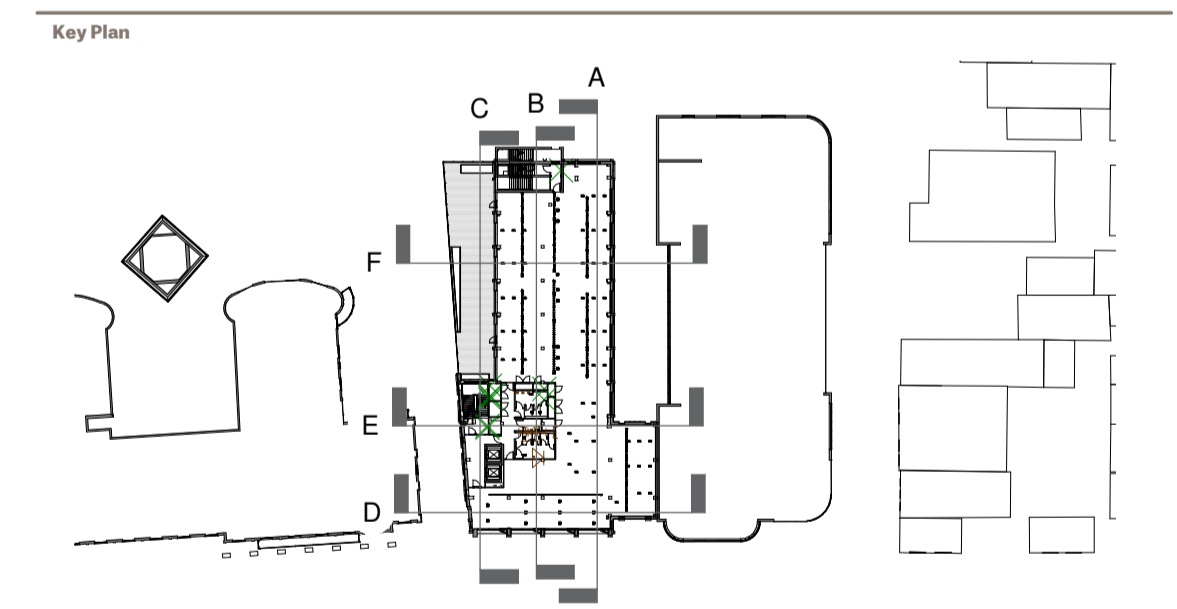


Revision	Description	Author	Checker	Date
P1	First issue, issued for Board review	LC	PB	02.07.20
P2	Design freeze issue	LC	PB	07.08.20
P3	Final draft for comment prior to submission	LC	PB	26.08.20
P4	General amendments made	LC	PB	15.09.20
P5	Draft Planning Submission	LC	PB	23.09.20
P6	Planning Submission	LC	PB	17.11.20
P7	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P8	Reissued for Planning	GCJ	DW	19.08.21



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Section C
1 : 100



SPACE

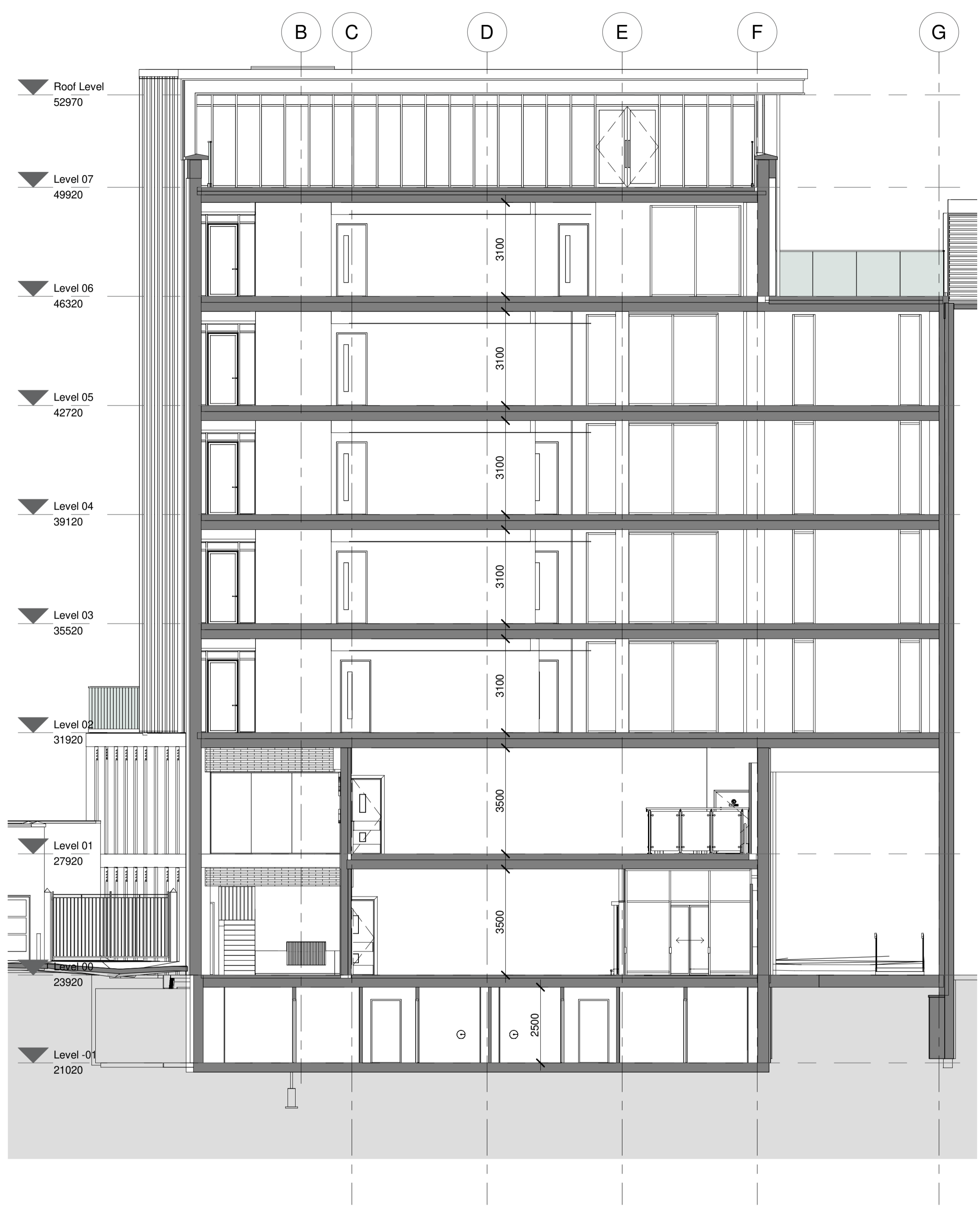
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Project 07758 - 19 Worple Road				
Drawing Title General Arrangement Sections - Proposed - Sheet 3				
Project No. 07758	Drawing No. ZZ-XX-DR-A-03-0003	Status P8	Revision P8	Scale at A1 As indicated
File Reference: 07758-SPACE-ZZ-XX-DR-A-03-0003				

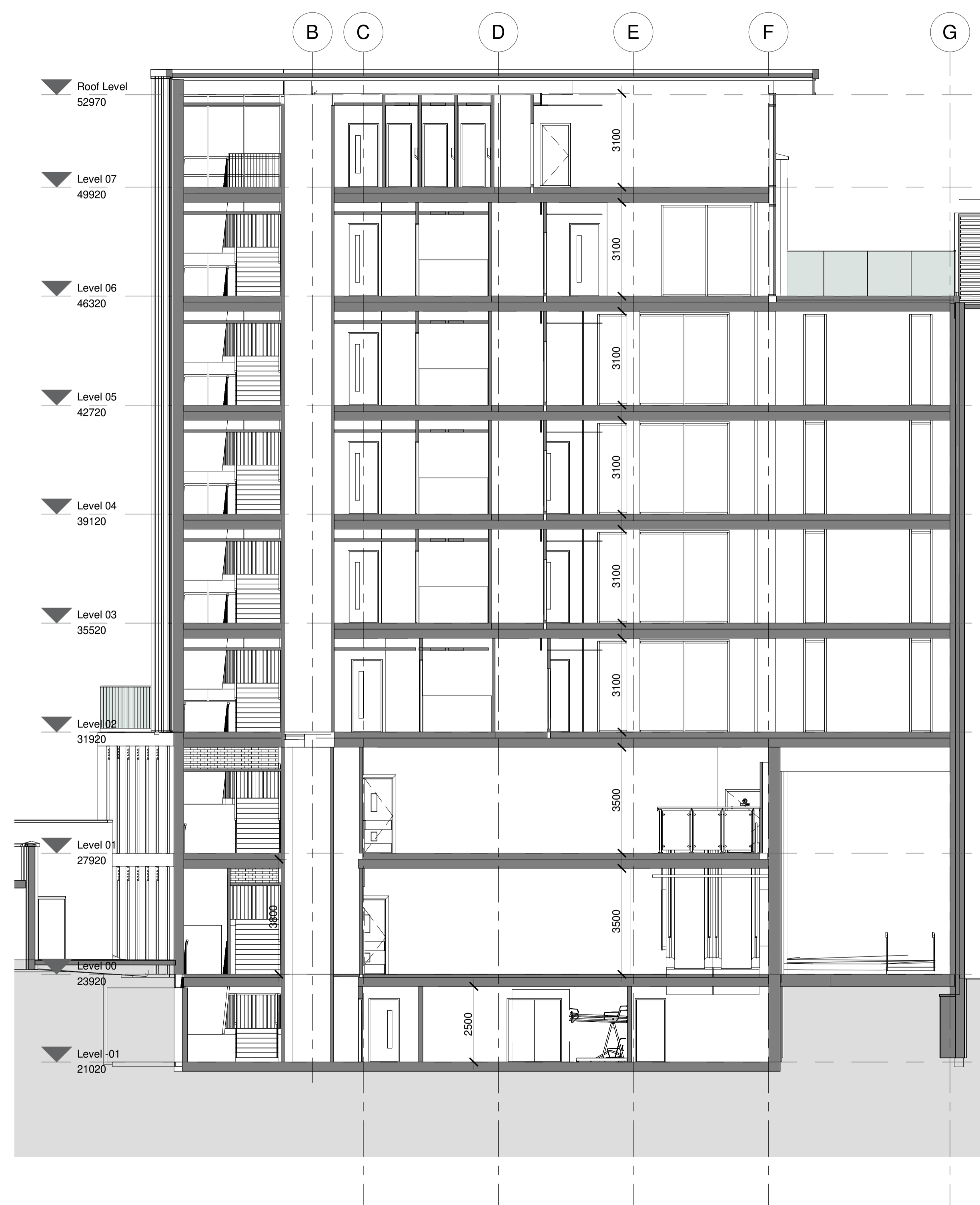


Revision	Description	Author	Checker	Date
P1	First issue, issued for Board review	LC	PB	02.07.20
P2	Design freeze issue	LC	PB	07.08.20
P3	Final draft for comment prior to submission	LC	PB	26.08.20
P4	General amendments made	LC	PB	15.09.20
P5	Draft Planning Submission	LC	PB	23.09.20
P6	Planning Submission	LC	PB	17.11.20
P7	Updated to show extent of Piles	GCI	DW	17.03.21
P8	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P9	Reissued for Planning	GCI	DW	19.08.21

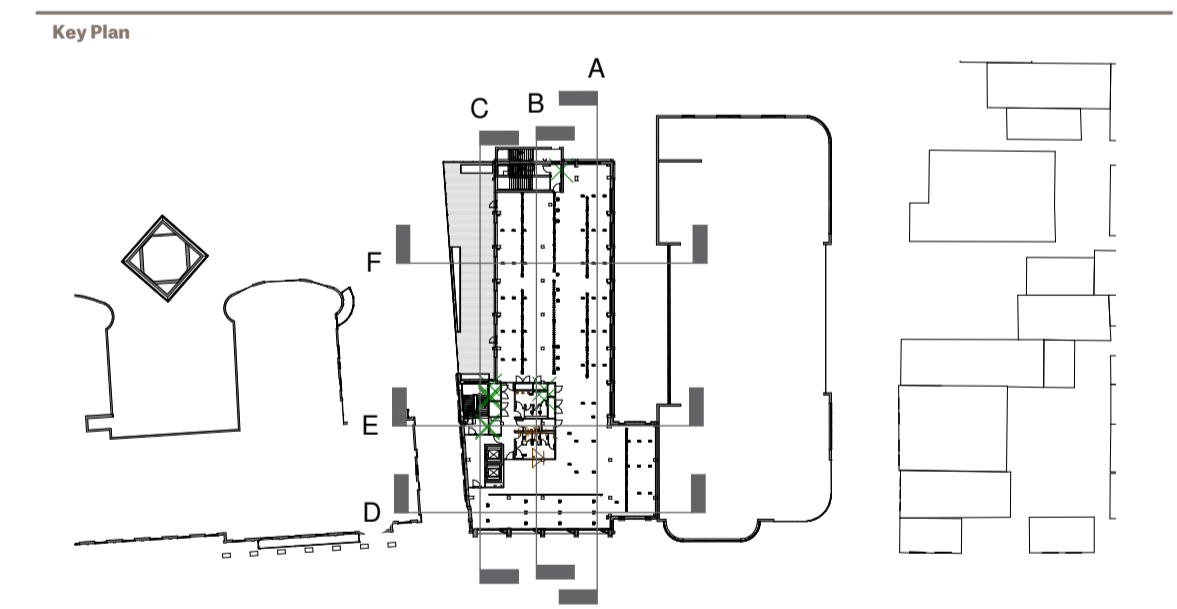
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Section D
1 : 100



Section E
1 : 100



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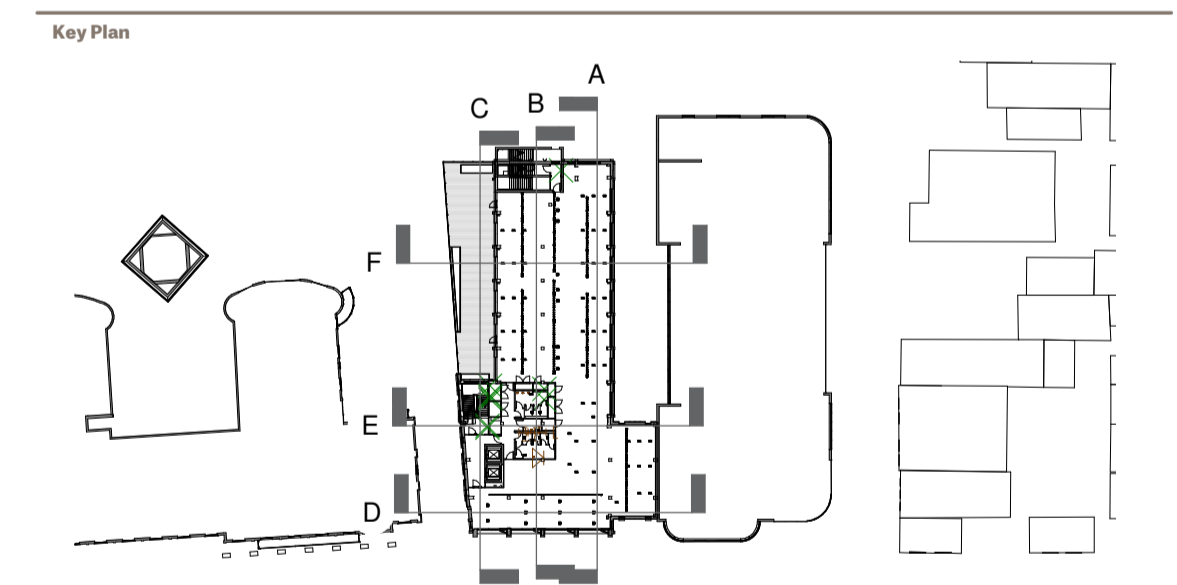
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Drawing Title General Arrangement Sections - Proposed - Sheet 4				
Project No. 07758	Drawing No. ZZ-XX-DR-A-03-0004	Status P9	Revision As indicated	Scale at A1 As indicated
File Reference: 07758-SPACE-ZZ-XX-DR-A-03-0004				



Revision	Description	Author	Checker	Date
P1	First issue, issued for Board review	LC	PB	02.07.20
P2	Design freeze issue	LC	PB	07.08.20
P3	Final draft for comment prior to submission	LC	PB	26.08.20
P4	General amendments made	LC	PB	15.09.20
P5	Draft Planning Submission	LC	PB	23.09.20
P6	Planning Submission	LC	PB	17.11.20
P7	Updated to show extent of Piles	GCI	DW	17.03.21
P8	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P9	Reissued for Planning	GCI	DW	19.08.21



Section F
1 : 100



SPACE

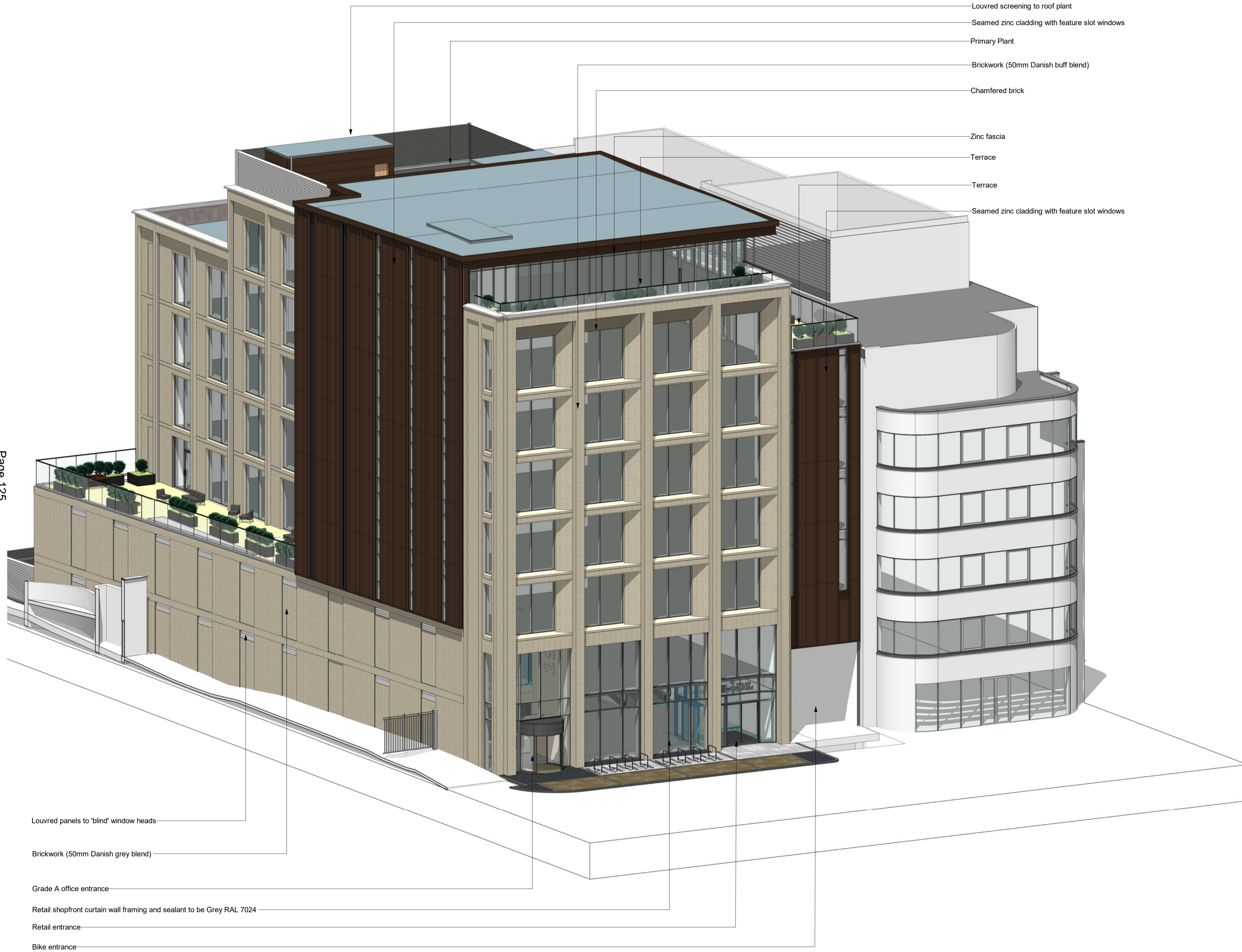
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Project 07758 - 19 Worple Road				
Drawing Title General Arrangement Sections - Proposed - Sheet 5				
Project No. 07758	Drawing No. ZZ-XX-DR-A-03-0005	Status P9	Revision As indicated	Scale at A1 As indicated
File Reference: 07758-SPACE-ZZ-XX-DR-A-03-0005				



Revision	Description	Author	Checker	Date
P1	First issue of planning drawings	LC	PB	29.06.20
P2	General design development, issued for Board review	LC	PB	02.07.20
P3	Design freeze issue	LC	PB	07.08.20
P4	Final draft for comment prior to submission	LC	PB	26.08.20
P5	Draft Planning Submission	LC	PB	23.09.20
P6	Planning Submission	LC	PB	17.11.20
P7	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P8	Reissued for Planning	GCJ	DW	19.08.21
P9	Lidl Store curtain glazing frames revised from RAL 5010 to RAL 7024, in accordance with client request.	AW	DW	08.09.21

Page 125



- Louvre screening to roof plant
- Seamed zinc cladding with feature slot windows
- Primary Plant
- Brickwork (50mm Danish buff blend)
- Chamfered brick
- Zinc fascia
- Terrace
- Terrace
- Seamed zinc cladding with feature slot windows

- Louvre panels to 'blind' window heads
- Brickwork (50mm Danish grey blend)
- Grade A office entrance
- Retail shopfront curtain wall framing and sealant to be Grey RAL 7024
- Retail entrance
- Bike entrance

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Project				
07758 - 19 Worple Road				
Drawing Title				
3D Views - Proposed - Sheet 1				
Project No.	Drawing No.	Status	Revision	Scale at A1
07758	ZZ-XX-DR-A-04-0001		P9	
File Reference: 07758-SPACE-ZZ-XX-DR-A-04-0001				

Revision	Description	Author	Checker	Date
P1	First issue of planning drawings	LC	PB	29.06.20
P2	General design development, issued for Board review	LC	PB	02.07.20
P3	Design freeze issue	LC	PB	07.08.20
P4	Final draft for comment prior to submission	LC	PB	26.08.20
P5	Draft Planning Submission	LC	PB	23.09.20
P6	Planning Submission	LC	PB	17.11.20
P7	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P8	Reissued for Planning	GCJ	DW	19.08.21



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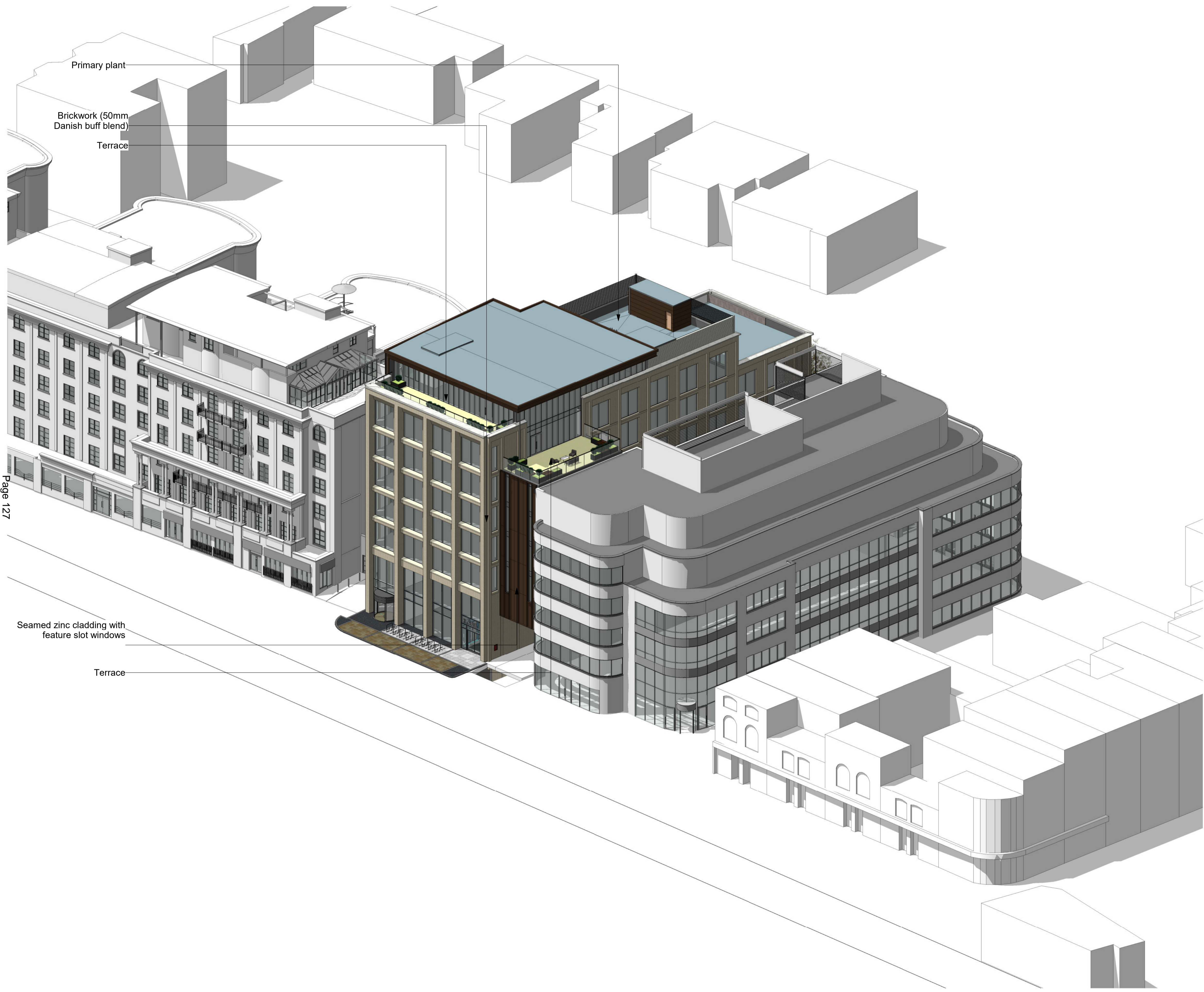
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Project 07758 - 19 Worples Road				
Drawing Title 3D Views - Proposed - Sheet 2				
Project No. 07758	Drawing No. ZZ-XX-DR-A-04-0002	Status P8	Revision P8	Scale at A1
File Reference: 07758-SPACE-ZZ-XX-DR-A-04-0002				

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P1	First issue of planning drawings	LC	PB	29.06.20
P2	General design development, issued for Board review	LC	PB	02.07.20
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P7	Planning issue, responding to comments received from the Local Planning Authority.	AW	DW	06.08.21
P8	Reissued for Planning	GCJ	DW	19.08.21
P9	Lidl Store curtain glazing frames revised from RAL 5010 to RAL 7024, in accordance with client request.	AW	DW	08.09.21

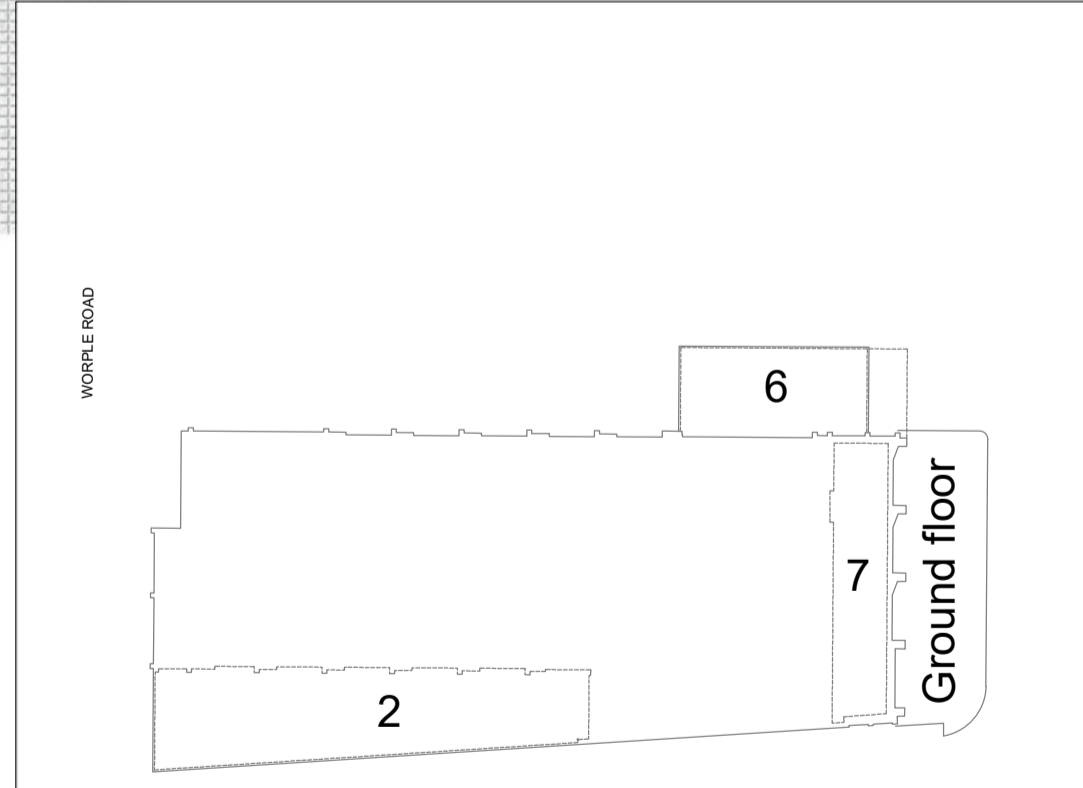


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Project 07758 - 19 Worple Road				
Drawing Title 3D Views - Proposed - Sheet 3				
Project No. 07758	Drawing No. ZZ-XX-DR-A-04-0003	Status P9	Revision P9	Scale at A1
File Reference: 07758-SPACE-ZZ-XX-DR-A-04-0003				

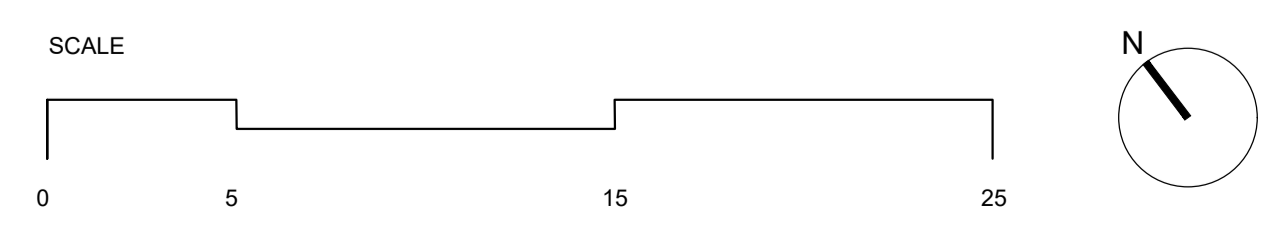


001 Navigation map - terrace locations
Scale: n/a

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SIGNIFICANT HEALTH, SAFETY & ENVIRONMENTAL INFORMATION RELATING TO CDM
HEALTH:
SAFETY:
ENVIRONMENTAL:
IT IS ASSUMED ALL WORKS WILL BE CARRIED OUT BY A COMPETANT CONTRACTOR WORKING, WHERE APPROPRIATE, TO AN AGREED METHOD STATEMENT

Rev.	Description	date	by	checked
P00	First Issue	18/09/20	MN	KG
P01	Softworks removed from ground floor. Terraces revised. Application boundary amended.	28/09/20	MN	KG
P02	Revised design based on planning comments.	01/09/21	CHC	KG



Client
Lidl UK GmbH
Client address
19 Worples Road
Wimbledon
London
SW19 4JS
United Kingdom

Project Title
LIDL, 19 Worples Road
Drawing Title
Rendered Landscape Masterplan
Project Stage
Planning

PLACE
Design + Planning
scale 1:100 at A1 date 18/09/20
892-PL-002

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drawn by MN checked by KG
rev no. **P02**

PLANNING APPLICATIONS COMMITTEE
18th August 2022.

Item No:

UPRN	APPLICATION NO.	DATE VALID
	21/P2571	29.06.2021

Address/Site Sandham House
Boundary Business Court
92 - 94 Church Road
Mitcham
CR4 3TD

Ward: Cricket Green

Proposal: Erection of a single storey extension, including alterations to the car park layout plus associated works to existing office unit to facilitate use as a Tesco retail store.

Drawing Nos; Site location plan and drawings PL006-REV17, PL007-REV16, PL008-REV06, PL010-REV09, PL011-REV08, PL012-REV03 & PL016-REV02.

Contact Officer: Leigh Harrington (020 8545 3836)

RECOMMENDATION

Grant planning permission subject to relevant conditions and a s106 agreement for highway alterations/adoption.

CHECKLIST INFORMATION.

- Heads of agreement: Yes
- Is a screening opinion required: No
- Is an Environmental Statement required: No
- Has an Environmental Impact Assessment been submitted: No
- Design Review Panel consulted: No
- Number of neighbours consulted: 38
- Press notice – No
- Site notice –Yes
- External consultations: No
- Archaeological Priority Zone – No
- Controlled Parking Zone – No
- Number of jobs created: 25 full time equivalent

1 INTRODUCTION

- 1.1 The application has been brought before the Committee due to the level of public interest. The application is subject to a requirement for planning permission because it involves the erection of a new extension to the existing building and this is not an option under permitted development or prior approval.
- 1.2 The application and its linked application 22/P2570 were deferred from the June 2022 PAC meeting because of outstanding concerns relating to trees.

2. SITE AND SURROUNDINGS

- 2.1 The application site is a semi vacant office building located at the northern end of the Boundary Business Court, an industrial park situated on the eastern side of Church Road in Mitcham. The building has car parking areas to the north and east and an unused vehicle entrance from Church Road to the north. To the north of the site is a new as yet unfinished residential development of flats on the former site of the Fosters Auto Care Centre. The site is bounded to the east and south by the Business Court whilst residential properties to the west are located on the opposite side of Church Road and behind a large grass verge area.
- 2.2 The site is not located within a conservation area nor is it in anyway listed. The site is not located within a controlled parking zone and has a public transport accessibility level (PTAL) of 2 (0 being the lowest and 6b being the best).

3. CURRENT PROPOSAL

- 3.1 The proposal is for erection of a single storey extension, including alterations to the car park layout plus associated works to existing office unit to facilitate use as a Tesco retail store.
- 3.2 The new extension would be single storey and nearly 7m deep with an 86sqm footprint and be located on the eastern car parking area along with a designated delivery bay. The extension is required to provide sufficient floor space for trading areas, refrigerators, checkout areas and customer circulation space to enable the site to operate as a Tesco Express. The extension would increase the property's ground floor area to 316 sqm, including a trading area measuring 240 sqm. The extension would be single storey and be accompanied by a fenced service yard for mechanical plant and equipment, together with a refuse compound.
- 3.3 The existing entrance to the upper floor office would be retained and a new store entrance created in the north elevation.

- 3.4 The existing northern car park area would be reconfigured to provide 12 customer parking bays for the new retail use with a new security gate and fence. The layout has been amended to reflect concerns of the impact of the proposals on trees on site and includes one disabled bay and 2 active charging points. 18 covered cycle parking spaces would be provided.
- 3.5 As the pedestrian approach to the site from the south is currently quite restricted with an abnormally narrow pavement the application is accompanied by a proposal to widen the footpath by reducing the depth of the green verge in front of the other offices at Strudwick and Richardson Houses.
- 3.6 Following the deferral from the June PAC officers entered into further discussions with the applicants in relation to site trees and layout. As a result the car parking layout has been amended to protect the Rowan tree (T4). The arrangements for the refuse store have been amended to create a more porous base and the hard standing office entrance path narrowed and a strip of greenery added. The details of this are expanded upon with the trees section.

4. **PLANNING HISTORY**

- 4.1 21/P2570 - Application for advertisement consent for the display of new shop signage, comprising 1 x non-illuminated fascia sign, 2 x internally illuminated fascia signs and 1 x internally illuminated totem sign – Application decision for consideration at the August Planning Committee
- 4.2 21/P0162 - Lawful Development Certificate issued in respect of the proposed re-classification from class B1 to class E.
- 4.3 88/P1359 - Planning permission granted for redevelopment of the site by the erection of 21 no. new industrial units (use classes b1 & b2) with car parking landscaping fencing and formation of new access off Church Road.

Other relevant planning history

4.4 96 Church Road (Fosters' Autos site)

19/P0191 - Outline application (with landscaping a reserved matter) for the redevelopment of the site involving the erection of a 4 storey residential block to provide 20 x flats (revision of 17/P4147). Approved by PAC 22/08/2019.

19/P3178 - Application for approval of reserved matters (landscaping) relating to LBM outline permission 19/P0191 for the redevelopment of the site involving the erection of a 4 storey residential block to provide 20 x flats. Approved.

5. CONSULTATION

5.1 Consultation letters sent to neighbours and site notice posted. Objections were received from 42 persons raising concerns relating to;

Neighbour Amenity

- To build a commercial use like a supermarket will impact amenity of occupiers of the new flats
- Noise and disturbance from customers and their vehicles in close proximity to the new flats.
- Light pollution, but if not enough lighting then crime likely to rise as a result.
- There are also environmental/pollution concerns. Residents will have to suffer the increased impact of vehicles using the Tesco parking bays and increased flow of delivery vehicles passing your property. Stationary vehicles with their engines on will create noise and environmental pollution. The proposed store is likely to be open throughout the evening into late hours and this will also cause noise concerns.
- Noise report ignores those flats and concentrates on existing residential neighbours at Goodwin Close.
- Negative impact on other business users, the quiet space for office work within the court will be disrupted by the noise and refuse that is commensurate with a busy retail store, with vehicles, deliveries and packing boxes all impacting on the rear of the property and the neighbouring units.
- Members of the public will be able to gain access to the site, causing noise pollution and security concerns with a store open for 18 hours a day.
- The noise assessment was undertaken during the pandemic when things were quieter.

Highways

- Not enough car parking for a supermarket
- There will be overflow parking elsewhere on the industrial estate and surrounding roads
- Can't enforce that staff walk to work.
- Where will the upstairs office staff park?
- There won't be enough parking space left for the rest of the estate.
- Articulated lorries will have to use the whole site causing disturbance
- The extension would be located on parking spaces provided to other companies as part of their lease agreements (Manroland Sheetfed 1st flr Southerton House)
- Pedestrian access to the site is severely constrained
- There will be inevitable overspill of parking from staff/customers using the store and the spaces currently used for existing business staff may be lost

- An increase in traffic going in and out of the business park itself and outside is likely to cause an accident prone area. There are neighbouring schools and the area is also surrounded by an elderly/aged population.
- The estate could not cope with the extra traffic
- C&W is instructed by Royal Mail Group (RMG) to submit a holding objection to the above planning application on the grounds that the proposed development will have a significant adverse impact upon the ability of occupiers to use the Sandham House Boundary Business Court and will create significant highway safety risks by virtue of the layout of the proposed car park and the proposed car parking arrangement is likely to create significant operational and safety issues for RMG and other occupiers of the estate.

Use/Design

- The loss of office space will outweigh any job creation that is likely to result from the new supermarket opening. The current office building blends in well with the area and offers a "clean" and "modern" look. However, changing the structure of the property to include a retail entrance, with large retail glass front, illuminated signs will take away from the Property's current appearance.

Other

- If the shop sells alcohol it will have a negative impact on local safety with increased anti-social behaviour
- The estate has attracted anti-social behaviour before and this will drive it up.
- Businesses currently using the Court will be driven away by the disruption that having a busy Tesco store will. The noise, refuse, compromised security, increased public footfall and the sale of liquor on the site will make the prospect of renting units within the court significantly less attractive, both for newcomers and for existing businesses.
- Other local businesses will not be able to compete with this large-scale supermarket and will eventually run them out of business. As the owner of a local post office myself, I am obligated to dispute this as I will have to take my post office shutdown eventually as well. Not only will this reduce local market competition but will lead to the closing of the many small businesses like small corner shops and nearby Londis.
- The proposed use of part of the property as a retail outlet (in particular one selling alcohol) is inconsistent with the purposes of the business park situated at the Property. The business park is wholly commercial in nature. There is no access to the public and members of the public do not visit inside the business park unless required. Allowing members of the public access to the business park is likely to make the businesses therein more vulnerable to crime and anti-social behaviour. Unauthorised and unnecessary

visits by members of the public to the business park are also likely to impact financially on your existing tenants as they will have to put procedures in place to deal with the consequences.

- No suitable site security for the other estate users is proposed
- The Police has said they have no objections but have not been to the site so their opinion should be given limited weight
- The subsequent amendments to the proposals do not overcome our initial concerns.

Merton Tree Wardens Group

- 5.2 Merton Tree Wardens Group raised concerns at claims that the trees were all Category C. There should be mitigation for the loss of the hedges as they are also important. A more eco-friendly surfacing material than tarmac should be used.

Former Cllr Alderman Munn

- 5.3 Alderman Munn commented that the proposed new function would cause demonstrable harm to the business court and the new neighbours at 96 Church Road. The proximity of those flats to the car park would lead to pollution and noise harmful to those residents. There would be a need for improved security for the rest of the business court.

Metropolitan Police

- 5.4 Metropolitan Police Safer by Design Officer noted that “security and safety should be of paramount importance given the location within a high crime area. Personally I have had no major crime issues with Tesco Express or Sainsbury stores having completed several in Lambeth and Wandsworth. Usually they will put in place security guards for the first 4 months of store opening to combat shoplifting and opportunistic theft at school times but this is subject to local policies.

Whilst I am not familiar with the local crime risk in Merton but I believe it to be considerably lower than Wandsworth and Lambeth. There may be issues with some Traveller activity at times which may well increase local crime rates occasionally and local Safer Neighbourhood Police who have local knowledge on this would be better placed to advise on this specific activity.

Tesco have a comprehensive corporate management and security plan for all their store portfolio and I would ask them to provide you this so that you can evaluate it against the local crime risk.

Deliveries sometimes cause residents’ concerns due to lorry activity and parking on busy roads however this can be sorted with a dedicated area that can be assessed by your transport and highways team.

Cigarette theft has occurred in some supermarket retail units but risk assessments now cater for this so the risk whilst there is low.

There have been some cases of drug dealing in supermarket car parks in London but again this is not common and where it has occurred it has been managed and risk assessed for.

Hours and operation are rarely a problem unless it is a 24 hour store again this will be licensing's area of expertise as they know the local crime / alcohol issues.

I would recommend the following:

- 1) That Tesco provide the council a comprehensive management plan and risk assessment for their store.
- 2) That Tesco have comprehensive CCTV coverage both inside the store and covering their curtilage / car park/ external cash machine (if applicable) etc.
- 3) That staff are trained to deescalate conflict (Covid masks etc).
- 4) You may feel that local policies in consultation with Licensing specialists may put in place restrictions on the sale of alcohol and Knives (such as kitchen knives).

In conclusion there is not enough specific evidence to object to this application as the benefits provided may be more positive to the community however if this does go to committee then the 4 recommendations above may be considered to mitigate any risks that are identified by other consultees”.

Councils Tree Officer

- 5.5 The Council's Arboricultural officer initially shared resident and colleague concerns regarding the loss of the trees and lack of suitable replacements and was involved in discussions with the applicants to improve arboricultural matters at the site. Following the improvements to the layout and the provision of five new trees the officers' concerns were addressed subject to conditions.
- 5.5.1 The officer raised no objections to the latest landscape drawings subject to the imposition of suitable conditions relating to; Tree Protection, Site Supervision, Landscaping and Existing retained trees.

Transport Planning

- 5.6 The proposed rear extension will result in the loss of eight of the existing 15 parking spaces shared between Sandham House and adjacent Southerton House. The seven remaining parking spaces will serve the remaining first floor office at Sandham House and adjacent Southerton House.

The proposal identifies 14 parking spaces to the north of the building, flanking the existing Boundary Business Court access road. These spaces to be for use by the proposed convenience store.

Two of the 14 convenience store parking spaces allocated for disabled users, with two spaces with active electric charging facilities for the use by customers and staff. The proposed car parking satisfies the London plan parking standards.

The proposed development will result in an additional 42 vehicle trips during the AM weekday peak, 35 during the PM weekday peak and 41 during the Saturday peak.

I concur with the applicant's assessment the trips to the convenience store will be derived from people who live close to the site, with a significant proportion linked with a trip made for another purpose (e.g. way home from work).

The proposed development is unlikely to have a significant impact on the adjoining highway network.

Raise no objection subject to:

- Car parking (with electric Charging points) as shown maintained
- Cycle parking (secure & undercover) maintained.

Following the reduction in the car parking to 12 spaces the Transport Planning Officer commented that the reduction in only two spaces was acceptable and the disabled and EV bays would still be provided.

5.7 Mitcham Village Residents' Association

- There are no formal security proposals and the costs should not be passed to other businesses on the site.
- Other businesses are initiating their break clauses and leaving which will reduce employment
- Loss of trees at a time of climate change, they help mitigate the impact on the new flats being built.
- Proposals will have a negative impact on the new occupiers because the sites are so close

Environmental Health

- 5.8 I would raise some concern regarding this development. The new residencies at 96 Church Road would have a façade that fronts the proposed car park of the Tesco and the new dwellings would be subjected to potential noise disturbance from vehicles engines, door slamming and customers particularly in the evening/night. There also seems to be provision of a 24h cash machine.

In addition, during the evenings, the vehicles headlights using the car park could also be distracting to the occupants in the dwellings facing the development.

It is not clear what the boundary treatments between the two sites will consist of.

I consider that the above issues should be addressed within the application.

Should you be minded to approve the application then I would also recommend the following planning conditions:-

- 1) Due to potential impact on the surrounding locality from the development the noise criteria specified in the 'Auricl Acoustic Consultancy' Plant Noise Assessment Report dated 18 May 2021, Document Reference: R/PNA/1/210518 shall be implemented in full and maintained as a minimum standard. A post completion noise assessment shall be undertaken to demonstrate compliance and submitted to the LPA for approval.
- 2) Any lighting installations should be assessed against the Institute of Lighting Professionals' "Guidance notes for the reduction of obtrusive light", relevant British Standards and codes of practice to prevent any light spillage or glare at residential premises.
- 3) The opening hours of the retail unit shall be limited to 7 am and 11pm.

6. POLICY CONTEXT

- 6.1 Relevant policies in the London Plan 2021 are;
- H2 (Small sites)
 - D1 (London's form, character and capacity for growth)
 - D3 (Optimising site capacity through a design lead approach)
 - D5 (Inclusive design)
 - D11 (Safety & Security)
 - D13 (Agent of Change)
 - D14 (Noise)
 - E 1 (Offices)
 - E9 (Retail, markets and hot food takeaways)
 - G7 (Trees and Woodlands)
 - GG2 (Making the best use of land)
 - GG6 (Increasing efficiency and resilience)
 - SI 2 (Minimising greenhouse gas emissions)
 - SI.3 (Sustainable drainage)
 - T4 (Assessing and mitigating transport impacts)
 - T5 (Cycling)
 - T6.3 (Retail Parking)

6.2 NPPF 2021

6.3 Merton Local Development Framework Core Strategy (2011)

Relevant policies include:

CS 11 Infrastructure

CS 13 Open space and nature conservation

CS 14 Design

CS 15 Climate Change

CS 17 Waste

CS 18 Transport

CS 20 Parking servicing and delivery

6.4 The relevant policies in the Council's Adopted Sites and Policies Plan 2014 are:

DM D1 Urban design and the public realm

DM D2 Design considerations

DM O2 Nature conservation, trees and hedges

DM T2 Transport impacts of development

DM T3 Car Parking and servicing standards

7. PLANNING CONSIDERATIONS

7.1 The principal planning considerations in this case relate to the principle of the retail use, the design and appearance of the extension, neighbour amenity, parking and trees.

7.2 Principle of retail use

7.2.1 Amendments to The Town & Country Planning Act Use Class Order 1987 were made in 2020 under The Town and Country Planning (Use Classes) (Amendment) (England) Regulations 2020, which came into effect on 1st September. The amendments re-classified a number of use classes under Schedule 2, Part A, with the introduction of a new class named 'Commercial, Business and Service' under 'Class E', incorporating previous classes A1, A2, A3, B1(a, b & c), D1 & D2. Offices are now categorised under Class E (g)(i).

7.2.2 The former A1 use class for retail use has now been reclassified as 'E(a) Display or retail sale of goods other than hot food'. Therefore, as both the existing office use and the proposed retail use are now both within the same use class (E) there is no requirement for planning permission for the change of use.

7.2.3 It must be noted that the principle of the change of use from office to retail has already been clarified/established under Lawful Development Certificate LBM Ref 21/P0162 - lawful development certificate issued in respect of the proposed re-classification from class B1 to class E. The change of use of the building from office to retail is already permitted and

is therefore not a material planning consideration for members of the planning committee to consider. Consequently, the principle of a retail use is acceptable and the application must be determined on the basis of the acceptability of the extension works, albeit that the extension is required to facilitate the Tesco Express use, and other physical changes to the site and the impact of those and not the retail use.

7.3 The impact of the design and appearance on the street scene

7.3.1 London Plan 2021 policy D3, Core Strategy policy CS14 and SPP DM D2 require developments to relate positively and appropriately to the siting, scale, proportions and massing of surrounding buildings and existing street patterns and which would enhance local context by delivering buildings and spaces that positively respond to local distinctiveness through their layout, orientation, scale, appearance and shape, with due regard to existing and emerging street hierarchy, building types, forms and proportions.

7.3.2 The site has an industrial character from both the office blocks and the more functional looking industrial units to the east of the site. The proposed extension would be single storey and set in from the side of the building such that it would not be visible from the street. The extension would be finished in brickwork to match existing, masonry construction with grey metal cladding dressed over parapet. The extension would have an associated enclosed service yard with hit and miss metal fencing enclosing it. That style of fencing enclosure reflects the existing site boundary fencing.

7.3.3 The proposed works are considered to be in keeping with the commercial nature of the industrial estate within which the site is located and are of a scale and design commensurate with the building and its proposed use. The proposal is therefore considered to respect the original building, surrounding area and is therefore considered to be acceptable.

7.4 The impact on neighbour amenity

7.4.1 SPP policy DM D2 and London Plan policy D3 require that proposals do not have a negative impact on neighbour amenity from loss of light, privacy, visual intrusion or increased disturbance and that people feel comfortable with their surroundings.

7.4.2 The proposals have generated objections on the grounds of amenity for future occupiers of the new flats on the basis of noise, anti-social behaviour and visual disturbance from the activity of the retail use and the proximity of the car parking area.

96 Church Road (previous Fosters' Auto Centre site)

7.4.3 The approved drawings show that the adjoining development of flats was designed to utilise as much of the site as possible and consequently the

flats closest to this application site were built within half a metre of the site boundary. All the ground floor habitable rooms in the closest flat would have primary windows directly overlooking the site. The flats at first and second floor levels would have the secondary windows in one of the bedrooms and in the combined lounge/kitchen/dining room facing the site whilst on the third floor the amenity terraces would face the site.

7.4.4 It would appear that that development was designed to utilise the existing boundary with the application site with some landscaping/planting. To mitigate the impact on the ground floor flats, the applicants have proposed a 1.5m high double slatted fence and 1.2m high planting along the boundary adjoining these neighbours. Additionally the associated advertising (subject of planning application ref 21/P2570) has been amended to reduce the illumination and visual intrusion it could cause. It is considered that these elements would assist in reducing the visual impact of car noise and headlights and it should be noted that the applicants could erect a 2m high solid boundary fence along the boundary without the need for planning permission.

7.4.5 With the upper level flats the windows are secondary to the main front and rear facing windows and consequently whilst there would be a level of overlooking this would be mitigated by the fact that they would be secondary to the main windows.

Boundary Business Court

7.4.6 The other business units within the estate are non-residential uses, therefore they do not have the same level of weight given to amenity value as that attached to residential uses which are protected by planning policy. Whilst there would be some additional noise and activity created by the proposal, given the commercial nature of the estate, location of the site on the periphery of the estate, the modest scale of the proposal and the nature of the retail activity, there would be no undue loss of amenity.

Noise

7.4.7 The commercial retail use of the site has the potential to generate noise disturbance from two main sources, the three condenser units and a refrigeration unit that are proposed within the proposed ground floor plant area at the north-eastern corner of the site and the general noise of customers, staff, car and delivery movements.

7.4.8 In relation to the plant noise, the application was accompanied by a Plant Noise Assessment Report which assessed the potential impact of the plant on the closest residential occupiers which would be those in the flats to the north at 96 Church Road. The report found that total predicted noise levels associated with the proposed plant do not exceed the proposed noise limits.

7.4.9 The Council's Environmental Health team were satisfied with the findings of the report and have recommended a condition that the noise criteria shown in that assessment shall be implemented in full and maintained as a minimum standard whilst a post completion noise assessment shall be undertaken to demonstrate compliance and submitted to the LPA for approval.

7.4.10 Noise generated by general activity is more difficult to quantify but the Environmental Health Officer has recommended opening hours of 7am to 11pm to limit impact on neighbouring amenity. This can be controlled via planning condition.

Conclusion

7.4.11 In view of the factors above and subject to the imposition of suitable conditions, the proposals are not considered to be materially harmful to the amenity of neighbouring such as to warrant a refusal of planning permission and the proposals are consequently recommended for approval.

7.5 Parking, servicing deliveries and access.

7.5.1 Core Strategy Policy CS 20 is concerned with issues surrounding pedestrian movement, safety, serving and loading facilities for local businesses and manoeuvring for emergency vehicles as well as refuse storage and collection.

7.5.1 The proposals generated objections on the grounds of a harmful impact on parking and traffic movements. The new car park for the store would be creating 12 new customer parking bays, reduced from an initial figure of 14 in order to retain more trees. The new extension would see a reduction in overall estate parking by 8 bays although these were bays allocated to the previous ground floor use.

Car Parking

7.5.2 The application was submitted with a car parking assessment which has been considered by the Council's Transport Officer. Car parking is currently privately managed with a set number of bays allocated to companies on the site as part of their lease agreements. None of the bays are currently allocated to the ground floor of Sandham House as Tesco will be taking this space and they will have their own car park which is to be built as per the application plans. In terms of the first floor at Sandham House the Estate management company have enough spaces to the side of unit 5 and unit 1 to offer to a new tenant wishing to take the first floor space.

7.5.3 The tenant of Southerton House will lose 2 spaces where the new extension will be built but the Estate management company have the ability in the lease to move those spaces, i.e. they only have a right to

use a set number of spaces and they will simply be allocated spaces opposite which are currently not in use or allocated to any other tenants on the estate.

- 7.5.4 The applicant has confirmed that the other bays to be removed as part of the proposed extension were allocated to the previous ground floor use. Therefore, there would be no loss of parking for the other units on the estate and therefore no material changes to parking allocation on the estate. The Councils Transport Planner has raised no objection to the proposal subject to conditions.
- 7.5.5 Staff parking would be discouraged and be subject of an internal company travel plan. Staff on site would be responsible for the management of this private car park. This management system would be reviewed six months after the opening of the new store and thereafter as necessary if it is apparent that the car park management system is not meeting its objective. As a commercial enterprise it would be to the applicants' benefit that parking was efficiently and effectively managed in order not to discourage customers in cars.
- 7.5.6 Objectors stated the proposals would lead to overflow parking elsewhere on the estate however the only access to the store and the car park would be through the northern site access which will be shut off from the rest of the estate by gates along the eastern site boundary.

Servicing

- 7.5.7 Access to the premises for servicing of the retail use, with deliveries and refuse collections etc, would be through the associated entrance from Church Road with a hashed area for the delivery vehicle set aside to the rear of the building and extension.

Cycle Parking

- 7.5.8 Core Strategy policy CS18 Active Transport and London Plan policy T5 Cycling encourage the provision of adequate amounts of secure sheltered cycle storage facilities. The proposals include space for secure cycle storage in the front of the building, 18 covered spaces in total and it is recommended that details of this proposed store be secured by condition.

Pedestrian Access

- 7.5.9 SPP policy DM D2 requires that developments provide safe and secure access. Given that currently there is virtually no footpath to the south of the site the proposal has been amended to include the provision of a new footpath to the south of the site which whilst not the standard 2m width would be sufficiently wide to allow a buggy to pass along to both the proposal site as well as the flats to the north. The proposed highway works are therefore seen as a benefit to the proposal as well as a wider

public benefit by providing enhanced pedestrian environments to the application site and beyond. The highway improvements can be secured via a S106 agreement. Details relating to whether the footpath is adopted by the Council will be agreed as part of the S106 process (discussion between the applicant and Councils Highway Section). In addition, the S106 agreement will ensure that the applicant is responsible for the costs of the works.

Members Comments

7.5.10 Members had raised a number of points of concern in relation to parking and road safety which have been put to the applicant and their comments sought;

7.5.11 Members queried;

1. A proper safety study for the provision of much wider/safer access and indeed provision of pavements to access the site.
2. Study of safety given that the store will be positioned opposite a very busy junction with Phipps Bridge Road where buses turn.
3. The inclusion of provision of safe crossing to the site, traffic lights or zebra crossing. The estates opposite have many children and mums with buggies, their safety is paramount given the proximity of two primary schools.

The applicants commented ;

In terms of items 1-3, it's important to stress that the applicant has already committed to providing substantial improvements to the footpath to the south of the site, which will provide good quality access for mums with buggies, children and vulnerable people. 100m to the south of the footpath improvements is an existing pedestrian crossing. Moreover, there are two further crossing points, one which is adjacent to Sandham House and the other which is 50m to the north of the site. It's also worth noting that Church Road is restricted to 20 mph. On the basis of the proposed enhancements together with the existing network around the site, it is considered the pedestrian environment is sufficiently safe.

In addition, the planning application was accompanied by a Transport Statement which covered pedestrian access / infrastructure which has been reviewed by the highway authority who considered the arrangement provides a suitable level of safety. That being said, and as a compromise, the applicant / Tesco would be willing to undertake an independent Road Safety Audit for the proposed footway improvements, which could be secured by way of a planning condition.

- 7.5.11 Members questioned the possibility of Preventative measures for "fly parking of cars" in front of the proposed store e.g. zigzag lines in tandem with a zebra crossing.

The applicants commented;

In terms of measures to stop vehicles parking on Church Road, this already has double yellow lines which, I suspect, would be a sufficient enough deterrent in itself. If further evidence is required of how this can be effectively managed, we could look into incorporating measures into a management plan.

- 7.5.12 Members asked about Cameras to catch poor parking practices.

The applicant responded;

'There will be comprehensive CCTV coverage of Tesco's dedicated car park but not on Church Road. As confirmed above, we consider the double-yellow lines would act as a deterrent for customers parking on the street'.

It should be noted that Tesco would not be legally permitted to enforce poor parking practices outside of the confines of their site but CCTV may have a deterrent value.

- 7.5.13 In relation to a question about Fast charge EV points the applicant has noted that the proposal comprises 2 EV charging points and the applicant/Tesco is happy for these to be 'fast charge' EV points.

7.6 **Trees**

- 7.6.1 London Plan policy G7, Core Strategy policy CS 13 and Sites and Policies Plan policy DM O2 all seek to protect against the loss of trees. Initially the application included a 14 space car park which necessitated the loss of 7 trees. Officers shared objector concerns regarding impact on trees which, although located within an industrial estate, included attractive specimens that improved the visual amenity of the wider area.

- 7.6.2 It should be noted that none of the trees on the site are protected, however the applicant has agreed to retain some trees and provide new planting following discussions with officers which is welcomed. Unfortunately two trees were damaged in the storms earlier this year and were consequently removed (noted permission would not have been required for removal in any event).

- 7.6.3 Along the northern boundary with the new flats there are currently four trees, T1 to T4. It would not be possible to retain all four trees and provide the parking bays. Following further discussions, the proposal would now move the parking bays to the west allowing for T1 & T4 to be retained. Of the four trees T4, a mature Rowan, is the best of the trees with the

other three being of low amenity view. To mitigate the loss of the two trees it is proposed to provide two new semi mature trees between the road and the parking bays.

7.6.4 T11 and T12 are attractive Silver Birch trees on the opposite side of the car park. The changes to the road surfaces have meant that the layout has required changes to facilitate the refuse store and provide suitable and sufficient space for the trees to thrive. In order not to reduce water access it is proposed that the refuse store be based on a grasscrete system that will allow for water to drain down whilst still being fit for purpose and the store would be enclosed within a lightweight fence arrangement supported on four wooden posts so as to minimise the impact on the roots. Further drawings have been supplied to demonstrate that the refuse store can fit within/under the canopy area without harming the trees. Further drainable space would be provided by reducing the amount of hardstanding in the office entrance area and providing a wider strip of greenery next to a reduced width footpath to the offices.

7.6.5 A further two new trees would be provided on the Church Road elevation and the advertising totem repositioned so as not to conflict with the closest retained tree with an additional new tree being added next to it. A total of five new trees are proposed.

7.6.6 Consequently, subject to the imposition of suitable conditions, the proposals will retain a number of attractive trees on site and provide four new specimens and are therefore considered to comply with relevant policies in this regard.

7.7 **Anti-social behaviour**

7.7.1 The application generated a number of objections suggesting that the proposals would lead to significant increases in anti-social behaviour particularly in relation to late night drinking. There is no evidence to suggest that this would be any more of an issue at these premises than it would at any other off licence or convenience store in Mitcham that had an off sales licence.

7.7.2 With regards to managing nuisance within the site, Tesco has run this store through their risk matrix and the store would get a minimum of 80 hours of guarding based on its risk positioning, meaning the store would have a security officer every night as a bare minimum. The MET police has also not raised any objection to the proposed development subject to conditions.

7.8 **Impact on the operation of the industrial estate**

7.8.1 There have been objections that the proposals would have a negative impact on the operation of the industrial estate for other businesses.

7.8.2 Royal Mail submitted an objection that the proposals would negatively impact their operations and the movement of their larger vehicles. As their premises are at the far end of the estate a site visit was conducted with a manager who confirmed that their vehicles used the southern estate access and not the northern one which is normally kept locked and consequently he could see no conflict between their use of the site and that of the proposed retail use.

7.8.3 Objectors have stated that companies were leaving the estate because of the proposals. The applicants have confirmed that only one tenant has exercised their lease break clause, but this was due to them needing to downsize.

7.9 Refuse

7.9.1 Core Strategy policy CS 17 relates to waste management. The applicant has indicated the provision of a refuse store within a dedicated enclosure near the store and away from residential properties which given the nature of the operation would be emptied/serviced on a more regular basis than would be the current situation.

7.9.2 The refuse store design has been amended to be a lighter structure to minimise impact on trees.

8. Sustainable design and construction.

8.1 New buildings must comply with the Mayor's and Merton's objectives on carbon emissions, renewable energy, sustainable design and construction, green roofs, flood risk management and sustainable drainage.

8.2 As a minor non-residential development of less than 500sqm of Gross Internal Floorspace there are no specific targets beyond current Building Regulations compliance.

9. Other matters

9.1 Objection letters refer to the proposals as a 'large scale' supermarket. The extension would facilitate a Tesco Express retail premises which with a trading area of less than 240sqm is small enough that it is not constrained by the same operating hours as a larger supermarket operation. This is explained within the applicants planning statement which states that a Tesco Express is designed as a 'top-up' facility to meet community retail needs. It has been proposed to predominantly serve an identified primary retail catchment of 500m from the site who will generally walk to the premises. The applicant states that there is limited convenience retail provision within the site's 500m walking catchment and it is for this reason that Tesco considers the Site to be suitable for an Express format store.

10. CONCLUSION

- 10.1 As a result of changes to the Use Classes Order the use of the existing building for retail purposes do not require planning permission and therefore the principle is considered acceptable. The proposed extension and its associated works along with the creation of the new car park require planning permission and are required to facilitate the use of the site as a Tesco Express store.
- 10.2 The extensions works are proposed to be single storey and situated to the rear of the building and are considered to be of an appropriate size and scale.
- 10.3 The car parking for customers has been amended and reduced to allow for the retention of more trees on site and is considered acceptable and with new security gates would not allow customer access to the rest of the estate.
- 10.4 The site would be operated with Tesco's security methodology and include appropriate CCTV provision along with on site security guarding measures in the evenings.
- 10.5 Through the use of appropriate boundary treatments and conditions relating to hours of operation and noise emissions from plant it is considered that the impact on neighbour amenity can be suitably mitigated.
- 10.6 The proposed provision of a new access footpath to the south of the site will improve accessibility and safety for persons walking on this side of Church Road, assisting access to this proposal as well as the flats at 96 Church Road.
- 10.7 In view of these considerations the proposals are considered to provide additional facilities for nearby residents, the extension/alterations are considered to be in keeping with the existing building and surrounding area with no undue impact on highway, trees or neighbour amenity to warrant refusal of planning permission. The development is therefore recommended for approval subject to conditions and a legal agreement for the new access path.

RECOMMENDATION

GRANT Planning Permission subject to:

The completion of a Legal Agreement covering the following heads of terms:-

1. Highway improvements (new pedestrian footpath) with associated costs.

2. The developer agreeing to meet the Councils costs of preparing, drafting and monitoring the Section 106 Obligations.

And Conditions

1. A1 Commencement of Development
2. A7 Approved Plans; Site location plan and drawings PL006-REV17, PL007-REV16, PL008-REV06, PL010-REV09, PL011-REV08, PL012-REV03 & PL016-REV02.
3. B1 External Materials to be Approved
4. C07 Refuse & Recycling – The retail use shall not commence until the refuse facilities shown on the approved plans are available and operational.
5. D10 External Lighting – Any external lighting shall be positioned and angled to prevent any light spillage or glare beyond the site boundary.
6. D11 Construction Times – No demolition or construction work or ancillary activities such as deliveries shall take place before 8am or after 6pm Mondays - Fridays inclusive, before 8am or after 1pm on Saturdays or at any time on Sundays or Bank Holidays.
7. F1 No development shall take place until full details of a landscaping and planting scheme has been submitted to and approved in writing by the Local Planning Authority.
8. B5 No development shall take place until details of all boundary walls, fences and gates are submitted in writing for approval to the Local Planning Authority.
9. B4 No development shall take place until details of the surfacing of all those parts of the site not covered by buildings or soft landscaping, including any parking, service areas or roads, footpaths, hard and soft have been submitted in writing for approval by the Local Planning Authority.
10. Cycle Parking – The use of the development hereby approved shall not commence until the secure cycle parking facilities for visitors to the development shown on the approved drawings have been fully implemented and made available for use prior to the first occupation of the development and thereafter retained for use at all times.
11. Car Parking - The use of the development hereby approved shall not commence until the car parking facilities, including disabled

and active vehicle charging bays, for the visitors to the development shown on the approved drawings have been fully implemented and made available for use prior to the first occupation of the development and thereafter retained for use at all times

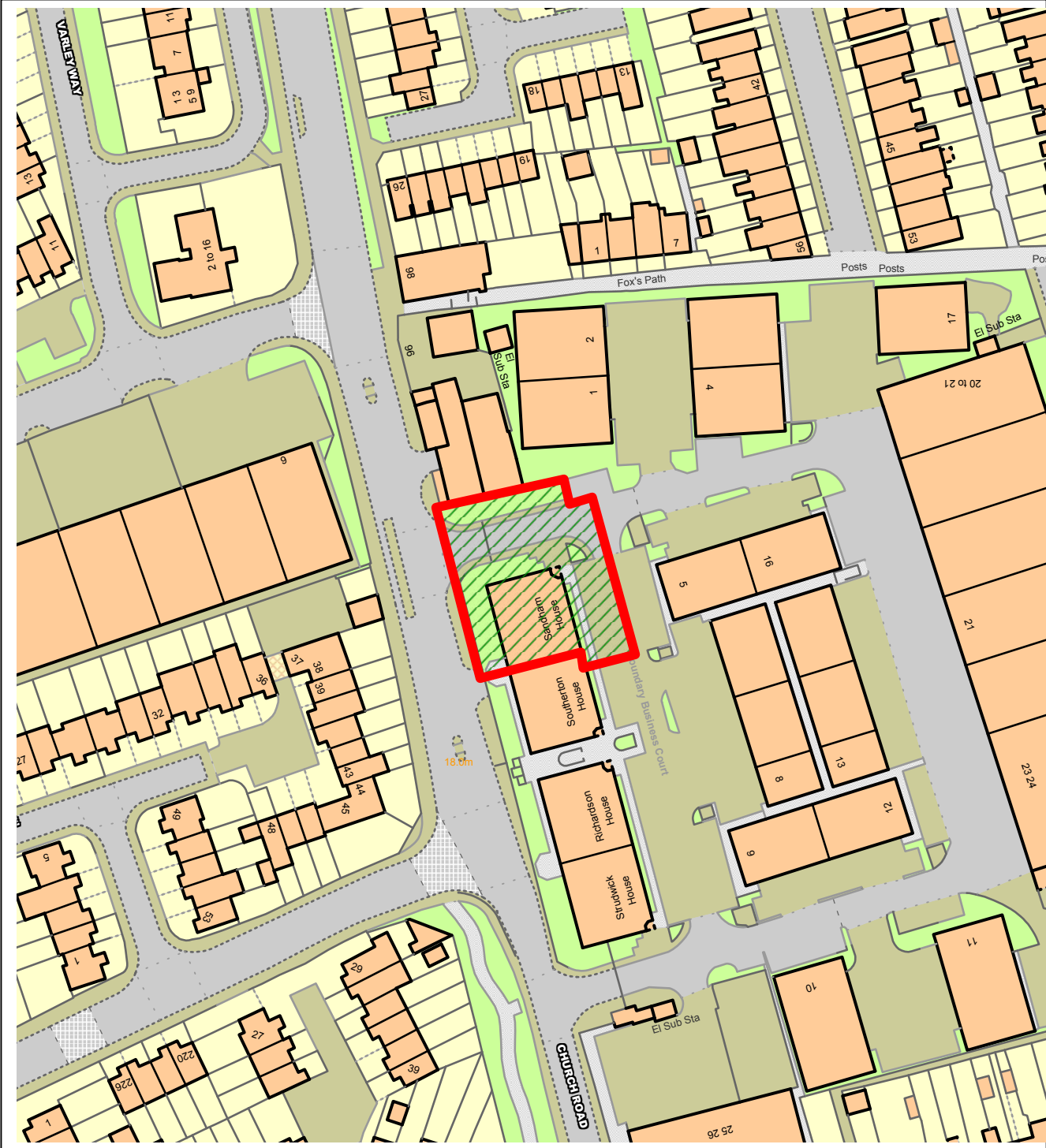
12. Due to potential impact on the surrounding locality from the development the noise criteria specified in the 'Auricl Acoustic Consultancy' Plant Noise Assessment Report dated 18 May 2021, Document Reference: R/PNA/1/210518 shall be implemented in full and maintained as a minimum standard. A post completion noise assessment shall be undertaken to demonstrate compliance and submitted to the LPA for approval.
13. The opening hours of the retail unit shall be limited to 7 am and 11pm.
14. F5 - Tree Protection
15. F8 - Site Supervision
16. Non Standard; Existing retained trees - The existing trees shown for retention on the approved plan drawing no. 'PL007 Rev.16' shall be retained and any trees that become seriously damaged, diseased, dying or dead, or removed within a period of 10 years from the completion of the development shall be replaced in the next planting season with another tree of the same/similar species and be a minimum 14 - 16cms girth at the time of planting.

Reason To protect and enhance biodiversity and arboricultural amenity.
17. Prior to the commencement of the retail use a comprehensive management plan and risk assessment including details of CCTV, security and crime prevention methodology and relevant staff training shall be submitted to and approved in writing by the Local Planning Authority.

Reason; To promote a safe and secure environment for staff and visitors in accordance with policy DM D2 of the Merton Sites and Policies Plan 2014.
18. Any lighting installations should be assessed against the Institute of Lighting Professionals' "Guidance notes for the reduction of obtrusive light", relevant British Standards and codes of practice to prevent any light spillage or glare at residential premises.

19. Travel Plan
20. Road Safety Audit

NORTHGATE SE GIS Print Template



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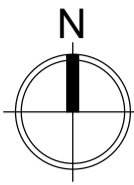


Site Location Plan
1:1250


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Survey information taken from
Greenhatch Topographical/Measured Survey
dated 10/02/2021 ref. 38114

Notes:



Site Area

Ownership Site Area 
 Approx. 24,165 sq m
 Approx. 260,112 sq ft
 Approx. 5.971 acres

Proposed Works Site Area 
 Approx. 1,122 sq m
 Approx. 12,087 sq ft
 Approx. 0.277 acres

06	Planning Issue	23.06.2021	ZG	HA
05	Draft Planning Issue	15.06.2021	JW	HA
04	Draft Planning Issue	01.06.2021	JW	HA
03	Scale bar added.	01.06.2021	JW	HA
02	Planning Issue	19.05.2021	JW	HA
01	Draft Planning Issue	18.02.2021	JW	HA
-	First Issue	15.02.2021	ZG	HA
Rev:	Notes:	Date:	Dwn:	Iss:
Suitability Code:				



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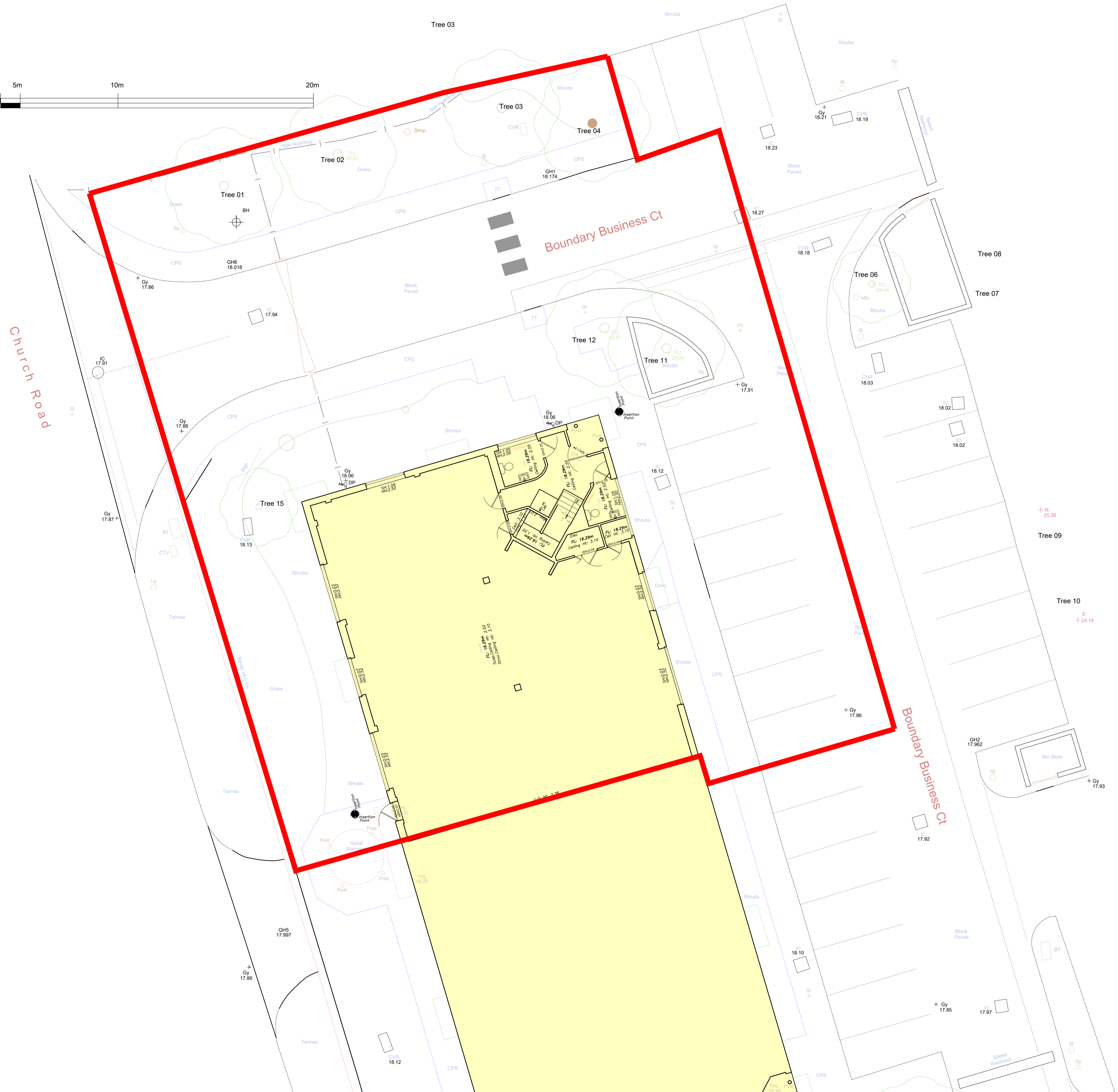
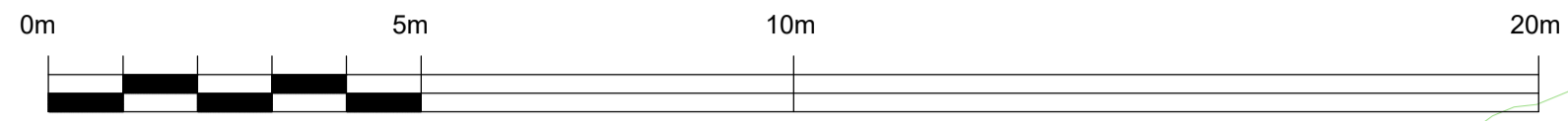
22c Leathermarket Street, London, SE1 3HP

Project:
Sandham House
Boundary Business Court,
92-94 Church Road, Mitcham, CR4 3TD

Drawing Title:
Site Location Plan

Project No: 00000	Scale @ A1 1: 1250 / 200	Revision: 06
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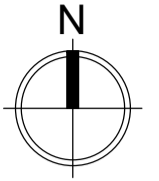
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PL001



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Notes:



Page 153

08	Trees Ammended	29.07.2022	JR	HA
07	Planning Issue - Exlg trees updated.	23.02.2022	jw	HA
06	Planning Issue - Exlg trees updated.	12.10.2021	jw	HA
05	Planning Issue	15.06.2021	jw	HA
04	Draft Planning Issue	02.06.2021	jw	HA
03	Scale bar added.	01.06.2021	jw	HA
02	Draft Planning Issue	19.05.2021	jw	HA
01	Draft Planning Issue	18.02.2021	jw	HA
-	First Issue	17.02.2021	ZG	HA
Rev:	Notes:	Date:	Dwn:	Iss:
Suitability Code:				



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ARCHITECTURE
22c Leathermarket Street, London, SE1 3HP

Project:
Sandham House
Boundary Business Court,
92-94 Church Road, Mitcham, CR4 3TD

Drawing Title:
Existing Site Plan
Existing Topo / Utilities Survey

Project No: P0035 / 21088	Scale @ A1/A3: 1: 100 / 1:200	Revision: 08
Drawing No: PL002		

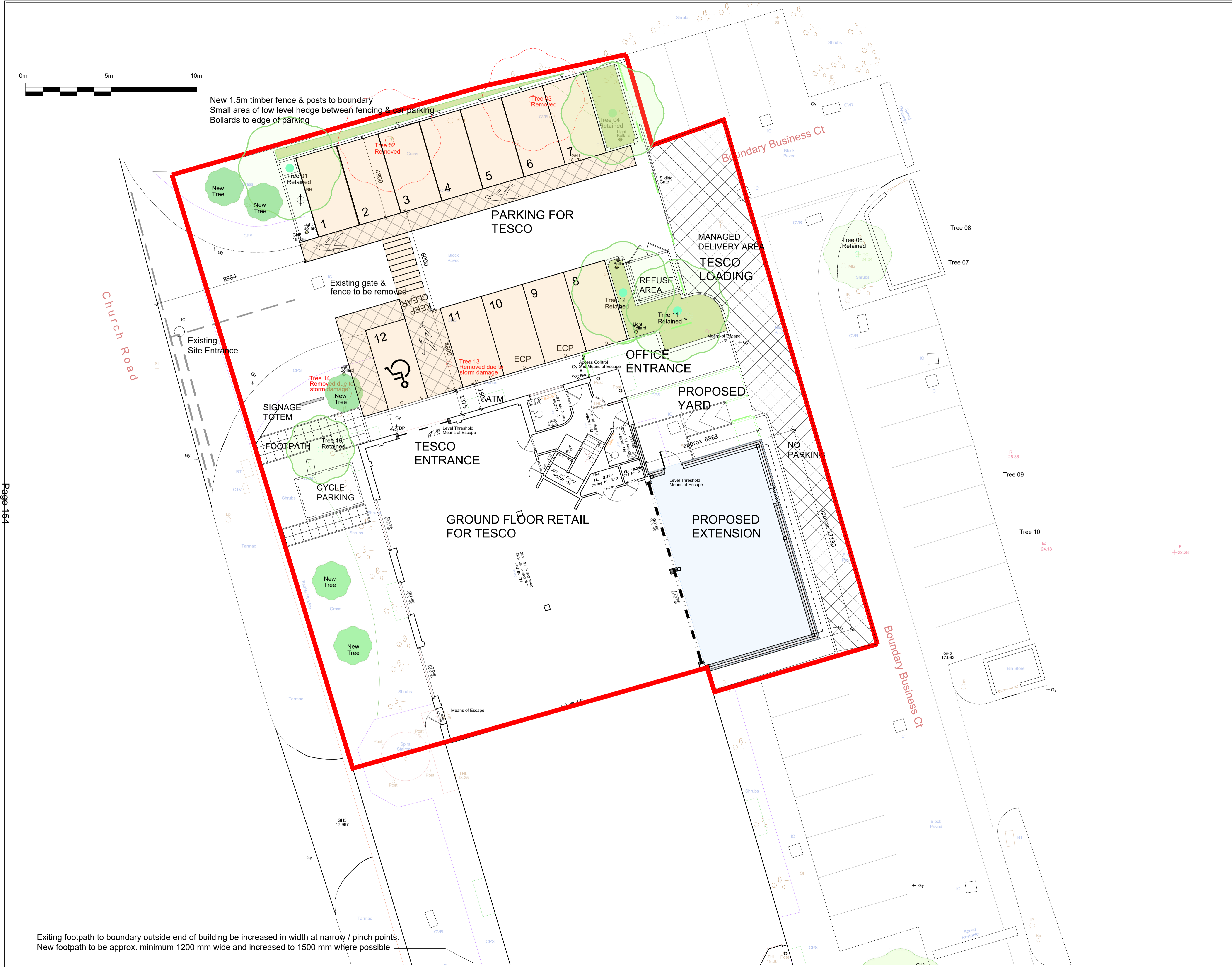


New 1.5m timber fence & posts to boundary
 Small area of low level hedge between fencing & car parking
 Bollards to edge of parking

Church Road

Boundary Business Ct

Boundary Business Ct



Exiting footpath to boundary outside end of building be increased in width at narrow / pinch points.
 New footpath to be approx. minimum 1200 mm wide and increased to 1500 mm where possible

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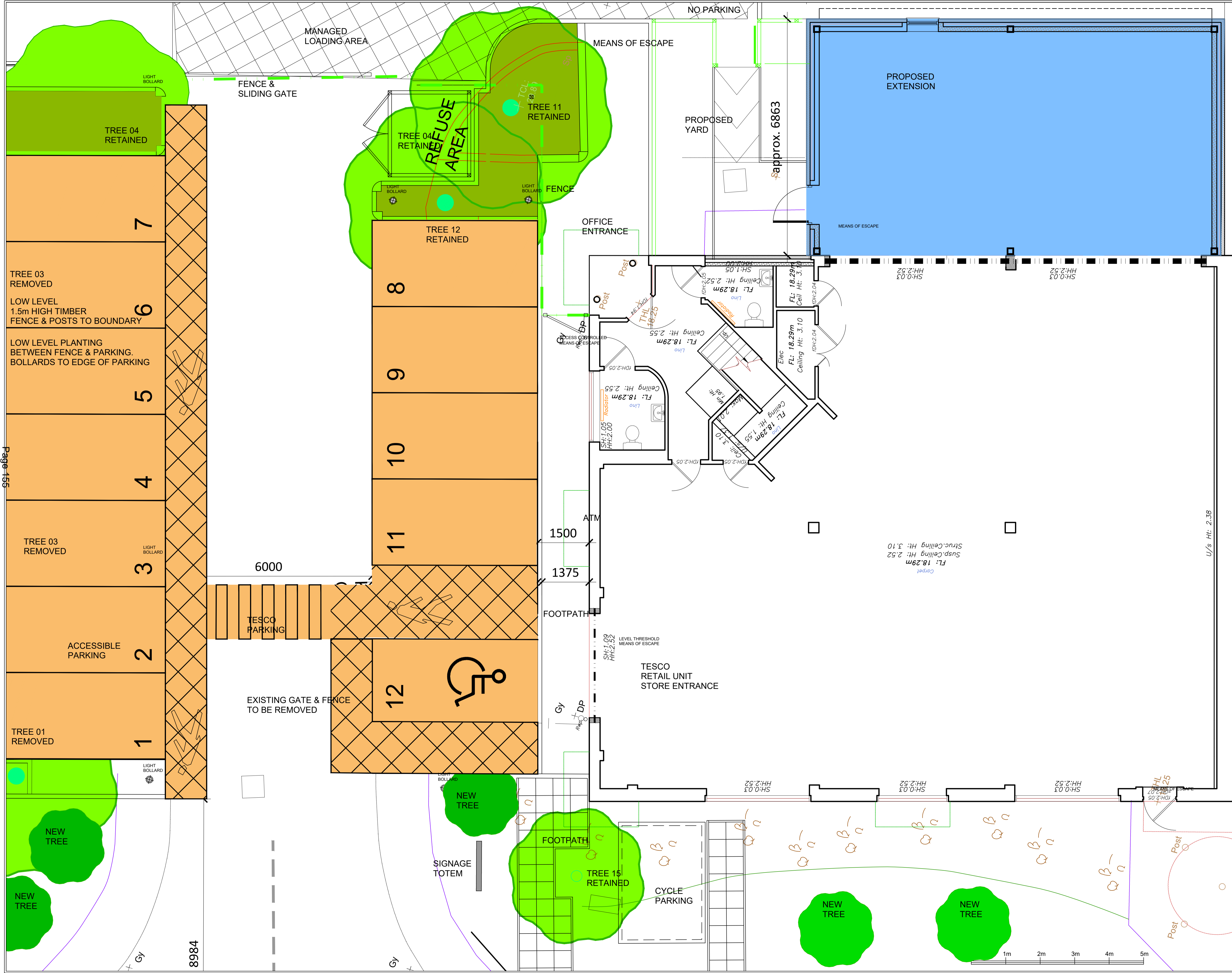


Rev	Notes	Date	Dwn	Iss
17	Planning Issue. Trees updated. Omit Tree 03 / Retain Tree 04 / Add new tree to entrance. Landscaping updated to suit Planning requirements.	26.07.22	jw	HA
16	Planning Issue. Storm damaged trees updated. Light bollards added to suit Planning requirements.	10.03.22	jw	HA
15	Planning Issue. Alternative parking layout to suit Planning requirements.	03.02.22	jw	HA
14	Planning Issue. Tree 014 retained to suit Planning requirements. 2 no. trees added to frontage.	03.02.22	jw	HA
13	Planning Issue. Tree 014 omitted to suit Planning requirements.	27.01.22	jw	HA
12	Planning Issue. Parking updated to suit 18.11.2021 Tesco requirements.		jw	HA
11	Planning Issue. Exlg. trees updated.	12.10.2021	jw	HA
10	Planning Issue. Parking amended.	07.09.2021	jw	HA
09	Planning Issue. Parking layout & trees amended.	06.09.2021	jw	HA
08	Planning Issue.	24.06.2021	jw	HA
07	Planning Issue.	15.06.2021	jw	HA
06	Draft Planning Issue.	02.06.2021	jw	HA
05	Site boundary amended.	02.06.2021	jw	HA
04	Scale bar added.	01.06.2021	jw	HA
03	Planning Issue. Site layout updated	27.05.2021	jw	HA
02	Parking layout updated	19.05.2021	jw	HA
01	Planning Issue	18.02.2021	jw	HA
-	First Issue	18.02.2021	ZG	HA



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 22c Leathermarket Street, London, SE1 3HP
 Project:
 Sandham House
 Boundary Business Court,
 92-94 Church Road, Mitcham, CR4 3TD
 Drawing Title:
 Proposed Site Plan

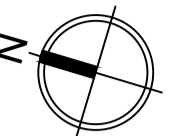
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Drawing No: PL006		



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Survey information taken from
Greenhatch Topographical/Measured Survey
dated 10/02/2021 ref. 38114

Notes:



16	Planning Issue. Trees updated. Omit Tree 03 / Retain Tree 04 / Add new tree to entrance. Landscaping amended to suit Planning requirements.	26.07.2022	jw	HA
15	Planning Issue. Storm damaged trees updated. Light bollards added to suit Planning requirements.	10.03.2022	jw	HA
14	Planning Issue. Alternative parking layout to suit Planning requirements.	03.02.2022	jw	HA
13	Planning Issue. Tree 014 retained to suit Planning requirements. 2 no. trees added to frontage	03.02.22	jw	HA
12	Planning Issue. Tree 014 omitted to suit Planning requirements.	27.01.2022	jw	HA
11	Planning Issue. Parking updated to suit Tesco requirements.	18.11.2021	jw	HA
10	Planning Issue. Extg trees updated.	12.10.2021	jw	HA
09	Planning Issue. Parking amended.	07.09.2021	jw	HA
08	Planning Issue. Car Parking & trees amended	06.09.2021	jw	HA
07	Planning Issue. Hatching Amended	25.06.2021	ZG	HA
06	Planning Issue. Car Parking & trees amended	02.06.2021	jw	HA
05	Site boundary amended.	02.06.2021	jw	HA
04	Scale bar added.	01.06.2021	jw	HA
03	Planning Issue. Refuse area updated	27.05.2021	jw	HA
02	Layout updated	19.05.2021	jw	HA
01	Draft Planning Issue	18.02.2021	jw	HA
-	First Issue	18.02.2021	ZG	HA
Rev:	Notes:	Date:	Dwn:	Iss:
Suitability Code:				

Client:

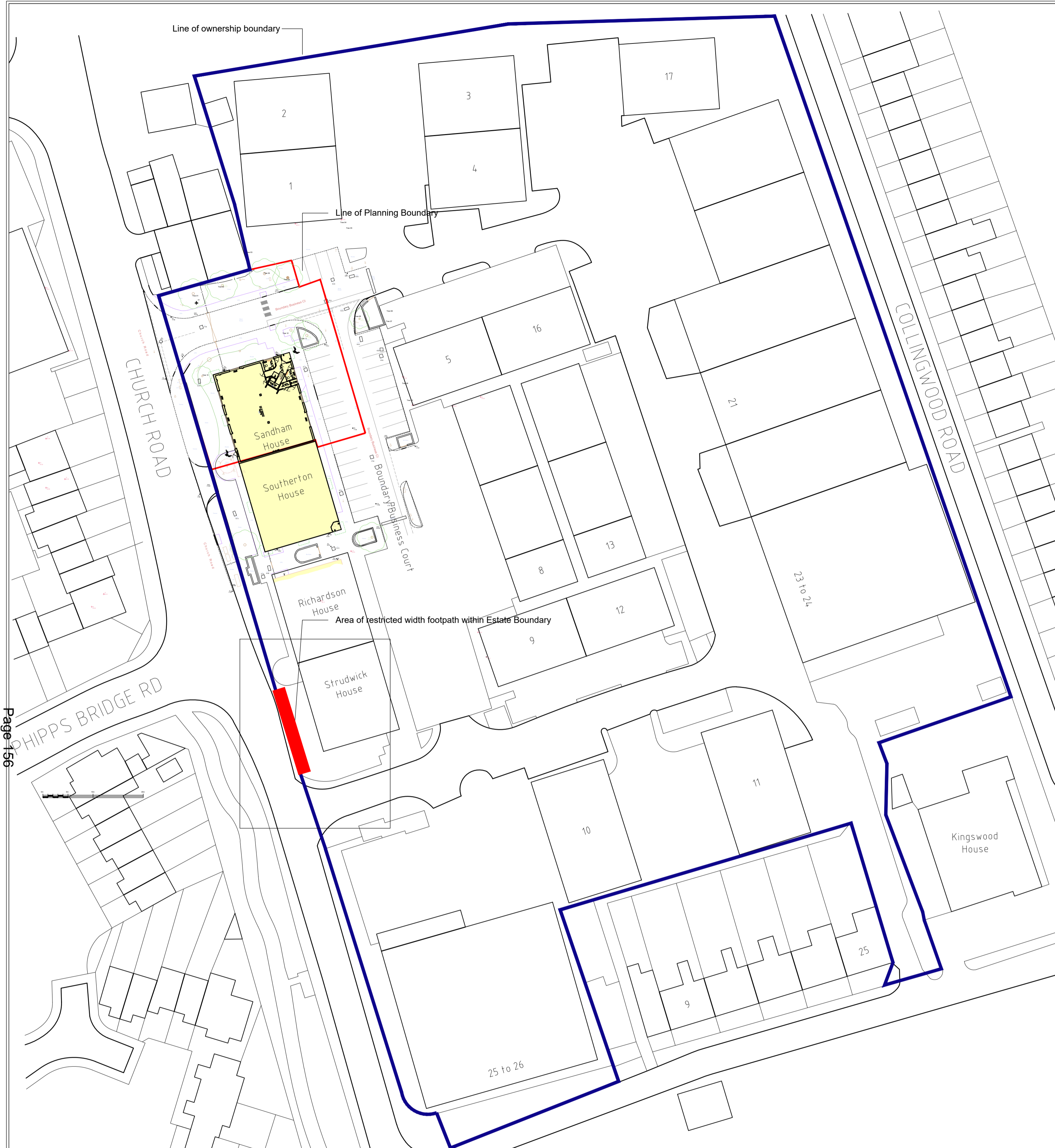


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22c Leathermarket Street, London, SE1 3HP

Project:
Sandham House
Boundary Business Court,
92-94 Church Road, Mitcham, CR4 3TD

Drawing Title:
Proposed Ground Floor Plan
Proposed Single Storey Extension

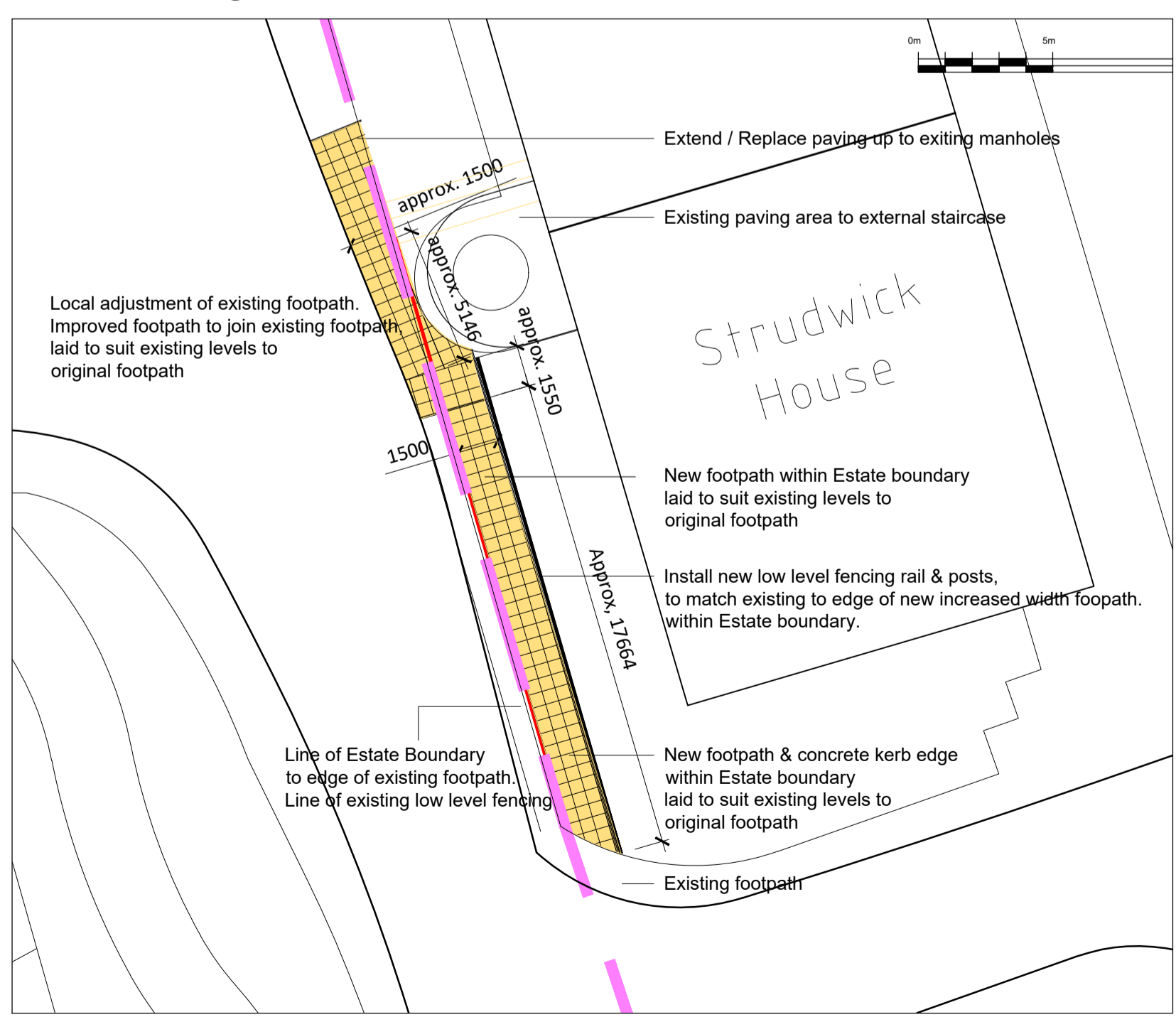
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Drawing No: PL007		



01 Existing Estate Site Plan
1:500 @ A1

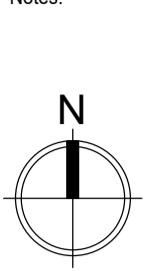


02 Proposed Estate Site Plan (Part)
1:500 @ A1



03 Proposed Footpath Improvements
1:200 @ A1

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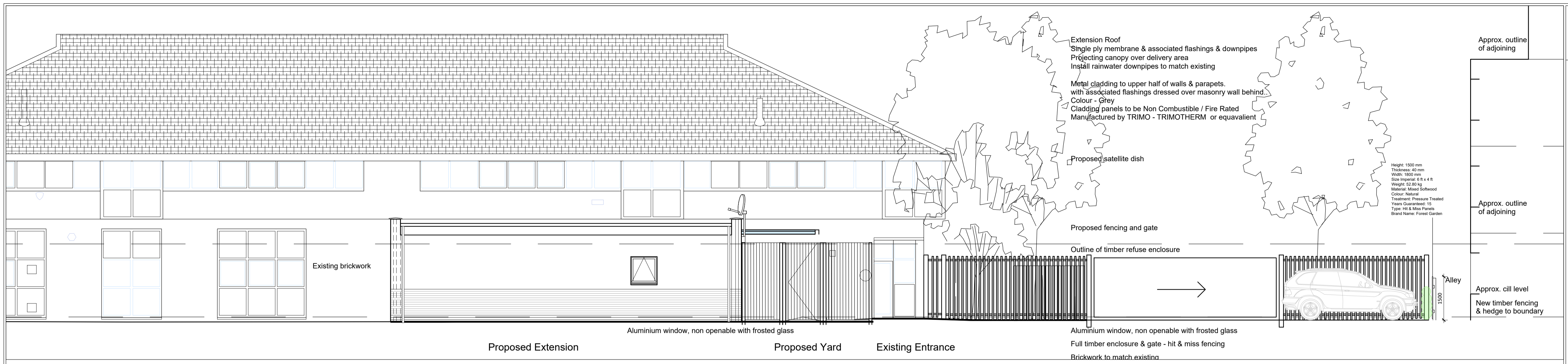


Rev:	Notes:	Date:	Dwn:	Iss:
06	Trees amended	29.07.2022	JR	HA
05	Planning Issue Trees & car parking & external works updated to suit Planning requirements.	26.07.2022	jw	HA
04	Planning Issue Trees & car parking & external works updated.	10.03.2022	jw	HA
03	Planning Issue Car parking layout & external works	03.03.2022	jw	HA
02	Planning Issue Footpath works updated	03.03.2022	jw	HA
01	Planning Issue	27.01.2022	jw	HA



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22c Leathermarket Street, London, SE1 3HP
Project:
Sandham House
Boundary Business Court,
92-94 Church Road, Mitcham, CR4 3TD
Drawing Title:
Existing Estate Site Plan
Proposed Estate External Works

Project No: P0035 / 21088	Scale @ A1/A3: 1: 500 / 1:200	Revision: 06
Drawing No: PL008		



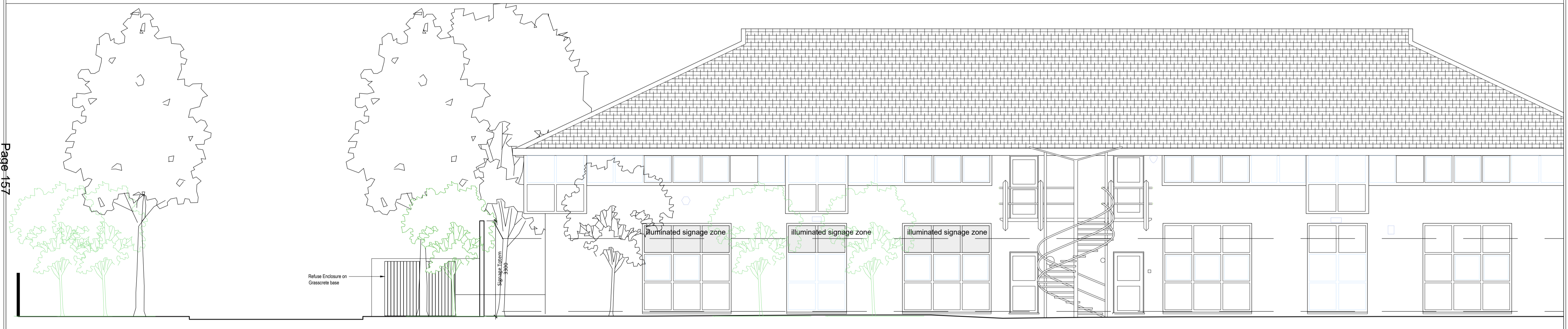
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Notes:

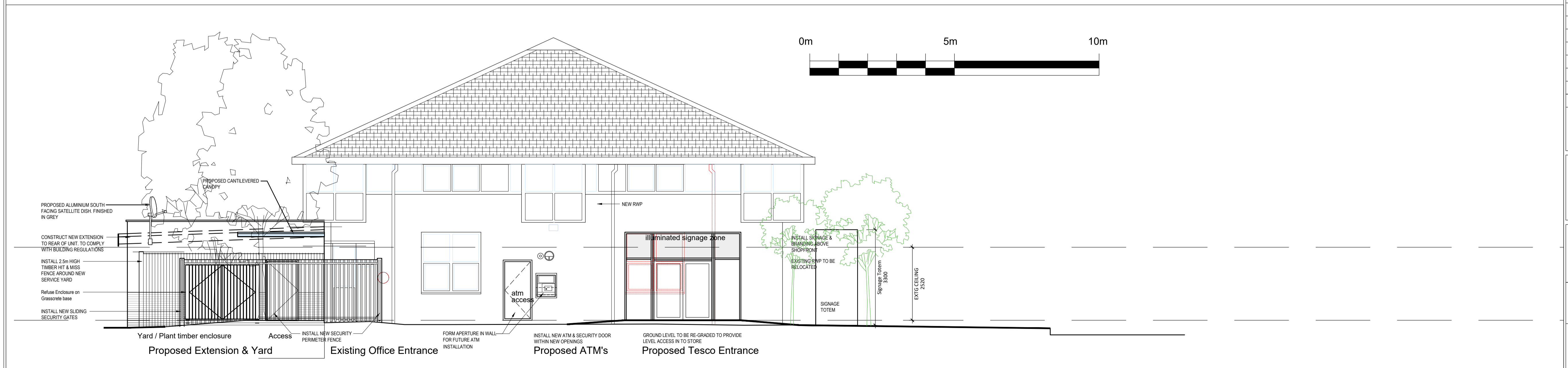
01 Proposed Elevation

1:150 @ A3



02 Proposed Elevation

1:150 @ A3



03 Proposed Elevation

1:150 @ A3

09	Refuse enclosure added	28.07.2022	JR	HA
08	Planning Issue. Trees updated.	26.07.2022	JW	HA
07	Planning Issue. Totem Sign added	18.11.2021	JW	HA
06	Planning Issue. Elevation Amended	25.06.2021	ZG	HA
05	Planning Issue. Scale bar added.	01.06.2021	jw	HA
04	Planning Issue	19.05.2021	jw	HA
03	Labels Amended	24.02.2021	ZG	HA
02	Planning Issue	22.02.2021	jw	HA
01	Draft Planning Issue	18.02.2021	jw	HA
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Rev:	Notes:	Date:	Dwn:	Iss:
Suitability Code:				



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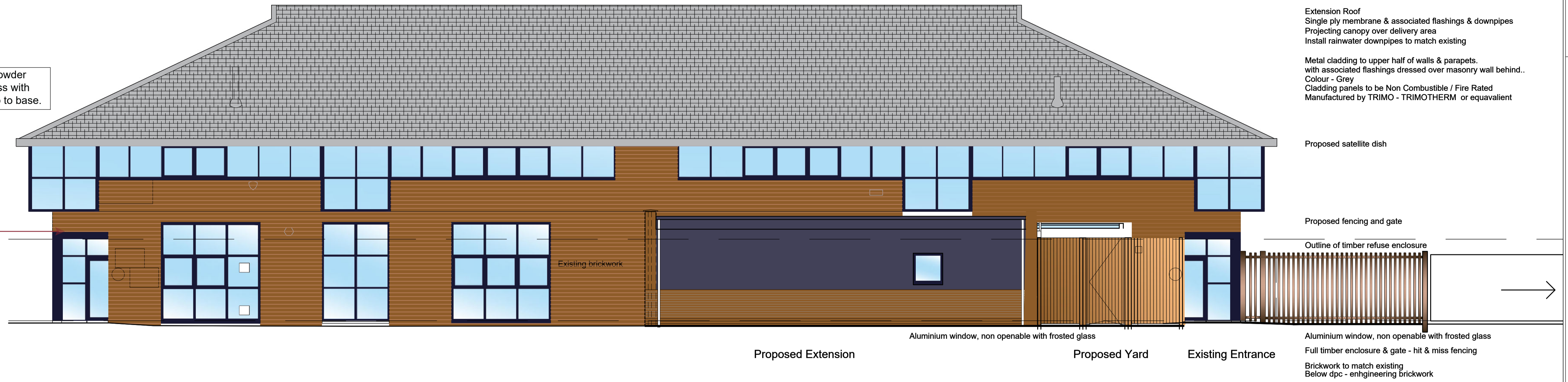
Project:
Sandham House
Boundary Business Court,
92-94 Church Road, Mitcham, CR4 3TD

Drawing Title:
Proposed Elevations

Project No: P0035 / 21088	Scale @ A1/A3: 1: 75 / 1:150	Revision: 09
Drawing No: PL010		

New fascia panel in Aluminium powder coated Blue RAL 5005, 30% Gloss with Brown Arizona Oak woodtex strip to base.

SIGN 4
6420x650mm



Extension Roof
Single ply membrane & associated flashings & downpipes
Projecting canopy over delivery area
Install rainwater downpipes to match existing

Metal cladding to upper half of walls & parapets.
with associated flashings dressed over masonry wall behind.
Colour - Grey
Cladding panels to be Non Combustible / Fire Rated
Manufactured by TRIMO - TRIMOTHERM or equivalent

Proposed satellite dish

Proposed fencing and gate

Outline of timber refuse enclosure

Aluminium window, non openable with frosted glass
Full timber enclosure & gate - hit & miss fencing
Brickwork to match existing
Below dpc - engineering brickwork

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Greenhatch Topographical/Measured Survey
dated 10/02/2021 ref. 38114

Notes:
Signage
Refer to Tesco signage drawings for details of
specific signs

This drawing is an extract of the Tesco drawing
Proposed Elevation Signage
Drawing no. 03-MITC-03 B

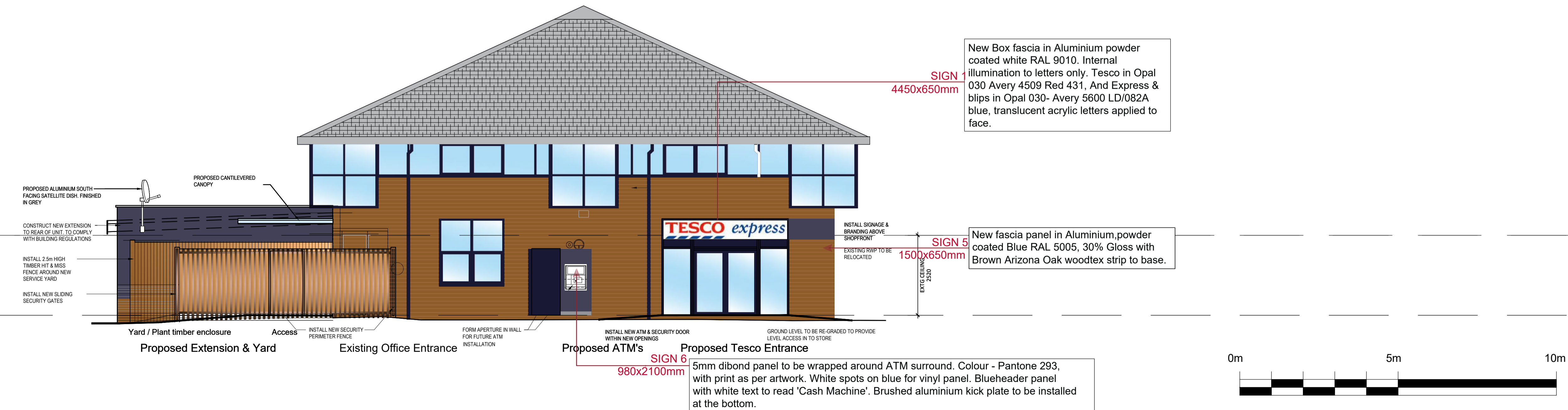
01 Proposed Rear Elevation

1:150 @ A3



02 Proposed Front Elevation

1:150 @ A3



SIGN 1
4450x650mm

New Box fascia in Aluminium powder coated white RAL 9010. Internal illumination to letters only. Tesco in Opal 030 Avery 4509 Red 431, And Express & blips in Opal 030- Avery 5600 LD/082A blue, translucent acrylic letters applied to face.

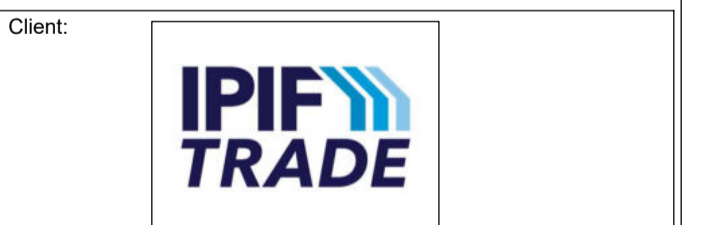
SIGN 5
1500x650mm

New fascia panel in Aluminium powder coated Blue RAL 5005, 30% Gloss with Brown Arizona Oak woodtex strip to base.

SIGN 6
980x2100mm

5mm dibond panel to be wrapped around ATM surround. Colour - Pantone 293, with print as per artwork. White spots on blue for vinyl panel. Blueheader panel with white text to read 'Cash Machine'. Brushed aluminium kick plate to be installed at the bottom.

01 First Issue	17.06.2021	ZG	HA
Rev:	Notes:	Date:	Dwn: Iss:
Suitability Code:			

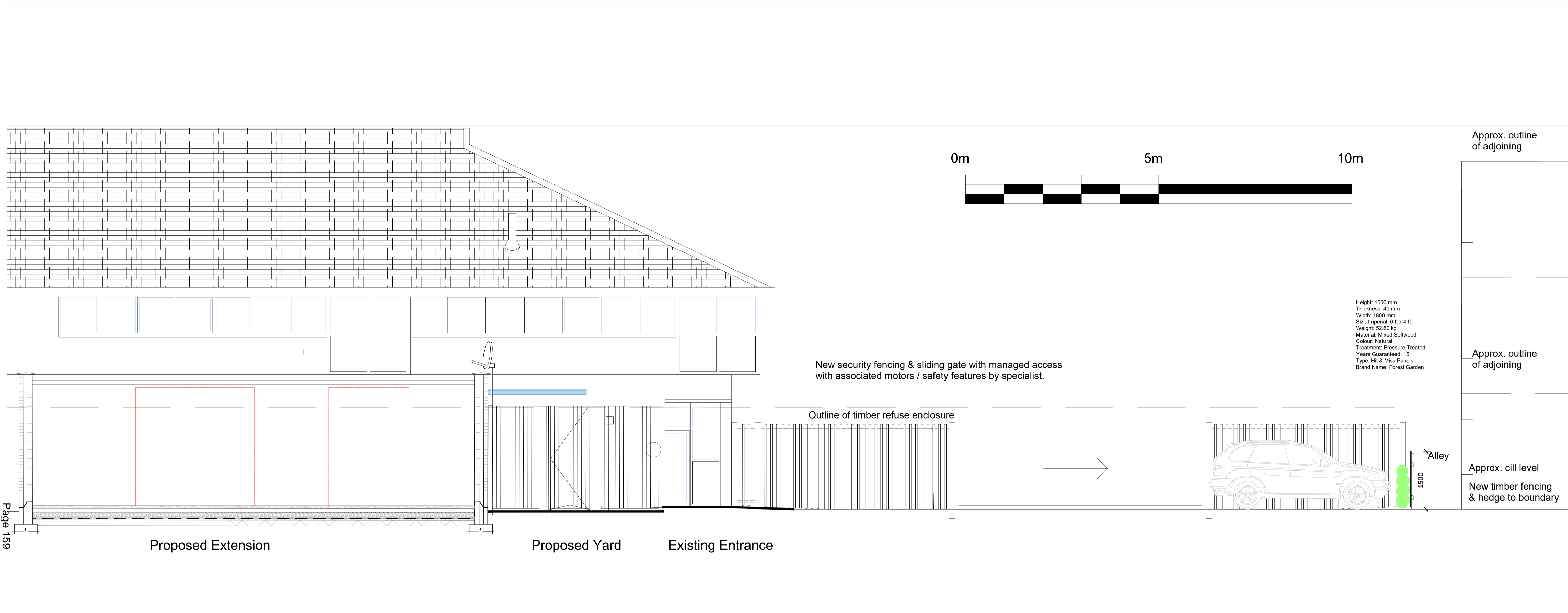


hale
ARCHITECTURE
22c Leathermarket Street, London, SE1 3HP
Project:
Sandham House
Boundary Business Court,
92-94 Church Road, Mitcham, CR4 3TD
Drawing Title:
Proposed Elevations
Proposed Signage

Project No: 00000	Scale @ A1/A3: 1: 75 / 1:150	Revision: 02
Drawing No: PL012		

03 Proposed Side Elevation

1:150 @ A3



Page 159

01 Proposed Fencing Elevation to Yard Area

1:50 @ A1



02 - Example of 1.2m high mature hedge



03 - Example of Double slatted fence panel



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Survey information taken from Greenhatch Topographical/Measured Survey dated 10/02/2021 ref. 38114			
Notes:			
Rev:	Notes:	Date:	Dwn: Iss:
-	First Issue	17.02.2021	ZG HA
01	Draft Planning Issue	24.02.2021	jw HA
02	Planning Issue	19.05.2021	jw HA
03	Draft Planning Issue. Scale bar added.	01.06.2021	jw HA
04	Planning Issue	25.06.2021	ZG HA
05	Vehicle and Fence added	10.03.2022	JR HA
06	Boundary fence details and hedge example added	11.03.2022	JR HA
07	Fence panel height amended	11.03.2022	JR HA
08	Title block amended	14.03.2022	JR HA

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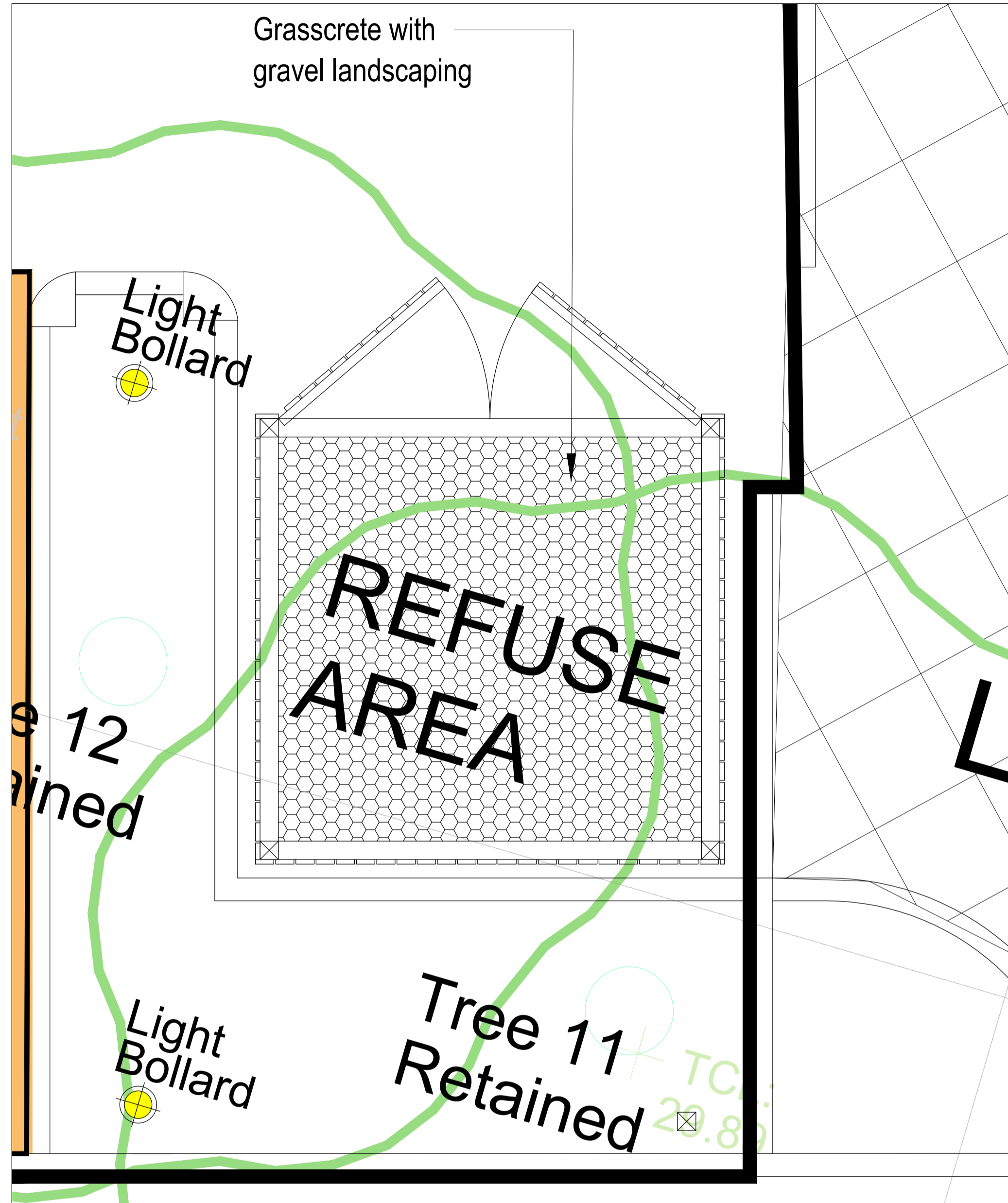
Client:
IPIFY

Issue Purpose:

Project:
Sandham House
Boundary Business Court,
92-94 Church Road, Mitcham, CR4 3TD

Drawing Title:
**Proposed Fencing Elevation to Yard
Proposed Section**

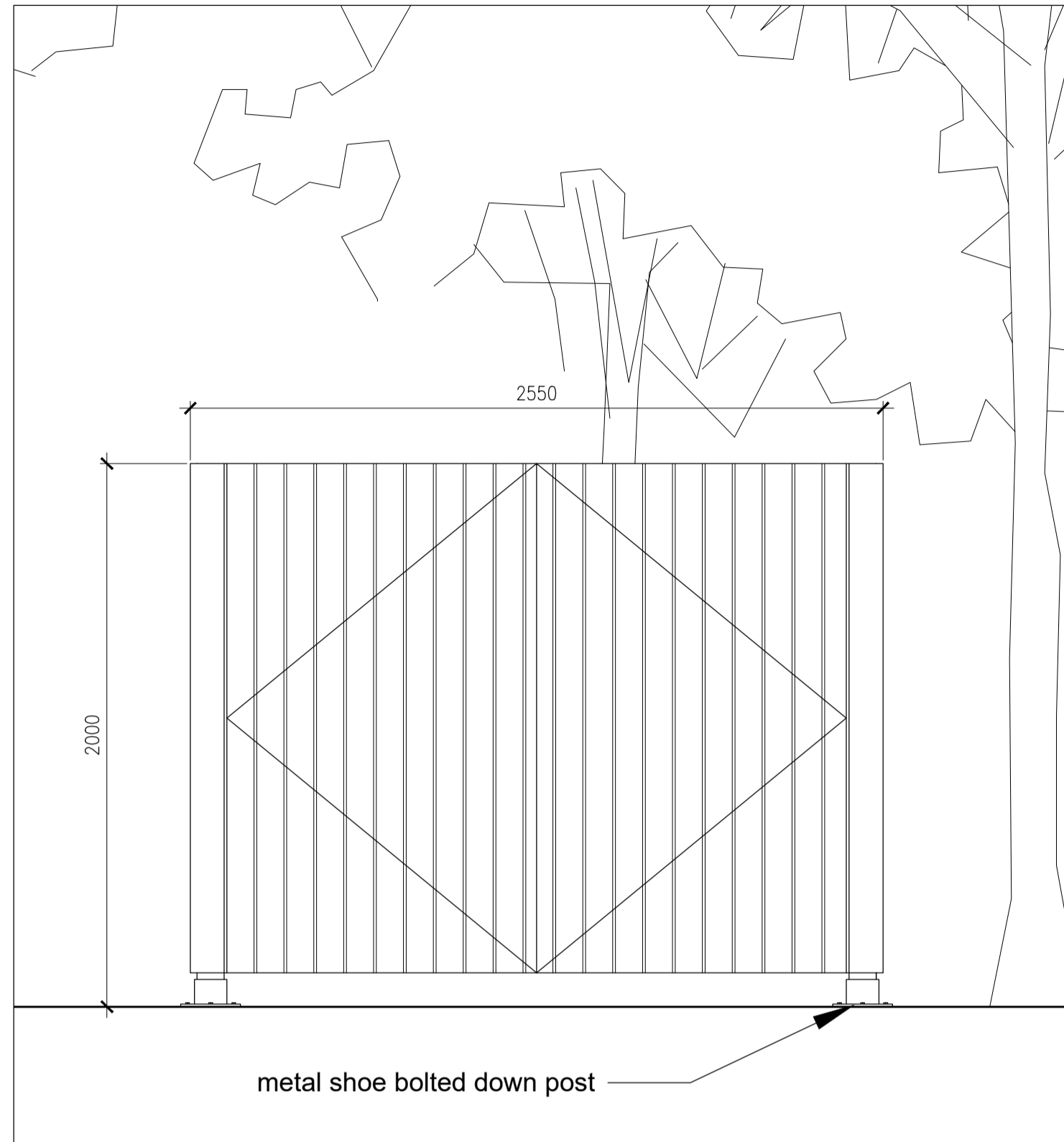
Project No: P0035 / 21088	Scale @ A1/A3: 1: 50 / 1:100	Revision: 08
Drawing No: PL011		



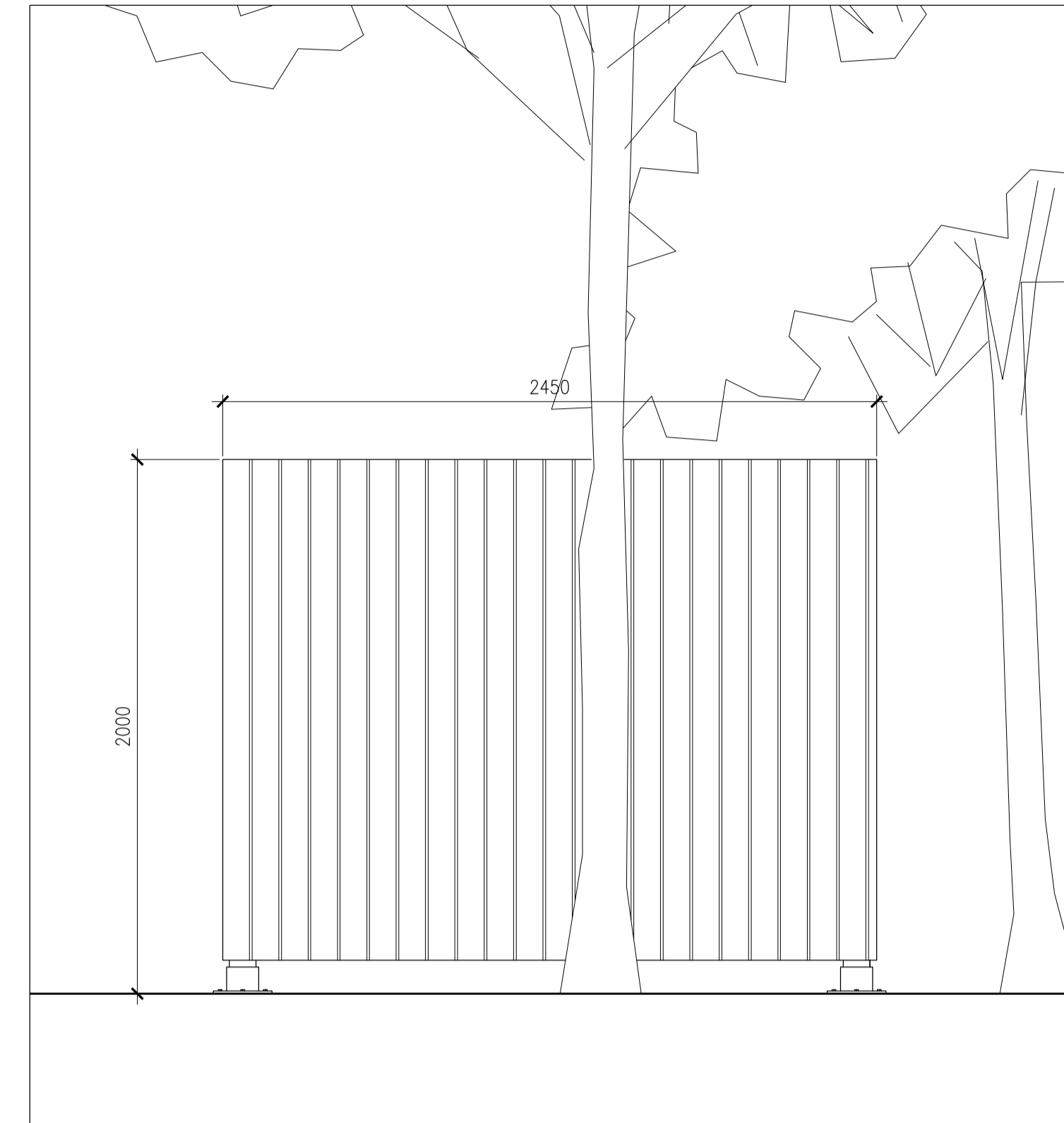
01 Proposed Plan
1:40 @ A3



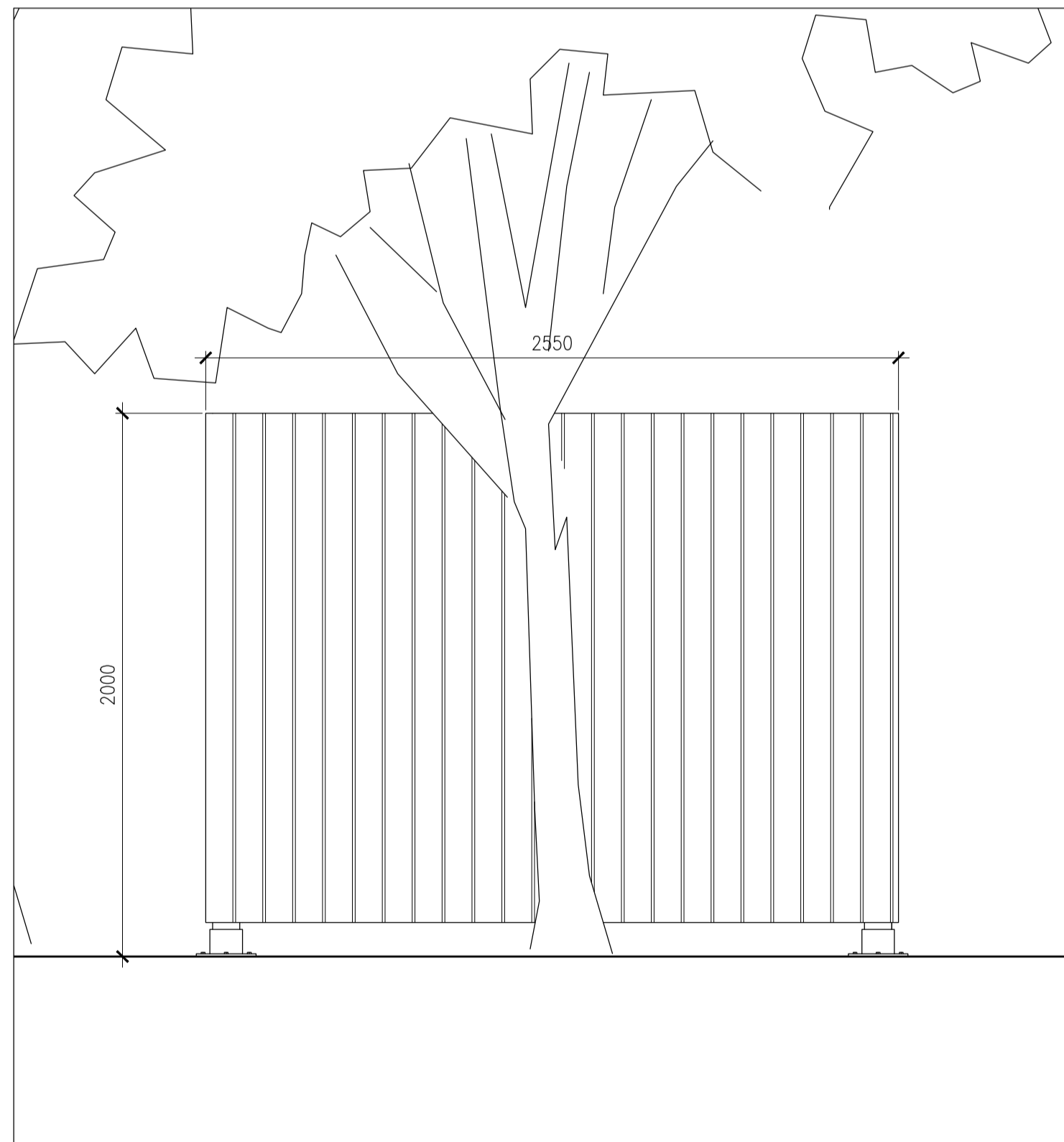
06 - Example fence panel



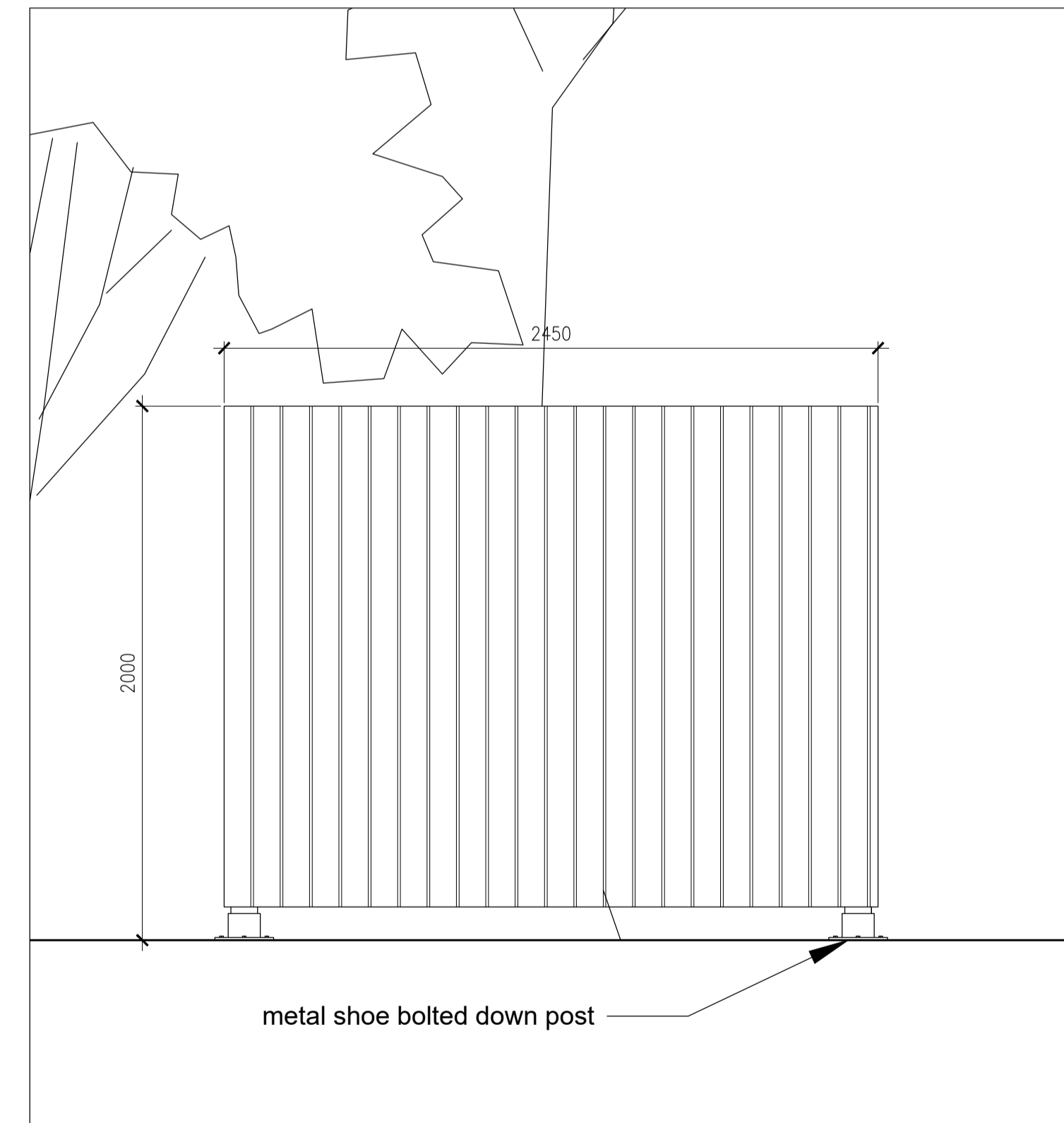
02 Proposed Front Elevation
1:40 @ A3



03 Proposed Side Elevation
1:40 @ A3



04 Proposed Rear Elevation
1:40 @ A3



05 Proposed Side Elevation
1:40 @ A3

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Subject to survey.

Survey information taken from
Greenhatch Topographical/Measured Survey
dated 10/02/2021 ref. 38114

Notes:

SCALE
0 0.2 0.5 1m

02 post shoe amended	29.07.2022	JR	HA
- First Issue	28.07.2022	JR	HA
Rev: Notes:	Date:	Dwn:	Iss:
Suitability Code:			



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ARCHITECTURE
22c Leathermarket Street, London, SE1 3HP

Project:
Sandham House
Boundary Business Court,
92-94 Church Road, Mitcham, CR4 3TD

Proposed Refuse Enclosure

Project No: P0035 / 21088	Scale @ A1/A3: 1: 20 / 1:40	Revision: 02
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Drawing No:
PL016

PLANNING APPLICATIONS COMMITTEE 18th August 2022.

UPRN	APPLICATION NO. 21/P2570	Item No: DATE VALID 29.06.2021
Address/Site	Sandham House Boundary Business Court 92 - 94 Church Road Mitcham CR4 3TD	
Ward:	Cricket Green	
Proposal:	Application for advertisement consent for the display of new shop signage, comprising 1 x non-illuminated fascia sign, 2 x internally illuminated fascia signs and 1 x non illuminated totem sign	
Drawing Nos:	Site location plan and drawings PL006 Rev 17, PL007 Rev 16, PL010 Rev 09, PL012 Rev 03, 02-MITC-02D, 03-MITC-03B and 03-MITC-03C_R1	
Contact Officer:	Leigh Harrington (020 8545 3836)	

RECOMMENDATION

Grant advertisement consent subject to conditions

CHECKLIST INFORMATION.

- Heads of agreement: No
- Is a screening opinion required: No
- Is an Environmental Statement required: No
- Has an Environmental Impact Assessment been submitted: No
- Design Review Panel consulted: No
- Number of neighbours consulted: 38
- Press notice – No
- Site notice –Yes
- External consultations: No
- Archaeological Priority Zone – No
- Controlled Parking Zone – No

1. **INTRODUCTION**

- 1.1 The application has been brought before the Committee due to the level of public interest. The application is subject to a requirement for advertisement consent because it involves the display of advertisements that are beyond those permitted under deemed advertising consent.
- 1.2 The application and its linked application 22/P2571 were deferred from the June 2022 PAC meeting because of outstanding concerns relating to trees.

2. **SITE AND SURROUNDINGS**

- 2.1 The application site is a semi vacant office building located at the northern end of the Boundary Business Court, an industrial park situated on the eastern side of Church Road in Mitcham. The building has car parking areas to the north and east and an unused vehicle entrance from Church Road to the north. To the north of the site is a new as yet unfinished residential development of flats on the former site of the Fosters Auto Care Centre. The site is bounded to the east and south by the Business Court whilst residential properties to the west are located on the opposite side of Church Road and behind a large grass verge area.
- 2.2 The site is not located within a conservation area nor is it in anyway listed. The site is not located within a controlled parking zone and has a public transport accessibility level (PTAL) of 2 (0 being the lowest and 6b being the best).

3. **CURRENT PROPOSAL**

- 3.1 The proposals are for adverts relating the use of the premises as a Tesco Express store and are linked to an application for extensions and car parking at the site under LBM 21/P2571 which is also under consideration at the 18th August 2022 Planning Applications Committee.
- 3.2 The proposal was originally for advertisement consent for the display of new shop signage, comprising 1 x non-illuminated fascia sign, 2 x internally illuminated fascia signs and 1 x internally illuminated totem sign.
- 3.3 Following objections from neighbours and officers the totem sign would now be non illuminated. This 3.3m high, 1.45m wide, 0.15m deep Totem sign would be situated in front of the store on the southern corner of the entrance and angled at 90degrees to the road so as to be most visible to traffic along Church Road.
- 3.4 On the Church Road elevation there would be a centrally located new Box fascia in Aluminium powder coated white RAL 9010. It would be internally illuminated for the letters only with Tesco in Opal 030, Avery 4509, Red 431, And Express & blips in Opal 030- Avery 5600 LD/082A blue, with translucent acrylic letters applied to the face. This would be flanked on each side by non illuminated

fascia panels in Aluminium, powder coated Blue RAL 5005, 30% Gloss with Brown Arizona Oak woodtex strip to base. The right hand panel would include Store Address and Est. year vinyl lettering applied.

- 3.5 On the entrance elevation facing the car park the non-illuminated blue banding would carry around from the Church Road elevation and lead to another of the illuminated Tesco Express panels described above which would be situated above the main entrance.
- 3.6 For the signage around the ATM there would be a 5mm dibond panel to be wrapped around the ATM surround in Colour - Pantone 293, with print as per artwork on drawing 03_MITC_03B. The design includes White spots on blue for vinyl panel and a blue header panel with white text to read 'Cash Machine'. a brushed aluminium kick plate would be installed at the bottom.

4. **PLANNING HISTORY**

- 4.1 21/P2571 - Erection of a single storey extension, including alterations to the car park layout plus associated works to existing office unit to facilitate use as a Tesco retail store – Application decision for consideration at the June Planning Committee
- 4.2 21/P0162 - Lawful Development Certificate issued in respect of the proposed re-classification from Class B1 to Class E.
- 4.3 88/P1359 - Planning permission granted for redevelopment of the site by the erection of 21 no. new industrial units (use classes B1 & B2) with car parking landscaping fencing and formation of new access off Church Road.

Other relevant planning history

4.4 96 Church Road (Fosters' Autos site)

19/P0191 - Outline application (with landscaping a reserved matter) for the redevelopment of the site involving the erection of a 4 storey residential block to provide 20 x flats (revision of 17/P4147). Approved by PAC 22/08/2019.

19/P3178 - Application for approval of reserved matters (landscaping) relating to LBM outline permission 19/P0191 for the redevelopment of the site involving the erection of a 4 storey residential block to provide 20 x flats. Approved.

5. **CONSULTATION**

- 5.1. Consultation letters sent to neighbours and site notice posted. Objections were received from 42 persons, however, please note that the main bulk of the objections relate to concerns with LBM 21/P2571 (main application also for

decision at 14th July 2022 committee meeting) rather than the advert application. These objections relating to the advertisement application raised the following concerns;

- The request for advertisement consent for illuminated signs and totems is inappropriate for a site being overlooked by residential flats. Illuminated signage on a business that will operate until 11pm or midnight would impact the amenity of the flats facing the lights. Any illuminated ATM operating 24/7 should not be facing any flats.

6. POLICY CONTEXT

- 6.1 Relevant policies in the London Plan 2021 are;
D3 (Optimising site capacity through a design lead approach)
E9 (Retail, markets and hot food takeaways)
- 6.2 NPPF 2021
- 6.3 Merton Local Development Framework Core Strategy (2011)
Relevant policies include:
CS 14 Design
- 6.4 The relevant policies in the Council's Adopted Sites and Policies Plan 2014 are:
DM D1 Urban design and the public realm
DM D2 Design considerations
DM D5 Advertisements
DM D7 Shop front design and signage

7. PLANNING CONSIDERATIONS

- 7.1 The principal planning considerations in this case relate to the scale, design and positioning of the advertisements on the appearance and character of the building and its wider setting.
- 7.2 **Design**
- 7.2.1 SPP policies DM D1(Urban design and the public realm), DM D5 (Advertisements) and DM D7(Shop front design and signage) require high quality design that relates to the appearance, scale and character of the shop front, does not harm local amenity through light pollution, protects the character and amenity of the borough and that this is not diluted or undermined by inappropriate or excessive advertising on buildings.
- 7.2.2 On the two side elevations where the adverts would be attached there would be a relatively narrow 0.65m deep band of advertising of which around only half would be internally illuminated. It is considered that the scale is in proportion to that of

the existing building whilst the quantum of illumination would allow for the presence of the shop to be apparent to passers by without it being visually intrusive from the wider realm.

- 7.2.3 With the totem being amended to non illuminated it is considered that there would be a significant reduction in its visibility after dark.

7.3 **The impact on neighbour amenity**

- 7.3.1 SPP policy DM D7 states that signage for shops should not be harmful to neighbour amenity. The closest residential occupiers would be located within the flats at 96 Church Road to the north of the site. As the totem would no longer be illuminated there is just one illuminated sign above the shop entrance and a small amount of illumination around the ATM near the entrance. These signs would be at least 18m from the closest habitable windows and therefore with their modest proportions it is considered that in view they would appear relatively small. The illumination would be such that whilst visible if looked at directly they would not otherwise be noticeable and would not cast light into neighbouring properties.

- 7.3.2 Whilst all the ground floor habitable rooms in the closest flat would have primary windows directly overlooking the site views of the signage would be restricted by the proposed boundary treatment. The flats at first and second floor levels would only have the secondary windows in one of the bedrooms and the combined lounge/kitchen/dining room facing the site whilst on the third floor the amenity terraces would face the site.

- 7.3.3 Objections raised concerns of light pollution from the 24/7 illumination around an ATM opposite flats but this is a small sized advert which casts very little glow and of which there are numerous examples throughout the borough of flats above shops opposite such signage and which have no material harm on neighbour amenity.

- 7.3.4 The adverts will have a luminance of 300 candela/m² which is the same level of brightness as most consumer desktop liquid crystal displays have which luminances of 200 to 300 cd/m²

- 7.3.4 Those factors combined with the location on a busy road in suburban London are considered to combine to result in signage that is of a size and level of illumination appropriate to its purpose and setting and which would not materially harm the amenity of neighbouring occupiers.

7.4 **Highways**

- 7.4.1 SPP policy DM D5 (Advertisements) states that advertisements should not represent a safety or security hazard to pedestrians or motorists by impeding

views, unduly distracting attention, creating opportunities for concealment or otherwise undermining perceptions of safety.

7.4.2 As the totem would be of modest proportions, be set back from the pavement edge, be non-illuminated and have no moving parts it is considered that the proposals would accord with the aims of this policy. Likewise, the signs above the shopfront are modest in size and have limited illumination. Transport planning raised no objection.

8. CONCLUSION

8.1 The proposed signage is of a design common throughout the Tesco Express property portfolio that has been appropriately scaled and designed to fit the space above the ground floor. The level of illuminated signage has been reduced by removing internal illumination for the totem whilst the remaining illuminated signs are relatively modest in the form of a main advert on each of the two main elevations and small illuminated surround for the ATM. The adverts are no brighter than a home computer monitor screen and members would have seen identical adverts on visits to similar Tesco Express stores.

10.2 The signs are located more than 18m from the nearest residential occupiers.

10.3 The proposed advertising is considered to be of an appropriate policy compliant form and it is recommended that advertisement consent be granted subject to conditions.

RECOMMENDATION

GRANT advert consent subject to the following conditions:

1. G1 Standard Time Condition (Advert) This consent shall expire 5 years from the date of this decision whereupon the signage shall be removed and any damage repaired unless a further consent to display has been granted by the Local Planning Authority.

Reason: To comply with Regulation 14 of the Town & Country Planning (Control of Advertisements) (England) Regulations 2007

2. A7 Approved Plans; Site location plan and drawings,

3. G4 Clean and Tidy; Condition Any advertisement displayed, and any site used for the display of advertisements shall be maintained in a condition that does not impair the visual amenity of the site.

Reason: To comply with Regulation 14 of the Town & Country Planning (Control of Advertisements) (England) Regulations 2007.

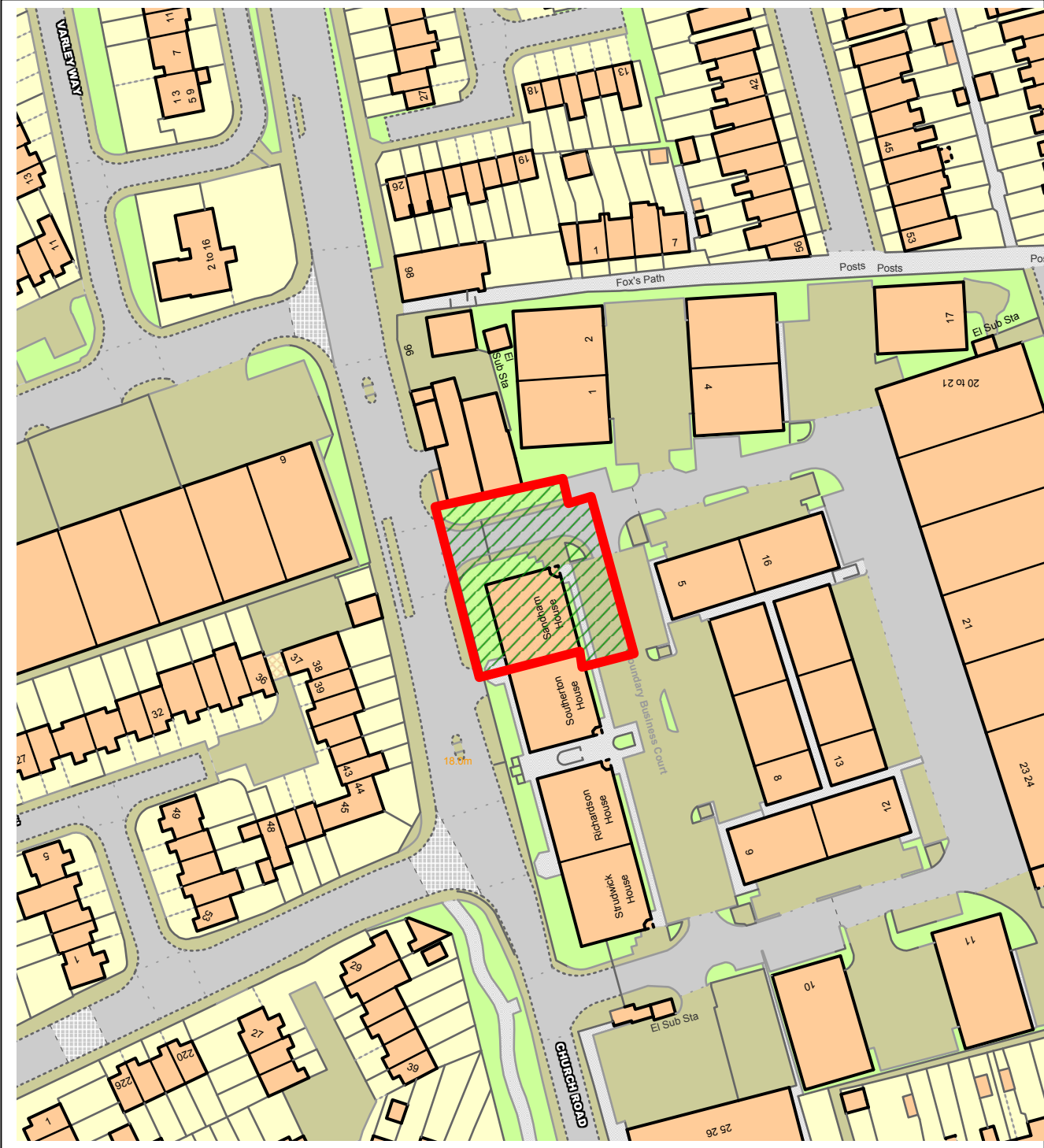
4. G3 Hazards; No advertisement shall be sited or displayed so as to obscure or hinder the ready interpretation of any road traffic sign, railway signal or aid to navigation by water or air, or so as otherwise to render hazardous the use of any highway, railway, waterway (including any coastal waters) or aerodrome (civil or military).

Reason: To accord with Regulation 14 of the Town and Country Planning (Control of Advertisements) (England) Regulations 2007. G No moving lights

5. G6 Removal if Necessary Where an Advertisement is required under these Regulations to be removed, the site shall be left in a condition that does not endanger the public or impair visual amenity.

Reason: To comply with Regulation 14 of the Town & Country Planning (Control of Advertisements) (England) Regulations 2007.

NORTHGATE SE GIS Print Template



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Greenhatch Topographical/Measured Survey
dated 10/02/2021 ref. 38114

Notes:

N

Site Area

Ownership Site Area
Approx. 24,165 sq m
Approx. 260,112 sq ft
Approx. 5.971 acres

Proposed Works Site Area
Approx. 1,122 sq m
Approx. 12,087 sq ft
Approx. 0.277 acres

06	Planning Issue	23.06.2021	ZG	HA
05	Draft Planning Issue	15.06.2021	JW	HA
04	Draft Planning Issue	01.06.2021	JW	HA
03	Scale bar added.	01.06.2021	JW	HA
02	Planning Issue	19.05.2021	JW	HA
01	Draft Planning Issue	18.02.2021	JW	HA
-	First Issue	15.02.2021	ZG	HA
Rev:	Notes:	Date:	Dwn:	Iss:
Suitability Code:				



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ARCHITECTURE
22c Leathermarket Street, London, SE1 3HP

Project:
Sandham House
Boundary Business Court,
92-94 Church Road, Mitcham, CR4 3TD

Drawing Title:
Site Location Plan

Project No: 00000	Scale @ A1 1: 1250 / 200	Revision: 06
Drawing No: PL001		

Site Location Plan
1:1250

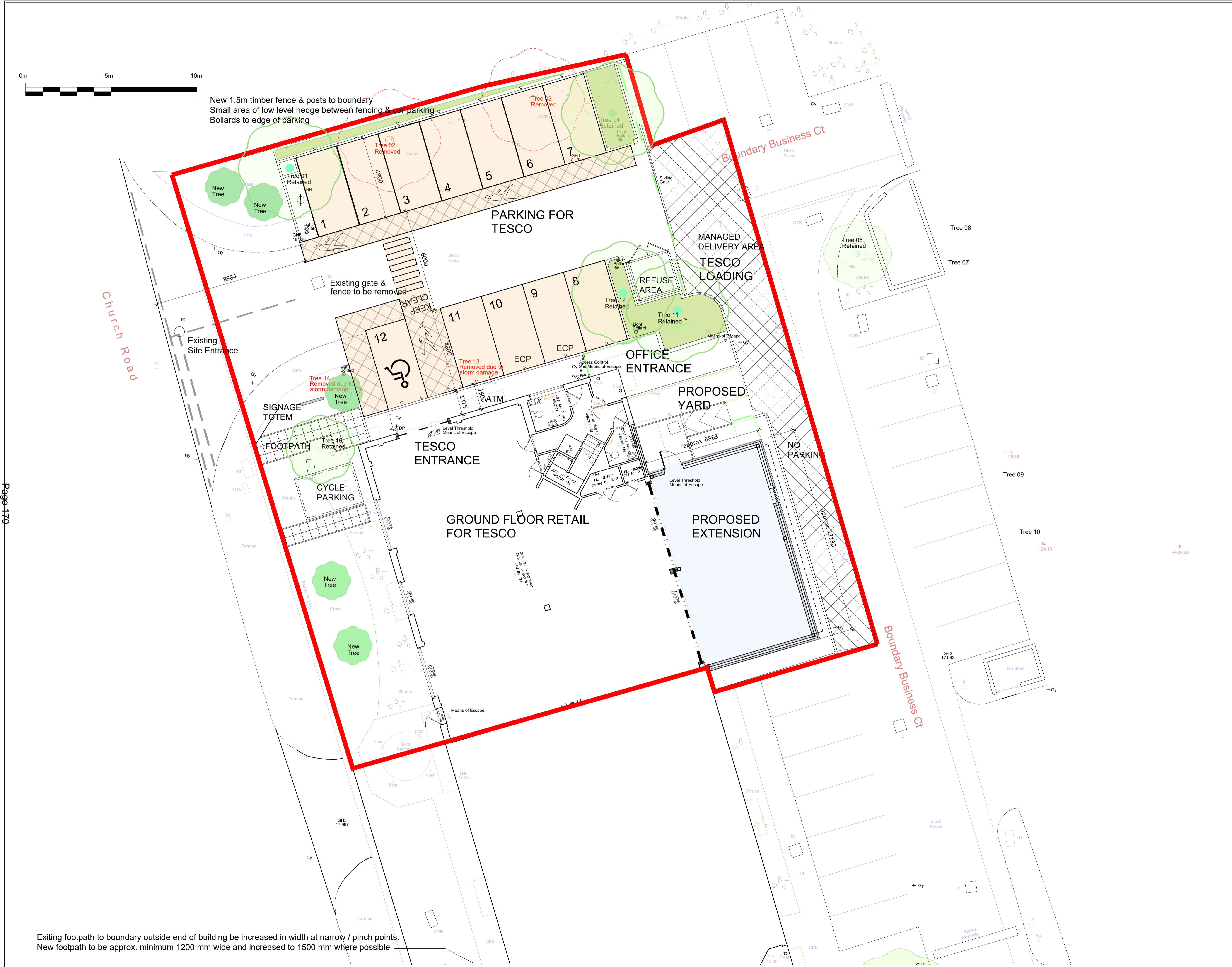


New 1.5m timber fence & posts to boundary
 Small area of low level hedge between fencing & car parking
 Bollards to edge of parking

Church Road

Boundary Business Ct

Boundary Business Ct



Exiting footpath to boundary outside end of building be increased in width at narrow / pinch points.
 New footpath to be approx. minimum 1200 mm wide and increased to 1500 mm where possible

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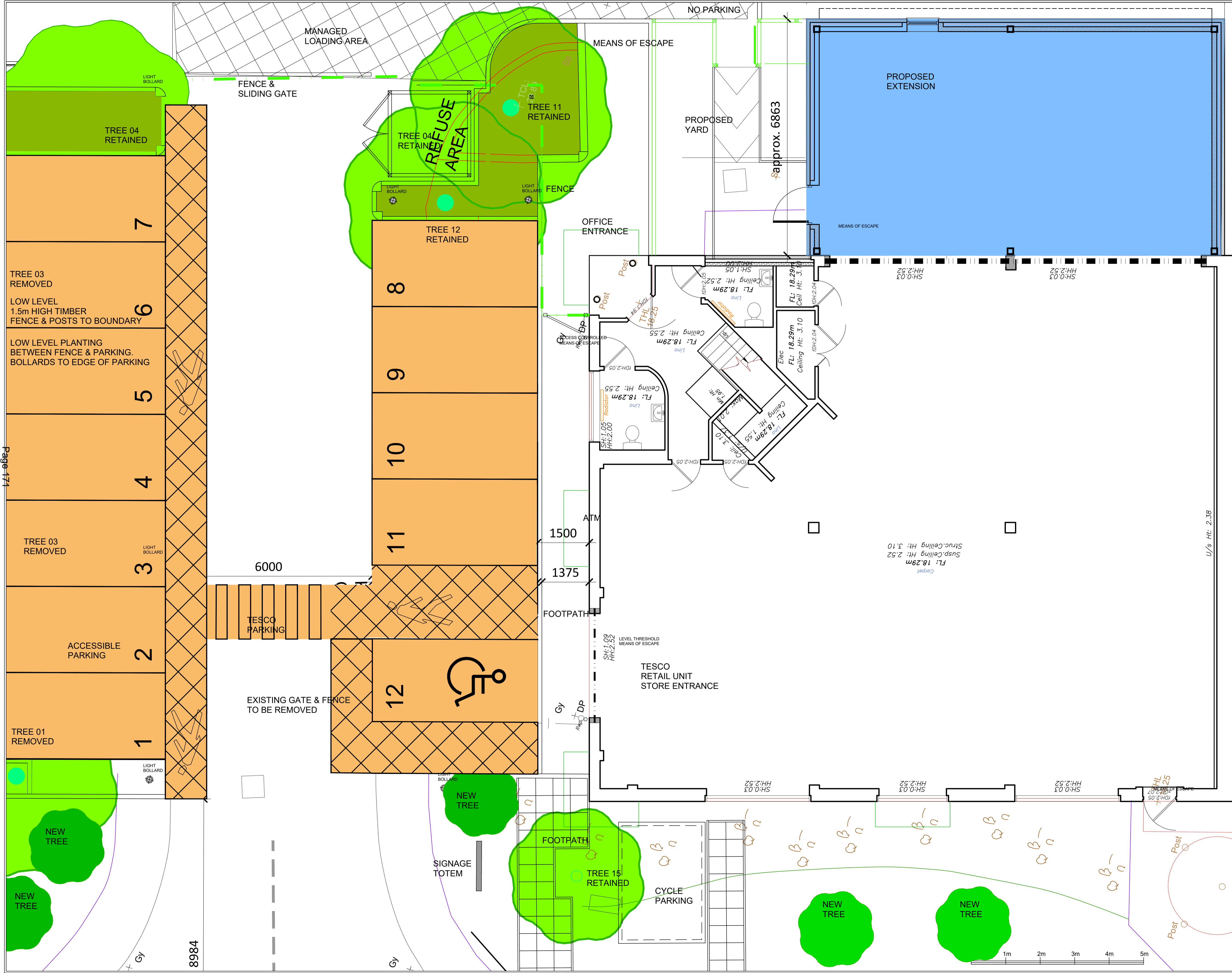


Rev	Notes	Date	Dwn	Iss
17	Planning Issue. Trees updated. Omit Tree 03 / Retain Tree 04 / Add new tree to entrance. Landscaping updated to suit Planning requirements.	26.07.22	jw	HA
16	Planning Issue. Storm damaged trees updated. Light bollards added to suit Planning requirements.	10.03.22	jw	HA
15	Planning Issue. Alternative parking layout to suit Planning requirements.	03.02.22	jw	HA
14	Planning Issue. Tree 014 retained to suit Planning requirements. 2 no. trees added to frontage.	03.02.22	jw	HA
13	Planning Issue. Tree 014 omitted to suit Planning requirements.	27.01.22	jw	HA
12	Planning Issue. Parking updated to suit 18.11.2021 Tesco requirements.		jw	HA
11	Planning Issue. Exlg. trees updated.	12.10.2021	jw	HA
10	Planning Issue. Parking amended.	07.09.2021	jw	HA
09	Planning Issue. Parking layout & trees amended.	06.09.2021	jw	HA
08	Planning Issue.	24.06.2021	jw	HA
07	Planning Issue.	15.06.2021	jw	HA
06	Draft Planning Issue.	02.06.2021	jw	HA
05	Site boundary amended.	02.06.2021	jw	HA
04	Scale bar added.	01.06.2021	jw	HA
03	Planning Issue. Site layout updated	27.05.2021	jw	HA
02	Parking layout updated	19.05.2021	jw	HA
01	Planning Issue	18.02.2021	jw	HA
-	First Issue	18.02.2021	ZG	HA



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 ARCHITECTURE
 22c Leathermarket Street, London, SE1 3HP
 Project:
Sandham House
 Boundary Business Court,
 92-94 Church Road, Mitcham, CR4 3TD
 Drawing Title:
Proposed Site Plan

Project No: P0035 / 21088	Scale @ A1/A3: 1: 100 / 1:200	Revision: 17
Drawing No: PL006		



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dated 10/02/2021 ref. 38114

Notes:

16	Planning Issue. Trees updated. Omit Tree 03 / Retain Tree 04 / Add new tree to entrance. Landscaping amended to suit Planning requirements.	26.07.2022	jw	HA
15	Planning Issue. Storm damaged trees updated. Light bollards added to suit Planning requirements.	10.03.2022	jw	HA
14	Planning Issue. Alternative parking layout to suit Planning requirements.	03.02.2022	jw	HA
13	Planning Issue. Tree 014 retained to suit Planning requirements. 2 no. trees added to frontage	03.02.22	jw	HA
12	Planning Issue. Tree 014 omitted to suit Planning requirements.	27.01.2022	jw	HA
11	Planning Issue. Parking updated to suit Tesco requirements.	18.11.2021	jw	HA
10	Planning Issue. Extg trees updated.	12.10.2021	jw	HA
09	Planning Issue. Parking amended.	07.09.2021	jw	HA
08	Planning Issue. Car Parking & trees amended	06.09.2021	jw	HA
07	Planning Issue. Hatching Amended	25.06.2021	ZG	HA
06	Planning Issue.	02.06.2021	jw	HA
05	Site boundary amended.	02.06.2021	jw	HA
04	Scale bar added.	01.06.2021	jw	HA
03	Planning Issue. Refuse area updated	27.05.2021	jw	HA
02	Layout updated	19.05.2021	jw	HA
01	Draft Planning Issue	18.02.2021	jw	HA
-	First Issue	18.02.2021	ZG	HA
Rev:	Notes:	Date:	Dwn:	Iss:
Suitability Code:				

Client:

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ARCHITECTURE
22c Leathermarket Street, London, SE1 3HP

Project:
Sandham House
Boundary Business Court,
92-94 Church Road, Mitcham, CR4 3TD

Drawing Title:
Proposed Ground Floor Plan
Proposed Single Storey Extension

Project No: P0035 / 21088	Scale @ A1/A3: 1: 50 / 1:100	Revision: 16
Drawing No: PL007		

New fascia panel in Aluminium, powder coated Blue RAL 5005, 30% Gloss with Brown Arizona Oak woodtex strip to base.

SIGN 5
SIGN 4

New fascia panel in Aluminium, powder coated Blue RAL 5005, 30% Gloss with Brown Arizona Oak woodtex strip to base. Store Address and Est. year vinyl lettering applied to new fascia, non illuminated.

SIGN 3

New Box fascia in Aluminium powder coated white RAL 9010. Internal illumination to letters only. Tesco in Opal 030 Avery 4509 Red 431, And Express & blips in Opal 030- Avery 5600 LD/082A blue, translucent acrylic letters applied to face.

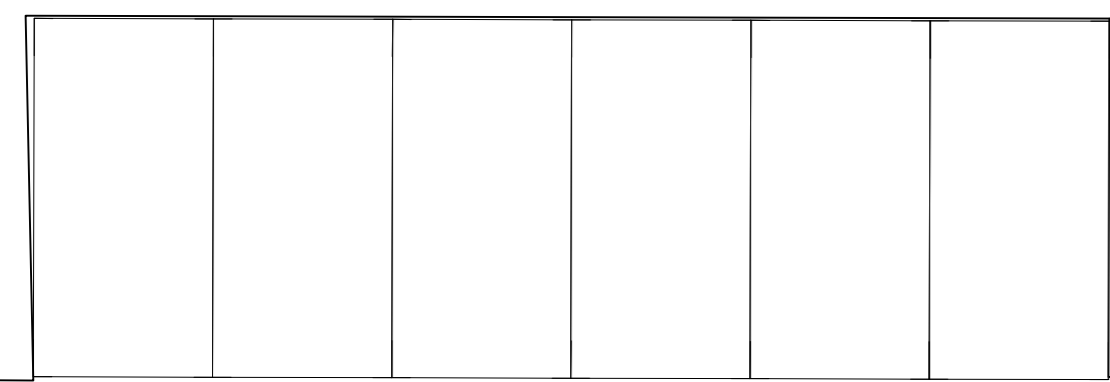
SIGN 2
SIGN 1

TESCO

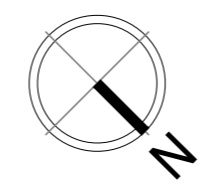
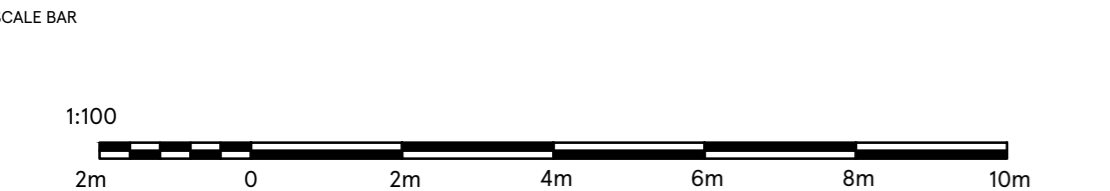
5mm dibond panel to be wrapped around ATM surround. Colour - Pantone 293, with print as per artwork. White spots on blue for vinyl panel. Blue header panel with white text to read 'Cash Machine'. Brushed aluminium kick plate to be installed at the bottom.

SIGN 6

Page 172



4 Car Spaces



DRAWING NO
02_MITC_02D

SCALE
1:100@A2

CHECKED BY
SHIKHA

DRAWN BY
DEEKSHA

STORE TYPE
EXPRESS

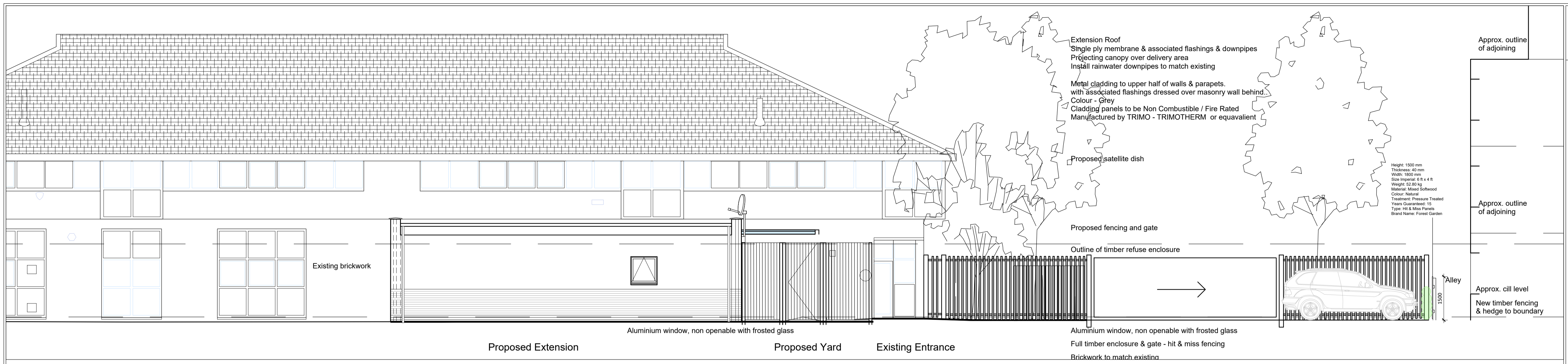
STORE NAME
MITCHAM

CONTENT
PROPOSED PLAN

TESCO

FEASIBILITY PLANNING
STORE PLANNING DEPARTMENT
PROPERTY SERVICES
HIGHWOODS BUILDING, KESTRAL WAY
WELWYN GARDEN CITY, HERTFORDSHIRE, AL7 1GB UK
TELEPHONE : 01707 395150

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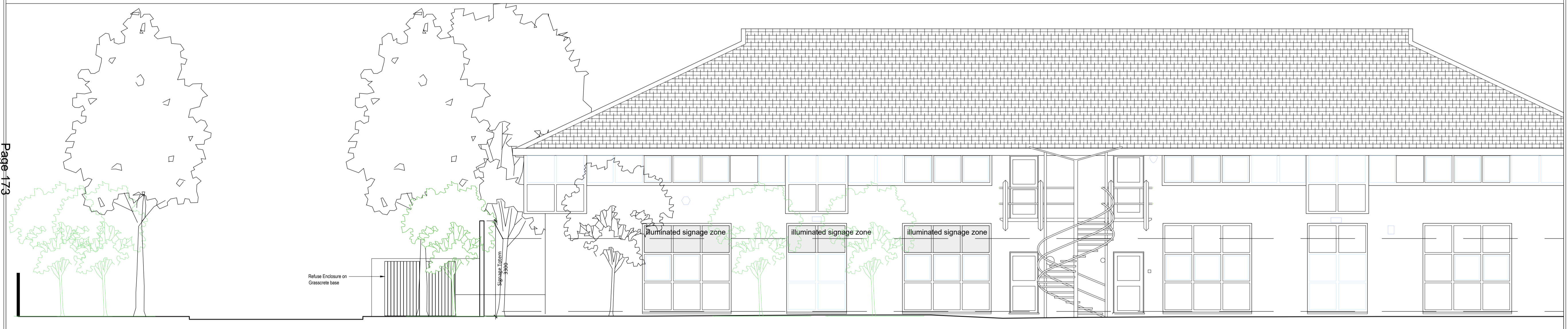
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dated 10/02/2021 ref. 38114

Notes:

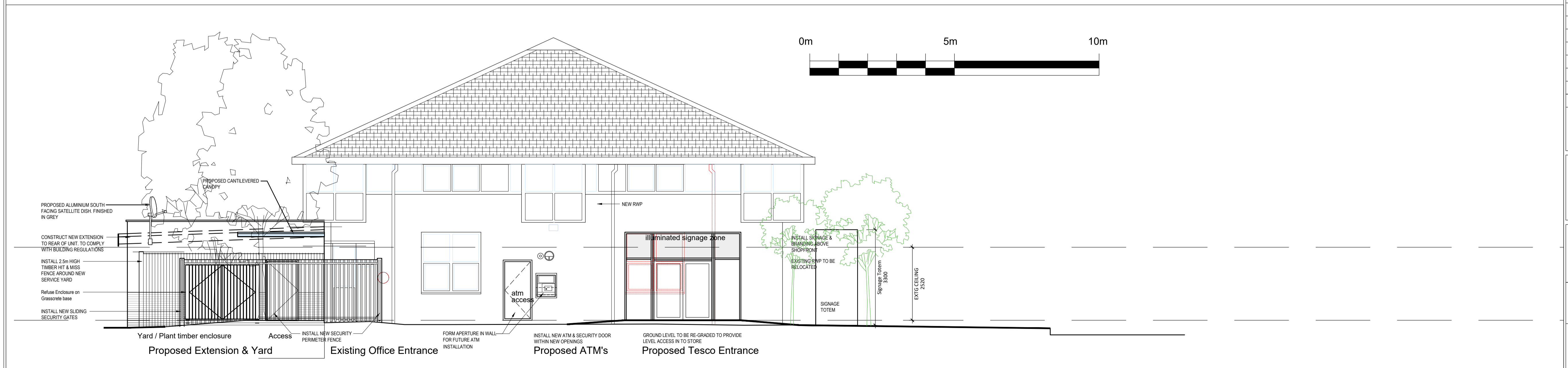
01 Proposed Elevation

1:150 @ A3



02 Proposed Elevation

1:150 @ A3



03 Proposed Elevation

1:150 @ A3

09	Refuse enclosure added	28.07.2022	JR	HA
08	Planning Issue. Trees updated.	26.07.2022	JW	HA
07	Planning Issue. Totem Sign added	18.11.2021	JW	HA
06	Planning Issue. Elevation Amended	25.06.2021	ZG	HA
05	Planning Issue. Scale bar added.	01.06.2021	jw	HA
04	Planning Issue	19.05.2021	jw	HA
03	Labels Amended	24.02.2021	ZG	HA
02	Planning Issue	22.02.2021	jw	HA
01	Draft Planning Issue	18.02.2021	jw	HA
-	First Issue	17.02.2021	ZG	HA
Rev:	Notes:	Date:	Dwn:	Iss:
Suitability Code:				



hale
ARCHITECTURE
22c Leathermarket Street, London, SE1 3HP

Project:
Sandham House
Boundary Business Court,
92-94 Church Road, Mitcham, CR4 3TD

Drawing Title:
Proposed Elevations

Project No: P0035 / 21088	Scale @ A1/A3: 1: 75 / 1:150	Revision: 09
Drawing No: PL010		



New Box fascia in Aluminium powder coated white RAL 9010. Internal illumination to letters only. Tesco in Opal 030 Avery 4509 Red 431, And Express & blips in Opal 030- Avery 5600 LD/082A blue, translucent acrylic letters applied to face.

New fascia panel in Aluminium, powder coated Blue RAL 5005, 30% Gloss with Brown Arizona Oak woodtex strip to base.

5mm dibond panel to be wrapped around ATM surround. Colour - Pantone 293, with print as per artwork. White spots on blue for vinyl panel. Blue header panel with white text to read 'Cash Machine'. Brushed aluminium kick plate to be installed at the bottom.

WEST ELEVATION

Page 174

New fascia panel in Aluminium, powder coated Blue RAL 5005, 30% Gloss with Brown Arizona Oak woodtex strip to base.

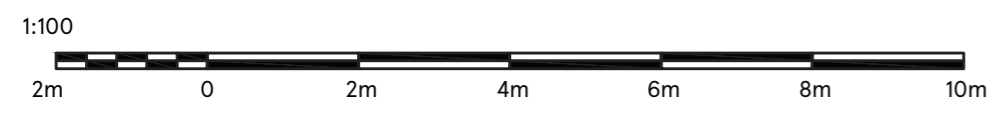


New Box fascia in Aluminium powder coated white RAL 9010. Internal illumination to letters only. Tesco in Opal 030 Avery 4509 Red 431, And Express & blips in Opal 030- Avery 5600 LD/082A blue, translucent acrylic letters applied to face.

New fascia panel in Aluminium, powder coated Blue RAL 5005, 30% Gloss with Brown Arizona Oak woodtex strip to base. Store Address and Est. year vinyl lettering applied to new fascia, non illuminated. Store Add.: 100mm, Est Yr.: 70mm

SOUTH ELEVATION

SCALE BAR



DRAWING NO 03_MITC_03B	DATE 04.06.2021
SCALE 1:100@A3	CHECKED BY SHIKHA
	DRAWN BY DEEKSHA

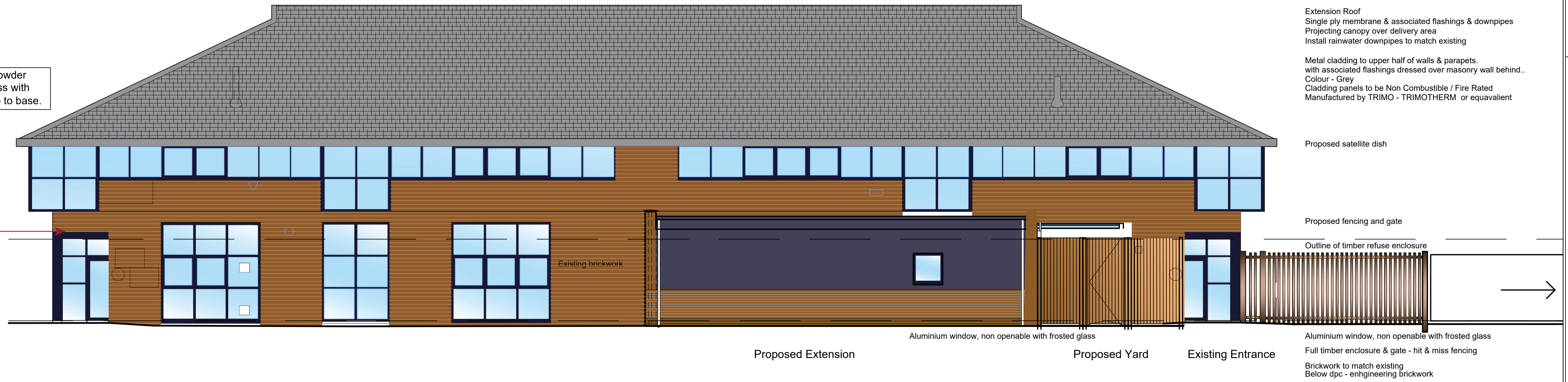
STORE TYPE EXPRESS
STORE NAME MITCHAM
CONTENT PROPOSED ELEVATIONS_SIGNAGE

TESCO
FEASIBILITY PLANNING
STORE PLANNING DEPARTMENT
PROPERTY SERVICES
HIGHWOODS BUILDING, KESTREL WAY
WELWYN GARDEN CITY, HERTFORDSHIRE, AL7 1GB UK
TELEPHONE : 01707 395150

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New fascia panel in Aluminium powder coated Blue RAL 5005, 30% Gloss with Brown Arizona Oak woodtex strip to base.

SIGN 4
6420x650mm



Extension Roof
Single ply membrane & associated flashings & downpipes
Projecting canopy over delivery area
Install rainwater downpipes to match existing

Metal cladding to upper half of walls & parapets.
with associated flashings dressed over masonry wall behind.
Colour - Grey
Cladding panels to be Non Combustible / Fire Rated
Manufactured by TRIMO - TRIMOTHERM or equivalent

Proposed satellite dish

Proposed fencing and gate

Outline of timber refuse enclosure

Aluminium window, non openable with frosted glass
Full timber enclosure & gate - hit & miss fencing
Brickwork to match existing
Below dpc - engineering brickwork

Disclaimer:
Subject to survey.
Survey information taken from
Greenhatch Topographical/Measured Survey
dated 10/02/2021 ref. 38114

Notes:
Signage
Refer to Tesco signage drawings for details of
specific signs

This drawing is an extract of the Tesco drawing
Proposed Elevation Signage
Drawing no. 03-MITC-03 B

01 Proposed Rear Elevation

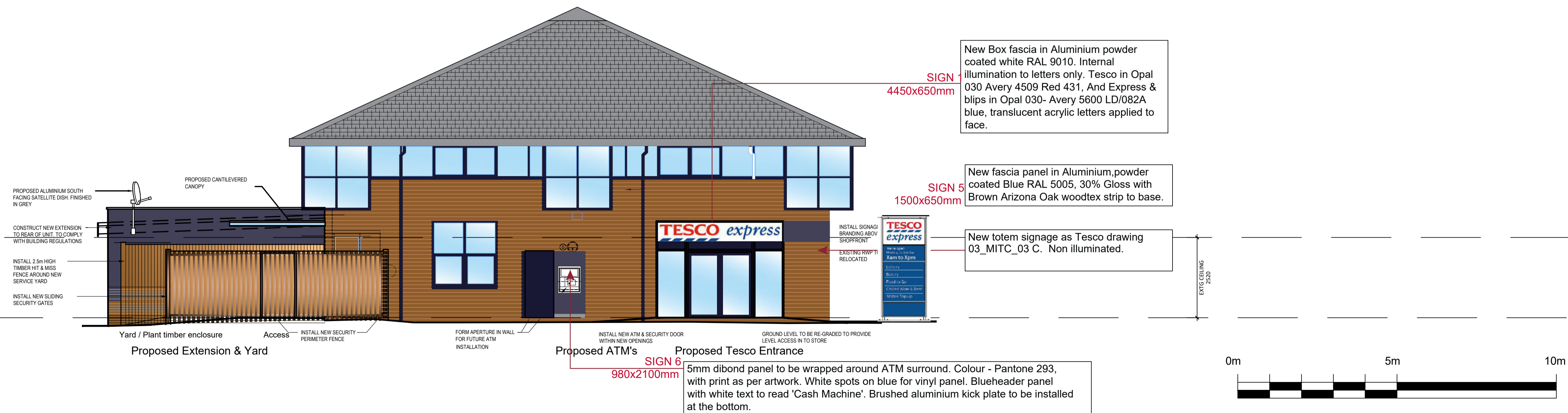
1:150 @ A3

Page 176



02 Proposed Front Elevation

1:150 @ A3



SIGN 4
4450x650mm

New Box fascia in Aluminium powder coated white RAL 9010. Internal illumination to letters only. Tesco in Opal 030 Avery 4509 Red 431, And Express & blips in Opal 030- Avery 5600 LD/082A blue, translucent acrylic letters applied to face.

SIGN 5
1500x650mm

New fascia panel in Aluminium powder coated Blue RAL 5005, 30% Gloss with Brown Arizona Oak woodtex strip to base.

New totem signage as Tesco drawing 03_MITC_03 C. Non illuminated.

SIGN 6
980x2100mm

5mm dibond panel to be wrapped around ATM surround. Colour - Pantone 293, with print as per artwork. White spots on blue for vinyl panel. Blueheader panel with white text to read 'Cash Machine'. Brushed aluminium kick plate to be installed at the bottom.

02 Tesco totem signage added	01.06.2022	JW	HA
01 First Issue	17.06.2021	ZG	HA
Rev:	Notes:	Date:	Dwn: Iss:
Suitability Code:			



hale
ARCHITECTURE
22c Leathermarket Street, London, SE1 3HP

Project:
Sandham House
Boundary Business Court,
92-94 Church Road, Mitcham, CR4 3TD
Drawing Title:
Proposed Elevations
Proposed Signage

Project No: P0035 / 21088	Scale @ A1/A3: 1: 75 / 1:150	Revision: 03
Drawing No: PL012		

03 Proposed Side Elevation


1:150 @ A3



New 3.3m High Gantry, non-Illuminated.
Size: 1445 x 3300 mm

SCALE BAR



DRAWING NO 03_MITC_03C_R1	STORE TYPE EXPRESS	 FEASIBILITY PLANNING STORE PLANNING DEPARTMENT PROPERTY SERVICES HIGHWOODS BUILDING, KESTREL WAY WELWYN GARDEN CITY, HERTFORDSHIRE, AL7 1GB UK TELEPHONE : 01707 395150 <small>THE COPYRIGHT AND ALL OTHER RIGHTS IN AND RELATING TO THIS PUBLICATION BELONG TO AND ARE THE SOLE PROPERTY OF TESCO STORES LIMITED. ALL RIGHTS ARE RESERVED. NEITHER THE WHOLE NOR ANY PART OF THIS PUBLICATION MAY BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY ELECTRONIC OR MECHANICAL MEANS INCLUDING PHOTOCOPYING AND RECORDING OR FROM ANY RETRIEVAL SYSTEM WITHOUT THE PRIOR WRITTEN LICENCE FROM THE OWNER © 1997 TESCO STORES LIMITED.</small>
SCALE 1:20@A3	DATE 01.09.2021	
CHECKED BY SHIKHA	STORE NAME MITCHAM	
DRAWN BY DEEKSHA	CONTENT PROPOSED SITE SIGNAGE	

PLANNING APPLICATIONS COMMITTEE

18th August 2022

Item No:

<u>UPRN</u>	<u>APPLICATION NO.</u>	<u>DATE VALID</u>
-------------	------------------------	-------------------

	21/P4190	24/11/2021
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Address/Site Blagdons Sports Ground Beverley Way New Malden KT3 4PU

(Ward) West Barnes

Proposal: DEMOLITION OF 2NO. EXISTING OUTBUILDINGS AND CREATION OF A NEW ALL-WEATHER SPORTS PITCH ("AWP") TO BE ENCLOSED BY BALL STOP FENCING AND ENTRANCE GATES, WITH ASSOCIATED FLOODLIGHT SYSTEM AND TREE PLANTING.

Drawing Nos J32-6962-PS-001, D19-041/DWG/0001 Rev 08, D19-041/DWG/0002 Rev 08, D19-041/DWG/0003 Rev 08, D19-041/DWG/0004 Rev 08, D19-041/DWG/0005 Rev 08, D19-041/DWG/0006 Rev 08, D19-041/DWG/0007 Rev 08, D19-041/DWG/0008 Rev 07, D19-041/DWG/0009 Rev 08, D19-041/DWG/0010 Rev 07, D19-041/DWG/0011 Rev 07, D19-041/DWG/0012 Rev 07, D19-041/DWG/0013 Rev 07, D19-041/DWG/0016 Rev 07, D19-041/DWG/0017 Rev 07, 710 Rev A & 701 Rev A.

Contact

Officer: Tim Lipscomb

RECOMMENDATION

Grant Planning Permission subject to any direction from the Mayor of London and conditions.

CHECKLIST INFORMATION

- Heads of s.106 Agreement: No
- Is a screening opinion required: No
- Is an Environmental Statement required: No
- Has an Environmental Statement been submitted: No
- Press notice: No
- Site notice: Yes
- Number of neighbours consulted: 23
- External consultations: Yes
- Conservation area: No
- Listed building: No

- Tree protection orders: No
- Controlled Parking Zone: No
- Flood Zone 2-3
- PTAL: 1b-2

1. **INTRODUCTION**

- 1.1 This application is being brought to the Planning Applications Committee for determination as officers recommend approval where Sport England, a statutory consultee, have objected to the proposal.

2. **SITE AND SURROUNDINGS**

- 2.1 Blagdons Sports Ground is located on Beverley Way, New Malden. Access to the site is from Beverley Way. The site forms part of the grassed playing fields for Emanuel School, an independent, co-educational day school, which has recently started admitting girls. The main school site is at Battersea Rise, SW11 1HS, in the London Borough of Wandsworth.
- 2.2 Existing built development is focused towards the south-eastern corner of the site, including Blagdon House, the pavilion, squash courts, hard courts, car parking and various outbuildings. A cricket practice facility is located near to the facilities adjacent to the site's eastern boundary.
- 2.3 Six (6no.) 15.0m high floodlights serve a grass rugby pitch located within the South Western part of the playing field. The submission indicates that these will be removed from the sports ground after March 2022, as there is no requirement to retain them to serve the curriculum rugby activities in the future.
- 2.4 The site is bordered by Beverley Way to the east. The Wyvern industrial estate lies to the south. Beverley Brook and Pyl Brook border the western and northern boundaries of the sports ground, beyond which is Beverley Park (to the west) and Jurassic Encounter Adventure Golf (to the north). The site is some distance from residential development, with the nearest properties being c. 70 metres away on Onslow Road beyond a tree belt. There are a number of trees to the perimeter of the wider school grounds and some trees beyond the southern boundary of the application site. Of the 33 individual trees recorded in the submitted Arboricultural Impact Assessment, one was classed as Category A (high quality), 14 as Category B (moderate quality), 16 as Category C (low quality) and two Category U (poor quality or dead). The 11 groups were recorded as four Category B, five Category C and two Category U. There are no Tree Preservation Orders on the site.
- 2.5 The site has the following planning designations:
 - Metropolitan Open Land ("MOL");
 - Open Space;
 - Flood Zones 2 and 3
 - Adjacent to Site of Importance for Nature Conservation
 - PTAL 1b-2
- 2.6 There are no statutorily or locally listed buildings within the site, nor is the site within a Conservation Area.

2.7 The grassed playing field area has historically been used by the Old Emanuel Cricket Club as follows:

- 20 fixtures on Saturdays (18 Surrey Championship League and 2 friendlies)
- 9 on Sundays (league matches, occasionally one or two KO rounds to be fixed last-minute)
- 1 x President's XI annual Fixture
- 6 on Wednesday evenings
- 40 x Tuesday and Thursday evening training sessions

The Viscount Cricket Club has also used the sports ground in the past as follows:

- 6 x Sunday fixtures

2.8 It is of note that the Old Emanuel CC have now secured alternative provision at a different site.

2.9 The site is also home to the Old Emanuel RFC (OERFC) as part of the Old Emanuel Association. The club reports;

- 4 senior teams including men's, women's and a veteran's team.
- Touch Rugby is played for all levels, men & women, and gives the local community access to non-contact rugby and introduction to the wider game.
- There is a large age grade section (350) drawn from the local community (New Malden and surrounds) (Age grade players are usually between 6 -18)

2.10 The cricket club is due to move to a site in Raynes Park but the time frame on this move is unclear.

3. PROPOSAL

3.1 Planning permission is sought for the installation of an 'All Weather Surface' or 'All Weather pitch' (AWP) of 5,930sqm which would replace part of the grass playing field in the southeastern corner of the sports grounds

3.2 The AWP would be enclosed by new 4.5 metre high ball stop fencing and entrance gates. It would be equipped with a new lighting system of 8no. 15 metre high LED floodlights.

3.3 The hours of use of the AWP were originally proposed to be 07.00 to 21.00 Monday to Sunday but these hours have been extended to 07.00 to 22.00 on the basis of Sport England's consultation comments (this matter is discussed later in this report). The floodlights would be turned on no earlier than 7.00 and turned off no later than 22.15.

3.4 Two existing outbuildings, currently used for storage, are proposed to be demolished. The existing non-turf cricket practice nets would be related to another part of the site (under concurrent planning application ref. 22/P0956).

3.5 Tree planting is proposed to the western and northern boundaries of the sports ground (8 trees to the northern boundary and 4 to the western boundary). It will be necessary to remove one Category C tree, T10 and one Category B tree from group 6 (G6). A group of predominantly dying / dead elm have also been recommended for removal as they are Category U (G2) and are close to the works areas. It can be

seen on the Tree Protection Plan (see drawing JSL4060_710) that the position of the AWP encroaches slightly on the RPA of the Category B trees T7, T8 and group G7 and measures are proposed to mitigate for the impact on this tree.

- 3.6 The AWP would be used by the school for the following sports:
- Hockey;
 - Tennis;
 - Netball;
 - Cricket; and
 - Football.
- 3.7 A separate standalone application for practice cricket nets has been submitted under reference 22/P0956.
- 3.8 The application is accompanied by a Community Use Agreement (CUA), which has been amended throughout the course of the application. The CUA sets out that the All-Weather Pitch will be made available for community use for a minimum of two weeknights (6-10pm) and on Sundays (9am-6pm) each week (this would be a minimum provision and the school would offer more when practicable). In advance of the start of each term and the summer holiday (4 times annually), the Outreach Coordinator will write to the local planning authority to confirm the availability of the All-Weather Pitch for community users for the coming term / summer holiday.” (Note that the CUA relates to the use of the AWP and cricket practice nets only, not the wider sports grounds at the Blagdons Site).
- 3.9 The school has outlined the need for the proposed AWP and sets out the following key points:
- The AWP responds to the growing demand and pupil numbers and reduces the school’s reliance on third party facilities, which effectively restricts sports participation in the current situation.
 - There is a notable rise in the number of girls enrolled at the school, from 309 in 2012 to 509 in 2021 (+ 64% over 9 years). This trend has meant that, over time, the school have not been able to offer the fullest range of sporting opportunities to all its students. The application proposals will significantly increase capacity and sporting participation, particularly in offering a full range of girls’ sport.
 - The number of girls’ cricket fixtures is anticipated to increase further by 2024. With the introduction of girls’ football in 2019, it is expected that girls’ participation in football will follow the same pattern. In 2015 the school had no girls’ cricket teams, no girls’ football teams, 15 netball teams, 10 hockey teams. By 2020 this had grown to 12 girls’ cricket teams, 6 girls’ football teams, 29 netball teams and 11 hockey teams (following a COVID year)”
 - The submitted documents outline the Outreach programme that the school runs, which offers support to disadvantaged children. The Planning statement sets out that it is hoped that the existing programme offered at the school’s Wandsworth site can be expanded to make a similar community offer using the new AWP at Blagdons Sports Ground. The submission sets out that Emanuel School is exploring the opportunity to

extend its established outreach programme to offer use of the proposed AWP to community partners when Emanuel School is not using the facility.

3.10 The AWP would take up playing pitch space that is currently used for rugby and cricket and would reduce the number of rugby pitches from 3 to 2 and would reduce the size available for the cricket pitch.

3.11 The table below sets out the existing and proposed sports provision:

TERM	PLAYING PITCH TYPE	EXISTING SITE LAYOUT	PROPOSED SITE LAYOUT
Autumn	15v15 rugby union pitch	3	2
	Mini rugby union	5	6
Spring	11v11 football pitch	2	2
	9v9 youth football pitch	2	-
Summer	Grass cricket pitches with 37m/47m outfield and 3m safety zone	15	12
	Non-turf cricket practice pitches	2	3 (refer to associated planning application)
AWP (all year)	11v11 hockey field	-	1
	Mini hockey pitch	-	2
	Tennis court	-	4
	Netball court	-	3

3.12 The application is accompanied by the following supporting documents:

- Air Quality Assessment
- Arboricultural Impact Assessment
- Badger Survey
- Bat Survey Report
- CIL form
- Cricket Boundary Risk Assessment
- Design and Access Statement
- Lighting Assessment
- Open Space Assessment
- Planning Statement
- Preliminary Ecological Appraisal
- Site Investigation Report (relating to ground conditions, ground water and contamination)
- Transport Statement

Additional/Amended Documents:

- Community Use Agreement (amended 01/04/2022)
- Flood Risk Assessment (amended 21/01/2022)
- Executive Summary – The Future of Blagdons Sports Ground (20/04/2022)
- Construction Logistics Plan and Working Method Statement (01/06/2022)
- Revised Plans to omit cricket nets (25/02/2022) – The cricket nets are to be determined under application 22/P0956.

4. **PLANNING HISTORY**

4.1 Relevant planning history is summarised as follows:

4.2 22/P0956 - Replacement non-turf cricket practice facility, enclosed by ball stop fencing and entrance gates. Pending decision.

4.3 14/P3811 - Demolition of the existing flat roofed two storey changing block and function room building [1030 square metres] and the construction of a replacement two-storey 'changing and function room pavilion' [774 square metres] with the retention of the existing house, demolition of the existing storage barn and squash courts and the removal of the existing derelict tennis courts, cricket nets and storage containers with the reconfiguration of the existing site layout with the retention and relocation of the existing cricket square, three rugby pitches and existing floodlights and the provision of a new multi-use synthetic playing pitch [97.4 metres by 61 metres] in the south west corner of the site [with six, new 15 metre columns each providing 4 lamps and pitch boundary structures up to 4 metres in height], works to the existing trees on the site and new tree planting, the provision of new hardstanding access paths and the formalising and extension of the existing car parking area to provide a total of 59 vehicle spaces and parking for 16 cycles. Withdrawn

4.4 07/P0414 - Installation of 6 floodlighting columns around training pitch. Grant Permission subject to Conditions 28-03-2007

4.5 88/P1164 - Erection of a single storey extension to existing club house to provide snooker room. Granted 11-09-1988

- 4.6 MER631/67 - Erection of new pavilion linked to existing pavilion and formation of car parking for fifty cars. Granted 11-04-2002
- 4.7 MER1022/67 - Use of section of playing field land for car parking in connection with adjoining car park for venders - for three years. Granted 12-04-2002
- 4.8 MER520/71 - Single storey pavilion. Granted 12-04-2002
- 4.9 MER1435/72 - New squash court building adjoining existing changing room. Granted 12-04-2002
- 4.10 MER825/73 - New squash court building adjoining existing changing room. Granted 12-04-2002

5. **CONSULTATION**

5.1 The application was advertised by way of individual letters to nearby occupiers. No representations have been received.

5.2 Internal consultees:

5.3 LBM Policy Open Space, Sport and Recreation:

The site is designated as the following on the Policies Maps (with relevant policies):

- Metropolitan Open Land 04 - Beverley Brook / A3 (CS13, DM01)
- Open Space S044 - Emanuel School Playing Fields (CS13, DM01)

The new Merton Local Plan has now been submitted to the Secretary of State for independent examination. These policies should also be considered and given an appropriate amount of weight in the assessment of this planning application.

In line with the NPPF, London Plan and Merton's policies, Metropolitan Open Land should be protected from inappropriate development. Para 99 of the NPPF states:

"Existing open space, sports and recreational buildings and land, including playing fields, should not be built on unless:

- a) an assessment has been undertaken which has clearly shown the open space, buildings or land to be surplus to requirements; or
- b) the loss resulting from the proposed development would be replaced by equivalent or better provision in terms of quantity and quality in a suitable location; or
- c) the development is for alternative sports and recreational provision, the benefits of which clearly outweigh the loss of the current or former use."

In addition, Merton Policy DMO1 states:

"c) Development proposals within designated open spaces, which have met the conditions set in part b) above, will be required to meet all the following criteria:

- i. the proposals are of a high quality design and do not harm the character, appearance or function of the open space;
- ii. the proposals retain and/or improve public access between existing public areas and open spaces through the creation of new and more direct footpath and cycle path links; and,

iii. the character and function of leisure walks and green chains are preserved or enhanced."

The applicant has stated that the proposed replacement sports facilities would enable greater participation of a variety of sports by school students. The proposed development is for alternative sports provision that would be of benefit to the school students with access to the site. There are some references in the application to the school exploring the opportunity to offer use of the new AWP to community partners. In line with London Plan Policy S5(B)(2) and new Merton Local Plan Policy IN14.3(f) the proposed development should maximise the multiple use of the new sports facilities and the applicant will need to clearly demonstrate how this is possible through the application. A Community Use Agreement should be used to formalise this, which could be conditioned if necessary. This is of particular importance, to demonstrate that the benefits of the new sports facilities will clearly outweigh the loss of a well used rugby pitch.

The Merton Playing Pitch Strategy identifies this site as being of importance for the protection and enhancement of both rugby and cricket. I note that Sport England, the RFU and ECB were not involved in the school's engagement activity prior to the planning application being submitted. Sport England should be consulted as part of the planning application for their views on this proposal, in conjunction with the relevant sporting bodies.

I would also refer you to the comments I made in the separate email relating to biodiversity - the applicant will need to provide further information to demonstrate that the location of the newly proposed cricket nets does not impact adversely on ecology.

5.4 LBM Biodiversity

The site has the following policy map designations, as identified on Merton Maps (with relevant policies):

- MOL 04 - Beverley Brook A3 (CS13, DM01)
- Open Space - Emanuel School Playing Fields (CS13, DM01)
- Green Corridor 01 - Beverley Park (CS13, DM02)

The northern and western part of the wider Blagdon's site is also designated as SINC (MeBII05), although I note that the proposal does not impact on this part of the site.

It should also be noted that the new Merton Local Plan has now been submitted to the Secretary of State for examination. The policies should also be considered in your assessment with the appropriate weight.

Green Infrastructure

The applicant has submitted a Preliminary Ecological Appraisal (PEA) Report, a Bat Survey Report and a Badger Survey Report.

A Phase 1 Habitat Survey was undertaken on 21st April 2021.

Two bat emergence surveys were undertaken in July 2021 of Building B3.

A badger survey was undertaken in October/November 2021.

After reviewing these three reports, I have some queries relating to the methodology and findings of the surveys.

Page 22 of the PEA report states the following:

"5.3.4 T32 and trees A and C within G11 were found to have moderate suitability to support roosting bats, therefore, further survey work is required prior to the construction of the artificial cricket practice facility that would be subjected to tree works. This would involve an aerial inspection of the feature(s) by a licenced bat ecologist. If this found the feature to be suitable, if evidence of bats were found, or if the tree was deemed unsuitable to climb, further, more detailed emergence surveys would be needed.

5.3.5 Further survey work would involve emergence/re-entry surveys to determine if bats are using the tree. This would involve surveyors equipped with bat detectors watching different aspects of the tree, to observe any bats emerging/re-entering. As the tree is deemed to be of moderate suitability, two surveys would be required between May and September, with at least one before mid August. Data from these surveys would then need to be analysed using computer software to confirm species identification."

I can't see in any of the ecology reports submitted by the applicant that this further survey work to the trees has been undertaken. The bat survey report only provides evidence that the building B3 was surveyed.

The badger survey found that badgers are likely absent from the site, however Section 4 of the report recommends that a further walkover be undertaken prior to construction to reassess the site for badgers:

"However, badgers are mobile creatures and often excavate new setts. Therefore, it is recommended that a walkover of the site shall be carried out to reassess the site for badgers immediately prior to construction commencing, to confirm that badgers are still absent from the site. Should the survey identify a change in badger activity and potential harm to the species or their setts, a badger mitigation strategy can be produced and submitted to the Local Planning Authority."

Please request the following from the applicant/applicant's ecologist:

- A bat survey report that evidences the further surveys undertaken on trees T32, tree A and C within G11, in accordance with the recommendations set out in the PEA (page 22).
- Justification as to why the proposed cricket nets are required in this woodland area of the site. Have any other locations on the site been considered and if so, why were these discounted? The location of the new cricket nets in the southern area of the site will require the removal of a number of trees, three of which have been identified by the ecologist as having moderate suitability to support roosting bats. This location is also where the disused badger sett is located. This information is required so we can make an appropriate assessment of Policies CS13 and DM02.

I note that Appendix E contains an Arboricultural Impact Assessment. I haven't reviewed this part of the report and it should be considered by the Council's Tree Officer.

5.8 LBM Environmental Health (Contaminated Land)

With regards contaminated-land we recommend three-conditions, the first two, subject to prior agreement:

- 1) No development shall occur until a preliminary risk-assessment is submitted to the approval of the LPA. Then an investigation conducted to consider the potential for contaminated-land and shall be submitted to and approved in writing by the local planning authority. Reason: To protect the health of future users of the site in accordance with policy 9.10.6 of the London Plan 2021 and policy DM EP4 of Merton's sites and policies plan 2014.
- 2) No development shall occur until a remediation method statement, described to make the site suitable for, intended use by removing unacceptable risks to sensitive receptors, and shall be submitted to and approved in writing by the local planning authority. Reason: To protect the health of future users of the site in accordance with policy 9.10.6 of the London Plan 2021 and policy DM EP4 of Merton's sites and policies plan 2014.
- 3) Prior to first occupation, the remediation shall be completed and a verification report, produced on completion of the remediation, shall be submitted to and approved in writing by the local planning authority. Reason: To protect the health of future users of the site in accordance with policy 9.10.6 of the London Plan 2021 and policy DM EP4 of Merton's sites and policies plan 2014.

Additional comments (following additional discussion between the agent and Environmental Health Officer) 12/05/2022:

The report submitted satisfies requirements around an investigation. We await the results of a watching brief.

5.9 LBM Highway Officer

Highway comments are H10 and H13.

Officer comment:

In response to the comments the applicant has submitted a Construction Logistics Plan and Working Method Statement in order to seek to provide the information required by way of condition. The Highway Officer has reviewed this document and confirms no objection subject to deliveries not being within peak hours and some additional clarification on vehicle routes in and out of the site. An updated on this matter will be reported to the committee.

5.10 LBM Transport Planning

Proposed Development

The proposed development will comprise a new All-Weather Pitch (AWP), with associated areas of hardstanding / paving and pedestrian access from the existing Blagdons Sports Ground.

Vehicular access

The proposed development will be accessed off Beverley Way (B282), which is a two lane one-way road operating in a south to north orientation between Shannon's Corner gyratory to the south and the A3 Kingston Bypass to the north. To the north of the existing vehicle access, Beverley Way splits to form the A298 Bushey Road, providing access to A24 to the north-east.

Parking

The proposed parking provision for the development is as follows:

- 50 standard parking spaces;
- 2 accessible parking spaces;
- 2 coach spaces; and,
- 2 mini bus parking spaces.

Electric vehicle charging points (EVCP)

The parking spaces to be in accordance with London Plan policy regarding electric vehicle charging points (EVCP). As such, 20% of the parking spaces should benefit from active electric vehicle charging provision (including 2 disabled spaces) and 20% to be passive to be converted to active spaces in the future as and when required.

Pedestrian and cycle access

Pedestrian and cycle access will be via the existing vehicular site access, where shared surface is provided.

Trip Generation

The maximum number of cars onsite associated with the proposed AWP will be minimal considering the majority of the day to day use of the proposed AWP will be associated with the Emanuel school, would therefore not attract a significant amount of additional vehicle movements assuming the majority of the participants will be commuting by a coach or minibus.

Cycle Parking

The proposal fails to provide cycle parking to those who wish to cycle to the site. Adequate cycle parking (secure & undercover) should be provide in accordance with the London Plan Standards.

Recommendation: It is considered unlikely that the number of new vehicle trips generated from the proposed development will result in a significant adverse impact on the surrounding highway network.

Raise no objection subject to:

- Cycle Parking (secure & undercover) to be provided in accordance with the London Plan Standards.

5.11 LBM Flood Risk and Drainage Officer:

It appears as though there will be a new outfall to the Pyl Brook and the applicants will need to obtain an Flood Risk Activity Permit for this which I hope the EA have already let them know.

From a drainage point of view I'd suggest the following:

Condition:

The development shall be carried out in accordance with the mitigation measures set out in the submitted Flood Risk Assessment dated November 2021 and prepared by JPP Consulting Ltd, as follows:

- Surface water from the development will be discharged to the Pyl Brook at a rate of 2.5 l/s
- The proposed all weather pitch will include a permeable sub-base, and will be surrounded by 200mm barriers to contain all surface water discharge up to the 100yr plus 40% climate change event is contained on site.

Reason: To ensure satisfactory means of surface water drainage, to reduce the risk of flooding and to comply with the following Development Plan policies for Merton: policy 5.13 of the London Plan 2016, policy CS16 of Merton's Core Planning Strategy 2011 and policy DM F2 of Merton's Sites and Policies Plan 2014.

Condition:

The applicant shall prepare a SUDS management plan for the new SUDS proposal and ensure it forms part of the site maintenance plan.

Reason: To ensure satisfactory means of surface water drainage, to reduce the risk of flooding and to comply with the following Development Plan policies for Merton: policy 5.13 of the London Plan 2016, policy CS16 of Merton's Core Planning Strategy 2011 and policy DM F2 of Merton's Sites and Policies Plan 2014.

5.12 External consultees:

5.13 Sport England (Summary of comments)

- Objection raised due to reduction in size of existing cricket pitch which would reduce availability to local cricket clubs.
- Objection raised due to reduction in number of rugby pitches availability which would reduce availability to local rugby clubs.
- The benefit of the all weather pitch to hockey is not considered to mitigate for this impact.
- The level of community use offered is also not sufficient to mitigate for the impact.
- Advise that the proposed development prejudices the use of playing field and therefore is referable to the GLA.

See para 5.9 for Sport England's final comments on the proposed development.

Detailed comments below:

5.4 Sport England (17/01/2022):

Sport England - Statutory Role and Policy

It is understood that the proposal prejudices the use, or leads to the loss of use, of land being used as a playing field or has been used as a playing field in the last five years, as defined in The Town and Country Planning (Development Management Procedure) (England) Order 2015 (Statutory Instrument 2015 No. 595). The consultation with Sport England is therefore a statutory requirement.

Sport England has considered the application in light of the National Planning Policy Framework (NPPF) (in particular Para. 99), and against its own playing fields policy, which states:

'Sport England will oppose the granting of planning permission for any development which would lead to the loss of, or would prejudice the use of:

- all or any part of a playing field, or
- land which has been used as a playing field and remains undeveloped, or
- land allocated for use as a playing field

unless, in the judgement of Sport England, the development as a whole meets with one or more of five specific exceptions.'

Sport England's Playing Fields Policy and Guidance document can be viewed via the below link:

www.sportengland.org/playingfieldspolicy

The Proposal and Impact on Playing Field

The proposal is to create a new artificial grass pitch and replacement non-turf cricket practice facility with associated ball stop fencing and sports lighting. This will lead to a significant loss of grass playing field used for cricket and rugby.

Assessment against Sport England Policy/NPPF

In assessing this application, I consulted the relevant National Governing Bodies for sport.

The ECB stated that it is extremely concerned regarding the proposed loss of community access to Blagdons Sports Ground, as stated in the application. This would result in the loss of a community cricket site within a borough that has a significant deficit of cricket pitches - Merton's 2019 Playing Pitch Strategy concluded that there is a deficit of 2-5 cricket pitches.

The ECB state that the current use of the ground is significant and have detailed this fully below – in total, this would result in the loss of 42 community fixtures annually, along with 40 community training sessions from the site. While it is understood that Old Emanuel CC have now secured alternative provision, the significant deficit of pitches available to the community (including other cricket clubs in the borough) will be further exacerbated by the proposed loss of Blagdons Sports Ground. From an ECB perspective, the proposal is detrimental to community cricket within the borough.

2020 Community Cricket Fixtures at Blagdons Sports Ground

Old Emanuel CC:

- 20 fixtures on Saturdays (18 Surrey Championship League and 2 friendlies)
- 9 on Sundays (league matches, occasionally one or two KO rounds to be fixed last-minute)
- 1 x President's XI annual Fixture
- 6 on Wednesday evenings
- 40 x Tuesday and Thursday evening training sessions

Viscount CC:

- 6 x Sunday fixtures

The ECB strongly objects to this proposal due to the loss of capacity for senior cricket, the lack of regard to the ball strike trajectory assessment recommendations, and the non-compliance of the proposed mitigation. Additionally, the ECB is concerned regarding the proposed loss of community access to the site, as detailed above. The proposal would lead to the loss of 9 senior cricket wickets (a reduction of capacity from 15 senior wickets currently, to 6 in the future) – the “proposed cricket arrangement” illustrates that only 6 of the wickets provide senior boundaries with the new layout.

The proposal does not include a satisfactory resolution to the significant ball stop netting recommendations provided by Labosport's Ball Strike Trajectory Assessment. This assessment recommends that 18m height netting be provided to the western and eastern boundaries for recreational cricket activity. The report highlights that installation of the AGP would prejudice cricket activity at the site unless the full extent of the recommended ball stop netting requirements are fulfilled and the netting is installed ahead of the AGP being utilised. It is further noted that should the square be used for junior cricket activity only, 7m of netting is recommended, and this is also not addressed by the proposal.

The proposal also includes reference to cricket activity being possible on the hockey AGP to offset the loss of capacity referenced above. AGP surfaces are not compliant with ECB guidance for any level of cricket (whether junior/senior, softball or hardball) and therefore not appropriate mitigation for the loss of wickets on the square.

The RFU states that the site is currently home to Old Emanuel RFC (OERFC) as part of the Old Emanuel Association. The club reports;

- 4 senior teams including men's, women's and a veteran's team.
- Touch Rugby is played for all levels, men & women, and gives the local community access to non-contact rugby and introduction to the wider game.
- There is a large age grade section (350) drawn from the local community (New Malden and surrounds) (Age grade players are usually between 6 - 18)
- Discounted rates for students (Including 2 local Universities).
- In addition, Surrey RFU specifically makes particular reference to the huge impact that Old Emanuel RFC has made in the development of women and girls' rugby in the area

The LB Merton Playing Pitch strategy identifies a current shortfall in pitch provision for rugby union, particularly that of floodlit provision. The loss of community access to the current provision will further exacerbate this position, with particular reference to the floodlighting issue. The RFU maintains its original objection to this proposal as

community access is being lost to 3 full size rugby pitches and floodlit provision without any definition or proposal of mitigation. Subject to additional examination, the pitch capacity deficit identified within the LB Merton PPS may now actually present a worse picture, with the overplay of football now identified on the existing rugby pitches.

The proposed facility represents a net loss of community provision for rugby union in terms of total pitch capacity and floodlit pitch capacity. The RFU objects to the current proposal due to the loss of community pitch provision and access.

In terms of more general comments, regarding the new proposed pitch layout, RFU state that 2 no. rugby pitches should be constructed to comply with World Rugby Law 1 with regard to run-off provision.

Pages 13 and 32 of the Design and Access Statement make reference to the school's ability to deliver sports provision in light of the growth of the number of girls' sports teams. This commentary should be viewed in light of the proposal's impact on the OERFC ability to deliver its women and girls' operations.

RFU also request that the proposed overmarking of the rugby / football pitches be clarified. They note that football provision appears to be overmarked in one drawing (D19-041/DWG/0003) but omitted in D19-041/DWG/0009. It is understood that football is to be played during the Spring term, but it would be appreciated if this could be clarified – particularly if access to other schools and community is yet to be determined.

Whilst accepting the data presented within the Planning Statement document at page 13, section 5.1 is a summary of planning applications relating to the Blagdons ground use as a sports site, there is the omission of the successful planning application 07/P0414 for the provision (and subsequent installation) of floodlighting to one of the current rugby pitches. Without this, the document may lose context of the importance of the Blagdon's pitch stock (particularly the floodlighting) to community rugby in LB Merton.

In general there seems to be little reference to rugby and the loss of the 3 pitches to the community and particularly the impact on the women and girls' game for which OERFC has been such an advocate.

England Hockey stated that it recognises that there has not previously been a sand AGP on the site but can confirm that there is a need for sand based pitches within the London Borough of Merton. The Merton PPS states that there is a need for further hockey pitches within the borough and should there be community access to the pitch to some, but not all, of the identified shortfall in pitches will be met by this development and there will be the requirement for at least two further pitches in the borough.

We note that the proposed Hockey pitch is predominantly to be used for curriculum activity but understand the school is considering community use during the evenings and weekends and England Hockey would welcome the opening of the pitch to community clubs.

Should a sand AGP be built England Hockey recommends that it is built to meet FIH specifications and would wish to see the detailed plans for the pitch. To enable

community use during the winter, sports lighting will be required and if part of the plan then this should be a minimum of 350 lux to enable community use for both training and competition. England Hockey supports the development and recognises that mitigation for the loss of other sport facilities will be required.

Conclusion

In light of the above, Sport England objects to the application because it is not considered to accord with any of the exceptions to Sport England's playing fields policy or with Paragraph 99 of the NPPF. While the benefits to hockey are recognised and acknowledged, the fact that there is a significant detrimental impact on both cricket and rugby in an area where there are deficits for both sports means that this benefit does not outweigh the loss of playing field in the wider area, regardless of the situations of existing and/or previous users. This is confirmed by Merton's own evidence base for sport, the Playing Pitch Strategy. The lack of any confirmed community use is also extremely disappointing and concerning. While the grassed playing fields can be used for a number of sports, the proposed AGP is significantly less flexible in terms of what it can accommodate. Therefore I am unable to conclude that this meets any of our exceptions. I would note that this has already been explained in detail to the applicants at pre-application stage when Sport England's views were sought. Please note that our previous offer to meet with the applicants to discuss potential ways forward still stands.

Should your Council be minded to grant planning permission for the development then in accordance with The Town and Country Planning (Consultation) (England) Direction 2009, the application should be referred to the Secretary of State, via the Planning Casework Unit.

In addition, the application is also considered to prejudice the use of a playing field of more than 2 hectares of land and is therefore development of 'potential strategic importance' (PSI) as defined by The Town and Country Planning (Mayor of London) Order 2008. As such, a copy of this application must be sent to the Mayor of London for consideration.

5.5 Sport England comment (17/02/2022):

Many thanks for the additional information provided. I understand the NGBs have responded directly but will outline their responses here for completeness as I have given them significant weight in my response.

Sport England wishes to maintain its objection to this application.

The ECB continues to object to the principle of lost community access to the site, given the substantial deficit of cricket pitches within the borough as concluded by Merton's 2019 Playing Pitch Strategy. They acknowledge that OEA has identified an alternative site, however there are significant number of cricket clubs and teams within the borough that are seeking access to suitable cricket sites such as this.

The ECB are also concerned that the proposal continues to reference to cricket activity being possible on the hockey AGP to offset the loss of capacity referenced above. To reiterate, AGP surfaces are not compliant with ECB guidance for any level of cricket (whether junior/senior, softball or hardball) and therefore not appropriate mitigation for the loss of wickets on the square. This is highlighted by the school's

reference in this response to interest from football and hockey clubs to access the AGP – it is notable that no community cricket partners are mentioned, since the surface is not suitable for cricket activity. It therefore does not provide mitigation for the loss of community cricket activity and a proposed reduction in the size of the cricket square at the school due to this application.

The ECB are also concerned that the proposal continues to provide very limited detail regarding the proposed netting installation to mitigate against ball strike risk with simultaneous activity on the cricket square and the AGP. Labosport's assessment recommends that 18m height netting be provided to the western and eastern boundaries for recreational cricket activity. The report highlights that installation of the AGP would prejudice cricket activity at the site unless the full extent of the recommended ball stop netting requirements are fulfilled and the netting is installed ahead of the AGP being utilised. It is further noted that should the square be used for junior cricket activity only, 7m of netting is recommended, and this is also not addressed by the proposal. The only additional detail provided within the applicant's response refers to temporary netting, with no detail regarding the height, management, and maintenance of the netting. This is unsatisfactory as it does not address the specific recommendations for 18m high netting to appropriately mitigate ball strike risk. Temporary netting at 7m high would prejudice cricket activity at the site and therefore is objected to by the ECB.

The RFU also wish to maintain their objection. They state that OERFC report that there is an indefinite delay on the move to the Raynes Park site due to the requirement of additional public consultation. Local Authority colleagues may have additional information on timescales, but presently the RFU would comment that until these are confirmed, OERFC demand for match play and training facilities is unmet for the forthcoming 22/23 season and the remnant of the 21/22 season. The RFU understands that OERFC is in discussion with the Blagdon site owners as to possible solutions and the RFU would support this dialogue wherever and however possible. RFU highlight the fact that the applicant's reference to Touch Rugby is reliant on the Community Use Agreement. With reference to Appendix 5: Schedules 1 and 2 of the response, can more detail be provided as to the confirmation of the facilities available for community use and the timing and pricing of this access? Without these details, this CUA is impossible to assess.

RFU also state that the proposed relocation of the OERFC to Raynes Park Sports Ground nominally provides access to two senior pitches without permanent floodlighting; a net reduction from the provision at Blagdons in total pitch capacity and mid-week access to floodlit training. The masterplan for the Raynes Park site is unknown currently due to the mix of sports and could be limited to a maximum of 2 full size rugby pitches. Raynes Park does not have permanent sports lighting to RFU recommendations, the installation of which would require planning consent. The relocation of OERFC from Blagdons to Raynes Park is a net reduction in match play and training provision currently. This exacerbates an existing deficit of total pitch capacity and floodlit training facility in LB Merton. Sport England would also note that Raynes Park Sports Ground is an existing playing field site and therefore simply moving OERFC there will not address the existing deficits in playing field.

The RFU state that team and Age Grade bandings will be changing for the Girls' game for the season 22/23. This will increase the demand for match play pitch space and overall pitch capacity. With a net reduction in pitches, the club will find a challenge in meeting pitch capacity needs. Accepting that omission of the planning

application record for the floodlights was not intentional, their presence on site and the lack of confirmed floodlighting to the same standard at the proposed alternative venue nonetheless represents a net reduction of this facility.

RFU also point out that the table at Page 7 refers to the total no. of pitches increasing in mitigation to the proposal. The provision for rugby;

- o Reduces by 1no. '15 v 15' (applicant's definition) pitch
- o Represents a reduction of floodlit capacity.
- o Is proposed to be mitigated by the additional provision of 1 no. 'mini' pitch.

This provision is not appropriate to mitigate the net loss of pitch capacity as previously described. The above mitigation suggested is also dependent upon community access as defined by the draft Community Use Agreement. As set out previously, the CUA provided has no specific detail on timings available and pricing, which is crucial in order to assess the value of this document. As additional commentary relating to this and other site applications under discussion, the RFU would again request that a holistic and strategic approach be taken to the provision of sporting facility in the borough to appropriately mitigate any loss of playing fields and ancillary facilities.

Having carefully considered the responses by the RFU and ECB, Sport England considers that, despite the acknowledged potential benefits to hockey, these do not outweigh the significant impacts on both sports when considering deficits in the area as set out in the Merton PPS (2019). I also acknowledge potential benefits to netball and tennis. However, as with cricket and rugby, the proposed level of community use that would be offered for any of the sports on site is unclear due to the fact that the CUA submitted does not include proposed times or costs. In order to assess this document properly, I would require a clear schedule to be submitted stating both the exact times/days the facility will be available outside of school core hours/term times. Given the extent of the impact on cricket and rugby here, I would also suggest that such an agreement should in fact cover the grassed playing fields and ancillary facilities on site as well as the proposed new facility in order to go some way to meeting Exception 5.

I do not consider that the benefits of the proposed artificial pitch, which will only support a limited number of sports, outweighs the loss of playing field here in an area where there is clear evidence of deficits and where the level of community use being offered remains unclear.

Having considered all the information available Sport England remains of the opinion that this application does not meet any of our policy exceptions.

5.6 Sport England comments (04/04/2022)

Having considered this, I don't feel that the NGB comments have been addressed – RFU and ECB each made a number of points previously and I can't see that many of these have been responded to?

I note the commentary around the netting; obviously any such netting would require planning permission and therefore could not be dealt with by way of a condition. Has the applicant submitted new plans showing this netting to amend their application?

In addition to this, I note that the email thread mentions offering only six hours a week community use (no updated CUA document has been submitted as I mentioned previously). We would not consider community use of only six hours a week to be anywhere near that required to go some way towards meeting E5. Generally schools that offer community use offer every weekday evening and weekend unless they need the facility themselves. Is Touch Rugby identified within these hours of use? We would also need specific details around charges and who would be charged what, as the comments the applicants have provided so far are fairly vague.

England Hockey have also stated that they would require community access every weekday until 10pm and from 8am to 9pm on weekends which would match current access to surrounding pitches. EH tell me that Wimbledon HC confirm that planning permission on the pitches at Ricards Lodge, Raynes Park and Kings College School all allow for community access until to 10pm. Access to the pitch at 6pm will allow for junior activity in the early slot. A 10pm finish will allow for adult hockey training to take place which within London boroughs tends to start later in the evening to accommodate travel time.

Please consider that Sport England's objection to this proposal still stands.

5.7 *Sport England comments (11/04/2022):*

As far I can see it looks like they have just offered a few extra hours on a Sunday for the AGP only. This isn't acceptable either. I understand they've suggested they might be able to offer more on an ad hoc basis by writing in with their availability each term but this is not much use to local groups and clubs who will need a bit more certainty as to when they're able to use the facility. I don't think my other queries below have been addressed either as far as I can tell.

I'm not really clear why community use is such an issue for them, it is rare for schools themselves to require the facility so much out of school hours and most schools including those such as Eton and Harrow are able to offer a decent level of community use so I am unsure what the issue is here.

Thanks for the clarification around the cricket nets – obviously we can only take into account what has been submitted as part of the planning application.

5.8 *Sport England comments (06/05/2022):*

I have discussed this with colleagues and while it's welcomed that the cricket nets will be part of the community use agreement, to have community use only available for three days a week is not considered an acceptable level of mitigation considering the impact that the proposed AGP will have in terms of lost playing field and in particular the impact on cricket and rugby. The fact that there is currently no community use is not relevant because we are seeking it to mitigate against the loss of flexible grass playing field by increasing it to a decent level. Community groups and clubs will not be able to plan if potential extra hours are only being offered on a term-by-term basis and I cannot consider this as mitigation if there are no guarantees.

I don't accept that amenity issues for the groundsman are a justifiable reason for the small amount of community use offered – presumably he will have been employed on

the understanding that his role is to facilitate sport on the site and this has certainly never been suggested as an issue to me on any other sites with live-in groundspeople. If time off is an issue then the school could manage that by using an alternative member of staff which could easily be funded by the extra revenue that the community use would generate.

If the school is not willing to offer a good level of community use (ie most if not all weekday evenings and weekends) then this simply does not go far enough to mitigate the impact of the new AGP and Sport England will maintain its objection as it is not considered to meet our policy exceptions.

5.9 Sport England comments (03/08/2022):

In summary – Sport England does not believe that the level of community use being offered here is sufficient to mitigate the losses suffered by rugby and cricket.

These losses have been addressed at length in my previous responses. RFU have come back to me again and they point out that the fact that the school intend to use the pitches as set out does not address the net loss of three senior pitches, one of which is lit. RFU would also question whether OERFC had access five nights a week as part of a lease providing exclusive use? The OERFC has lost access to lit training on an entire senior pitch and this is not being replaced. The proposed hockey pitch will not be compliant for contact rugby. Regarding the point around OEA having a new ground and writing in support of this application, the RFU believe that OERFC is in negotiation to occupy a new site in conjunction with another partner sporting org – this is not the OEA. Should this agreement be successful, the new ground would only provide two senior pitches neither of which are lit. Therefore, there remains a net loss of one pitch capacity. RFU have made this point previously.

Regarding the VC club, I am awaiting clarification on this point from the ECB. However, if it is the case that the use was informal/part of a verbal agreement rather than a written one, this is very common and it remains that the use still took place and the facilities fulfilled a need. I will let you know when I hear more from the ECB on this point, however I consider it a minor one as my focus is on ensuring adequate mitigation for a loss of flexible grassed playing field here.

Given the above concerns, Sport England would require a very good level of community use to be offered to mitigate this loss. Sport England generally requires this where there is a loss of playing field to a less flexible sport facility (such as the hockey pitch proposed here).

In terms of community use, Sport England would require the facilities to be available at times when the school does not require them for sport (ie most weekday evenings and weekends). The most benefit to sport and the community is when the facilities are regularly available at times that are useful to local sports clubs and groups. The proposed arrangement is for two weekday evenings and all day Sundays. This would be a very low level of community use compared to most CUAs which generally include most weekday evenings and weekend days (with the understanding that occasionally school events may take precedence). Is the school intending to regularly use the site for sport three weekday evenings a week and all day every Saturday?

Due to the losses outlined by the RFU both previously and above (and previously outlined by ECB) and the deficits in playing field in Merton and surrounding areas,

Sport England would require a much better level of community use to be of the opinion that this application qualified for its policy Exception 5 (a sport facility of sufficient benefit to the community) and was in line with the NPPF. I believe England Hockey would also require a better level of community use in order to fully support this application.

As I have previously set out, this is not purely an issue of the school's use; we also have to take into account the impact on sport on the wider community. It is undeniable that there has been a negative impact on rugby and cricket as previously set out.

The agent's response that there is likely to be use by the school on evenings and weekends is vague – if this is the case, I would need to see a full timetable confirming which facilities have regular use on which days.

I disagree that it is not possible or reasonable for the school to commit to more use than they are currently offering. Sport England deals with hundreds of CUAs each year and it is usual for this level of use to be agreed at various different types of school. If there are any concerns then a higher level of use can be reviewed after a year, rather than suggesting a minimum and offering to review it later, as this offers no comfort that the level of community use required to mitigate the loss of playing field here will ever be achieved.

Regarding the point around the groundsman, community use is a source of income and should cover maintenance costs for the facilities however the school wish to manage them.

Regarding pitch capacity, it is not appropriate to discuss this purely in terms of numbers of pitches as this does not recognise that different pitches will have different sizes and capacities. Instead this is calculated in Match Equivalent Sessions - this is covered in Merton's Playing Pitch strategy which looks at pitch capacity across the borough and shows clear deficits. This application would result in the loss of senior cricket and rugby pitches for the local community.

The AWP will be suitable for performance levels of play for hockey, though recreational netball and tennis could also take place on it – it is clearly primarily a hockey pitch. It will however be completely unsuitable for rugby and cricket.

5.10 Greater London Authority Stage 1 referral

<p>Strategic planning application stage 1 referral</p> <p>Town & Country Planning Act 1990 (as amended); Greater London Authority Acts 1999 and 2007; Town & Country Planning (Mayor of London) Order 2008.</p>
<p>The proposal</p> <p>Demolition of two existing outbuildings and creation of a new all-weather sports pitch to be enclosed by ball stop fencing and entrance gates, with associated floodlight system and tree planting.</p>
<p>The applicant</p> <p>The applicant is Emanuel School and the Agent is HGH Consulting.</p>
<p>Strategic issues summary</p> <p>Metropolitan Open Land: The proposed use meets the exceptions test specified by the NPPF. Therefore the principle of development is acceptable in terms of development of MOL (paragraphs 16 to 22).</p> <p>Land use principles: The proposed development would result in the loss of some seasonal sport facilities but would make new provision for year-round participation in a wider variety of sport. This may be acceptable subject to further information regarding replacement facilities for seasonal sports and community access (paragraphs 23 to 32). Other issues on transport; sustainable development; and environmental issues also require resolution prior to the Mayor's decision making stage.</p>
<p>Recommendation</p> <p>That Merton Council be advised that the application does not yet comply with the London Plan for the reasons set out in paragraph 61. Possible remedies set out in this report could address these deficiencies. The Mayor does not need to be consulted again if the borough decides to refuse the application.</p>

Context

1. On 28 April 2022 the Mayor of London received documents from Merton Council notifying him of a planning application of potential strategic importance to develop the above site for the above uses. Under the provisions of the Town & Country Planning (Mayor of London) Order 2008, the Mayor must provide the Council with a statement setting out whether he considers that the application complies with the London Plan, and his reasons for taking that view. The Mayor may also provide other comments. This report sets out information for the Mayor's use in deciding what decision to make.

2. The application is referable under the following Category/categories of the Schedule to the Order 2008:

- 3Cb "Development may prejudice use of playing field 2 hectares or more which has been used at any time in last 5 years before application".

3. Once Merton Council has resolved to determine the application, it is required to refer it back to the Mayor for his decision as to whether to direct refusal; or, allow the Council to determine it itself. In this case, the Council need not refer the application back to the Mayor if it resolves to refuse permission.

4. The Mayor of London's statement on this case will be made available on the GLA's public register: <https://planning.london.gov.uk/pr/s/>

Site description

5. The application site comprises an open sports field, known as Blagdons Sports Ground, located in New Malden. The site is bound by Pyl Brook to the north; Beverley Way to the east; Shannon Commercial Centre industrial development to the south; and Beverley Brook to the west.

6. The site is in use by Emanuel School to provide sports facilities for students for the purposes of rugby in the autumn; football in the spring; and cricket in the summer.

7. The planning statement states that the site has previously been used in conjunction with the Old Emanuel Association (OEA), however an agreement with OEA means that OEA ceased use of the site and facilities in March 2022.

8. The site is designated Metropolitan Open Land (MOL) and is located in Flood Zones 2 and 3. The site is adjacent to Pyl Brook and Beverley Way which both form a Site of Importance for Nature Conservation (SINC). The application site itself is not designated SINC.

Details of this proposal

9. The application is for the creation of a new 5,930 sq.m. all weather pitch (AWP); erection of a 4.5m high ball stop fence and entrance gates; and 8 x 15m high LED flood light. The AWP and ancillary works would be located at the south eastern corner of the site.

10. The scheme would involve demolition of two outbuildings which are currently used for storage. There would be no change to site access and parking arrangements.

11. The AWP would provide facilities for year-round hockey; tennis; netball; and cricket. Proposed hours of use would be 07:00 to 22:00 Monday to Sunday, with floodlights turned off by 22:30.

Case history

12. There is no strategic case history.

Strategic planning issues and relevant policies and guidance

13. For the purposes of Section 38(6) of the Planning and Compulsory Purchase Act 2004, the development plan in force for the area comprises the Merton Core Planning Strategy (2011); Merton Sites and Policies Plan and Policies Maps (2014) and, the London Plan 2021.

14. The following are also relevant material considerations:

- The National Planning Policy Framework and National Planning Practice
- Guidance;
- Draft New Merton Local Plan.

15.The relevant issues, corresponding strategic policies and guidance (supplementary planning guidance (SPG) and London Plan guidance (LPG)), are as follows:

- Good Growth - London Plan;
- MOL - London Plan;
- Playing fields - London Plan;
- Open land - London ;Plan; All London Green Grid SPG;
- Education facilities - London Plan; Social Infrastructure SPG;
- Sports facilities - London Plan; Social Infrastructure SPG;
- Urban design - London Plan; Character and Context SPG;
- Inclusive access - London Plan; Accessible London: achieving an inclusive environment SPG; Public London Charter LPG
- Sustainable development - London Plan; Circular Economy Statements LPG; Whole-life Carbon Assessments LPG; 'Be Seen' Energy Monitoring Guidance LPG; Mayor's Environment Strategy;
- Transport and parking - London Plan; the Mayor's Transport Strategy;
- Equality - London Plan; the Mayor's Strategy for Equality, Diversity and
- Inclusion; Planning for Equality and Diversity in London SPG;
- Biodiversity - London Plan; the Mayor's Environment Strategy; Preparing
- Borough Tree and Woodland Strategies SPG.

Land use principles

Metropolitan open land

16.The application site comprises designated MOL and is currently in use as a sports playing field for a school.

17.London Plan Policy G3 affords MOL the same status and level of protection as Green Belt. Therefore, MOL should be protected from inappropriate development in accordance with the requirements of the National Planning Policy Framework (NPPF), in which paragraph 137 of the NPPF states "the fundamental aim of Green Belt policy is to prevent urban sprawl by keeping land permanently open; the essential characteristics of Green Belts are their openness and their permanence".

18.Paragraph 147 of the NPPF advises that inappropriate development is, by definition, harmful to the Green Belt/MOL and should not be approved except in very special circumstances.

19.Paragraph 148 of the NPPF states that local planning authorities should ensure that substantial weight is given to any harm to the Green Belt/MOL when making planning decisions and confirms that very special circumstances will not exist unless the potential harm to the Green Belt/MOL by reason of inappropriateness, and any other harm resulting from the proposal, is clearly outweighed by other considerations.

Exceptions test

20.Paragraph 149 of the NPPF regards the construction of new buildings in Green Belt/MOL as inappropriate. Exceptions to this specified by the NPPF include:

“the provision of appropriate facilities (in connection with the existing use of land or a change of use) for outdoor sport, outdoor recreation, cemeteries and burial grounds and allotments; as long as the facilities preserve the openness of the Green Belt and do not conflict with the purposes of including land within it”

21. The proposed development is for outdoor sport and recreation facilities which would be used in connection with the existing sport and recreation use of the land. The proposed AWP would be enclosed by a fence which would be visually permeable and would involve the erection of eight free-standing flood lights. The development would not be enclosed by a roof. It is also noted that the scheme involves the demolition of two outbuildings.

22. GLA officers are satisfied that the proposed development constitutes the provision of appropriate facilities in connection with the existing use of the land for outdoor sport and recreation. It is considered that the design and appearance of the development would be open in character and appearance, and thereby would preserve the openness of the MOL and would not conflict with the purposes of including land within it. As such, GLA officers consider that the proposed development would meet the NPPF exceptions test for development on MOL in accordance with paragraph 149 of the NPPF.

Sport and recreation facilities

23. Paragraph 99(C) of the NPPF and London Plan Policy S5(C) state that existing sports and recreational land (including playing fields) and facilities should be retained unless the development is for an alternative sports and recreational provision, the benefits of which clearly outweigh the loss of the current or former use.

24. The proposed development is for the creation of an AWP and ancillary works at an existing sport playing field. The scheme would result in the approximate net loss of two large rugby union pitches in autumn; two smaller football pitches in spring; and 15 cricket pitches in summer. However, the scheme would result in the gain of facilities for year-round hockey; tennis; netball; and cricket (as existing cricket is only available in summer). The scheme also provides some seasonal facilities for rugby (autumn), football (spring) and cricket (summer), albeit less than existing to make space for the proposed new facilities.

25. The planning statement details the school's need for the proposed new facilities, illustrating historic and projected demand for the various sports offered by the school, and detailing that the school is not able to meet these needs with its current facilities. The planning statement also specifies that historic lack of growth in some sports, such as hockey, is not due to an absence of demand but is the result of a lack of suitable available facilities. The evidence provided in the planning statement suggests that the proposed development would be of significant benefit to the school students as the scheme would enable more students to participate in sports and they would not be seasonally limited for hockey, tennis, netball and cricket. The provision of facilities that would enable year-round sport participation and which would make sport and recreation accessible to an increased number of students is welcomed.

26. Prior to Stage 2 the applicant must clearly demonstrate that where facilities would

be reduced under the scheme, suitable provision exists elsewhere, and that the benefit of the proposed on-site provision would outweigh the proposed loss of seasonal rugby, football and cricket facilities. Additional information should be submitted to demonstrate either the school's reduced demand for seasonal rugby, football and cricket facilities or, demonstrate that the school's demand for these sports is sufficiently catered for in another suitable location.

27. Notwithstanding the above, it is acknowledged that Paragraph 5.5.2 of the London Plan identifies that the provision of artificial grass pitches within the capital is not currently meeting demand, with only 55% of demand being met. The London Plan also specifies that this level of unmet demand is projected to increase towards 2041 if no new facilities are provided. The proposed AWP, which would comprise artificial grass, would contribute towards meeting this need and this is supported in principle. Nevertheless, to ensure the AWP helps to meet the more general need for artificial grass pitches, opportunities for community access must be maximised (refer below). In accordance with Paragraph 5.5.2 of the London Plan, the opening hours of the proposed AWP should also be maximised in order to increase access to this in-demand facility type.

28. In addition to the scheme contributing towards meeting demand for artificial grass pitches as identified by the London Plan, the Council should also ensure that the proposals would contribute towards meeting local needs for sport and recreation facilities.

Community access

29. London Plan Policy S5 states that proposals for sports and recreation facilities should maximise the multiple use of facilities and encourage the co-location of services between sports providers, schools, colleges, universities and other community facilities. Paragraph 5.1.10 of the London Plan specifies that shared use could include schools opening their facilities out of hours for use by the community.

30. It is understood that the site is operated by Emanuel School, with access to the site being gated and not accessible to the general public. Access can be, and has historically been, provided to local schools and community groups on a pre-arranged basis. It is understood that until March 2022, OEA had access to use the site for its sporting teams, however, this has now ceased.

31. As stated in paragraph 27, opportunities for community access, as well as opening hours, must be maximised to ensure compliance with London Plan policies. It is noted that since the original submission of the application, there have been amendments to the scheme, including a Community Use Agreement (CUA) which has evolved in response to concerns raised by Sport England regarding community access to help mitigate loss of seasonal playing fields. It appears that to date, despite the updates to the CUA, Sport England still have concerns regarding the lack of hours proposed for community access.

32. Prior to Stage 2, the applicant should provide details of the days and times OEA previously utilised the site and its facilities and ensure that, despite OEA's agreement with the school ceasing, as a minimum there would not be a reduction in that baseline of community use hours offered as part of the CUA. Further to this the application should seek to improve on the previous arrangement in line with the London Plan objective to maximise community access.

Transport

Healthy streets

33. The proposed development will provide improved sporting facilities. Given the large existing provision of car parking and anticipated intensification in sporting use, it is unclear how the development will support the Healthy Streets indicators in terms of improving the public realm, reducing car dominance and promoting sustainable and active travel. The Transport Assessment (TA) should be amended to demonstrate this and should also include an Active Travel Zone (ATZ) assessment in line with TfL guidance. It is also noted that an accident analysis has not been undertaken, which the TA should include to identify any improvements required to support the Mayor's Vision Zero approach.

Trip generation

34. The trip generation methodology is acceptable; however it only includes trips made by vehicle. TfL requires all modes of transport to be included in the trip generation assessment, including a breakdown of public transport by mode and direction to determine the impact.

Car parking

35. The proposed development will provide 50 standard parking spaces, 2 accessible Blue Badge (BB) spaces, 2 coach spaces and 2 minibus spaces. The existing access off Beverley Way will be utilised.

36. The vehicle trip generation indicates that a maximum of 50 cars are expected to visit the site across a two-hour period on Saturdays only. It is considered that users of the sports ground should be able to use sustainable and active modes of transport given the proximity of Motspur Park rail station, the local cycle network and bus routes. Furthermore, given the land use, it is considered that a number of these trips are likely to be pick-up / drop-off and will not be parked at the site simultaneously, which further negates the need for this level of parking. Therefore, TfL requires the parking provision to be significantly reduced to reflect demand and support the sustainable travel objectives of the London Plan.

37. Further to the above, some provision must be made for Electric Vehicle (EV) charging infrastructure in line with London Plan Policy T6.

38. A Car Parking Design and Management Plan demonstrating how parking will be managed, monitored and repurposed in the future should be secured by condition.

Cycle parking

39. It is unclear whether any cycle parking will be provided. The site area is 8,000sqm and therefore a minimum of 80 short-stay cycle parking spaces should be provided to comply with London Plan Policy T5.

40. The number of staff should be clarified so that the short-stay cycle parking provision can be identified.

41. At least 5% of the cycle parking spaces should be for larger and adapted cycles in line Chapter 8 of the London Cycling Design Standards (LCDS).

42. Detailed layout plans demonstrating that all cycle parking have been designed and laid out in line with the LCDS should be secured by condition.

Sustainable development

Energy strategy

43. London Plan Policy SI 2 requires development proposals to reduce carbon dioxide emissions in accordance with the energy hierarchy. Energy comments have been provided to the applicant and the Council in full under a separate cover. The applicant should respond to this detailed note to address outstanding issues to ensure compliance with the London Plan in advance of the borough planning committee to ensure that any conditions can be appropriately secured.

44. In summary, the applicant must confirm the energy uses on site and seek to minimise CO2 emissions with energy efficiency measures, including maximising opportunities for the inclusion of PV.

45. Conditions should be appropriately secured to ensure the development meets London Plan standards.

Whole Life Carbon

46. It appears that no Whole Life-cycle Carbon (WLC) assessment has been submitted. All applicants are expected to submit a completed WLC assessment template (as an Excel document, not a PDF) and follow the GLA WLC guidance; both of which are available here: <https://www.london.gov.uk/what-we-do/planning/london-plan/london-plan-guidance/whole-life-cycle-carbon-assessments-guidance>

47. The applicant should submit a WLC assessment template in full. This is important to allow results to be recorded and tracked through to the post-construction stages, and to allow a proper review of the results against material quantities and other assumptions made.

48. As per the GLA 'Whole Life-cycle Carbon Assessment – March 2022 – guidance document' this assessment should comply with EN 15978 and cover all building elements.

Circular Economy

49. In accordance with London Plan Policy SI 2, applicants are expected to calculate and reduce whole life-cycle carbon (WLC) emissions to fully capture the development's carbon footprint.

50. In summary, in order to demonstrate compliance with Policy SI 2 a WLC assessment should be submitted. This is required prior to Stage 2. A monitoring report should be secured by condition.

Environmental issues

Flood risk

51.London Plan Policy SI 12 requires development proposals to ensure flood risk is minimised and mitigated and that residual risk is addressed.

52.The site is located within Flood Zones 2 and 3 and the application was supported by a Flood Risk Assessment (FRA) which appears to have been reviewed by the LPA and the Environment Agency which recommended conditions relating to construction ground levels and barrier installation.

53.Planning conditions should be appropriately secured by the LPA to ensure the development meets London Plan standards in relation to flood risk and management.

Drainage

54.London Plan SI 13 seeks to ensure developments achieve greenfield run-off rates and avoid impermeable surfaces where possible.

55.The LPA should secure planning conditions to ensure the development meets London Plan standards in relation to drainage.

Biodiversity

56.London Plan Policy G6 requires development proposals to manage impacts on biodiversity and aim to secure net biodiversity gain. London Plan Policy G7 seeks to ensure that, where possible, existing trees of value are retained and, where the granting of a planning permission necessitates the removal of trees, adequate replacement is provided.

57.The Council should ensure that planning conditions secure the protection of retained vegetation, management of floodlighting and secure the mitigation measures recommended in the Preliminary Ecological Appraisal.

Local planning authority's position

58.Merton Council planning officers are currently assessing the application. In due course the Council will formally consider the application at a planning committee meeting.

Legal considerations

59.Under the arrangements set out in Article 4 of the Town and Country Planning (Mayor of London) Order 2008 the Mayor is required to provide the local planning authority with a statement setting out whether he considers that the application complies with the London Plan, and his reasons for taking that view. Unless notified otherwise by the Mayor, the Council must consult the Mayor again under Article 5 of the Order if it subsequently resolves to make a draft decision on the application, in order that the Mayor may decide whether to allow the draft decision to proceed unchanged; or, direct the Council under Article 6 of the Order to refuse the application. In this case, the Council need not refer the application back to the Mayor if it resolves to refuse permission. There is no obligation at this

stage for the Mayor to indicate his intentions regarding a possible direction, and no such decision should be inferred from the Mayor's statement and comments.

Financial considerations

60. There are no financial considerations at this stage.

Conclusion

61. London Plan policies on sport and recreation facilities; education facilities; transport, sustainable development; and the environment are relevant to this application. The proposed provision of an AWP artificial grass pitch to be primarily used by a school to offer a greater range of sport options to students could be supported subject to the maximisation of the hours of operation and community access to the facilities. The application does not fully comply with the London Plan as summarised below:

- Metropolitan Open Land: The proposed development would satisfy the exceptions test and would be open in character and appearance so as to preserve the openness of the land and the purposes of including it within MOL.
- Land use principles: Further information is required regarding community access and assurance the level of sport provision is suitable to meet demand.
- Transport: Further information is required regarding ATZ; trip generation; car parking; and cycle parking.
- Sustainable development: Further information is required regarding the energy strategy and WLC assessment. A circular economy statement is also required.
- Environmental issues: The Council should secure appropriate conditions relating to flood risk; drainage and biodiversity.

5.11 Greater London Authority (Additional comments 03.08.2022):

In terms of the proposed community hours, I can see there is an uplift from the previous in terms of hours available for both weekdays and weekends which translates to an overall uplift in hours to be made available (noting that the VCC was not previously authorised to use the grounds). This also translates to an increase to the number of weeks in a year the facilities would be made available due to the nature/different maintenance requirements of the AWP, and a wider variety of sports available to be played by community groups. This would support London Plan Policy S5 and Paragraph 5.1.10 (as specified in the GLA's Stage 1 report) and is considered a positive improvement.

I am interested to know the views of Sport England on this, and how far this goes in addressing outstanding concerns and SE's objection.

5.12 Transport for London (05/07/2022)

I write to provide detailed strategic transport comments on this application reference 21/P4190. These provide more detail on the matters raised in the GLA Stage 1 Planning Report GLA/2022/0343 dated 27/06/2022. Please note that these are

additional also to any response you may have received from my colleagues in infrastructure or asset protection and from TfL as a party with a property interest.

The London Plan (LP) was adopted on the 2nd March 2021. TfL expects all current planning proposals to consider the policies set out within this document.

Proposed Development

The proposal entails the construction of an All Weather Pitch (AWP) and sports facilities, which will be used by Emanuel School and partners (8,000sqm).

Site Description

The site is bound by Pyl Brook to the north, Beverley Brook to the west and Beverley Way to the east, which is a slip road off the A3 Kingston Bypass and forms part of the Transport for London Road Network (TLRN). TfL is the highway authority for the TLRN and is therefore concerned about any proposal which may affect the performance and/or safety of the TLRN. The A298 Bushey Road is also to the east of the site, which forms part of the Strategic Road Network (SRN).

The nearest station is Motspur Park Station, which is approximately 1km away and has rail services to London Waterloo. There are 4 bus routes within an acceptable walking distance of the site. The site therefore has a Public Transport Access Level (PTAL) of 1b/2, on a scale of 0 to 6b where 6b is the highest.

The site also well connected to the cycle network with Cycleway 31 approximately 370 metres north of the site and National Cycle Route 208 approximately 1.5km to the north-east of the site.

Healthy Streets

The proposed development will provide improved sporting facilities. Although supported, given the large provision of car parking and loss of green estate, it is unclear how the development will support the Healthy Streets indicators in terms of improving the public realm, reducing car dominance and promoting sustainable and active travel. The Transport Assessment should be amended to demonstrate this and should also include an Active Travel Zone (ATZ) assessment in line with TfL guidance. It is also noted that an accident analysis has not been undertaken, which the TA should include to identify any improvements required to support the Mayor's Vision Zero approach.

Trip Generation and Public Transport Impact

The trip generation methodology is acceptable; however it only includes trips made by vehicle. TfL requires all modes of transport to be included in the trip generation assessment, including a breakdown of public transport by mode and direction to determine the impact.

It is understood that the maximum vehicle accumulation will result on Saturdays, with two coaches, two mini buses and up to 50 cars every two hours between 07:00-1200 and 12:00-1800. During the weekday morning and evening peaks there will be limited impact on the transport network.

Car Parking and Access

The proposed development will provide 50 standard parking spaces, 2 accessible Blue Badge (BB) spaces, 2 coach spaces and 2 minibus spaces. The existing access off Beverley Way will be utilised.

The vehicle trip generation indicates that a maximum of 50 cars are expected to visit the site across a two-hour period on Saturdays only. It is considered that users of the sports ground should be able to use sustainable and active modes of transport given the proximity of Motspur Park rail station, the local cycle network and bus routes. Furthermore, given the land use, it is considered that a number of these trips are likely to be pick-up / drop-off and will not be parked at the site simultaneously, which further negates the need for this level of parking. Therefore, TfL requires the parking provision to be significantly reduced to reflect demand and support the sustainable travel objectives of the LP.

Further to the above, some provision must be made for Electric Vehicle (EV) charging infrastructure in line with Policy T6 of the London Plan.

It is understood that the existing vehicular access from Beverley Way will be retained.

A Car Parking Design and Management Plan demonstrating how parking will be managed, monitored and repurposed in the future should be secured by condition.

Cycle Parking

It is unclear whether any cycle parking will be provided. The site area is 8,000sqm and therefore a minimum of 80 short-stay cycle parking spaces should be provided to comply with Policy T5 of the LP.

The number of staff should be clarified so that the long-stay cycle parking provision can be identified.

At least 5% of the cycle parking spaces should be for larger and adapted cycles in line Chapter 8 of the London Cycling Design Standards (LCDS).

Detailed layout plans demonstrating that cycle parking has been designed and laid out in line with the LCDS should be secured by condition.

Construction

After review of the Construction Logistics Plan and Working Method Statement (CLPWMS), it is unclear how long the works are expected to last.

It is understood that all vehicles will use the existing vehicular access from Beverley Way and be loaded / unloaded on-site. All vehicles will be able to access and egress the site in forward gear, which is supported. The CLP should include a pedestrian and traffic management plan to ensure that all road and footway users are managed safely during construction.

The final CLP should be produced having regard to TfL's best practice guidance and be secured by condition.

Travel Plan

A Travel Plan (TP) should be prepared and contain measures to promote sustainable and active travel.

The TP should be secured, enforced, monitored and reviewed by the applicant as part of the s106 in line with LP Policy T4.

Mayoral CIL

This development will be liable for the Mayor of London's CIL based on the MCIL2 charging rate of £60 per square metre.

Summary

Further work is required to confirm that the application complies with the transport policies in the LP and is summarised below:

1. The TA should demonstrate how the development will support Healthy Streets indicators and identify Vision Zero improvements.
2. The trip generation should include all modes of transport.
3. The number of parking spaces should be reduced.
4. EV charging facilities should be provided.
5. Cycle parking should be provided in line with Policy T5 of the LP.
6. The number of staff should be confirmed.
7. At least 5% of the cycle parking spaces should be for larger, adapted cycles.
8. Layout plans of cycle parking to demonstrate compliance with the LCDS should be secured by condition.
9. The CLPWMS should be secured by condition.
10. A Car Parking Design and Management Plan and Travel Plan should be prepared and secured by condition.

Officer comment:

In response to the comments the applicant has submitted a Transport Statement Addendum to include an Active Travel Zone (ATZ) Healthy Streets Assessment, a review of TfL Personal Injury Collision Data, a review of the total person trips associated with the AWP, a review of cycle parking required at the site, conformation of the number of staff and the number of electric vehicle parking spaces and reference to a final Construction Logistics Plan and Car Parking Design and Management Plan needing to be secured by condition.

Officers consider that the additional information submitted would overcome the concerns of TfL. However, formal confirmation is awaited and this matter will be reported to the committee.

5.11 Environment Agency:

Environment Agency position

Thank you for consulting us again on this application. Following a review of the additional information provided, we are satisfied the proposed development will meet the National Planning Policy Framework's requirements in relation to flood risk if the following planning conditions are included:

Condition

The development shall be carried out in accordance with the submitted flood risk assessment (ref: R-FRA-22800-01-C, dated January 2022) and the following mitigation measures it details:

- The development shall be constructed at or below existing ground level to ensure there is no loss of flood storage.
 - The 200mm barrier surrounding the All Weather Pitch (AWP) shall be constructed of open steel mesh to ensure permeability and not impede flows.
- These mitigation measures shall be fully implemented prior to occupation and

subsequently in accordance with the scheme's timing/phasing arrangements. The measures detailed above shall be retained and maintained thereafter throughout the lifetime of the development.

Reasons

To ensure that there are no detrimental impacts to flood storage or flood flow routes as supported by Paragraph 159 of the NPPF.

Informative - Environmental permit

The Environmental Permitting (England and Wales) Regulations 2016 require a permit to be obtained for any activities which will take place:

- on or within 8 metres of a main river (16 metres if tidal)
- on or within 8 metres of a flood defence structure or culverted main river (16 metres if tidal)
- on or within 16 metres of a sea defence
- involving quarrying or excavation within 16 metres of any main river, flood defence (including a remote defence) or culvert
- in a floodplain more than 8 metres from the river bank, culvert or flood defence structure (16 metres if it's a tidal main river) and you don't already have planning permission

For further guidance please visit <https://www.gov.uk/guidance/flood-risk-activities-environmental-permits> or contact our National Customer Contact Centre on 03708 506 506.

The applicant should not assume that a permit will automatically be forthcoming once planning permission has been granted, and we advise them to consult with us at the earliest opportunity.

6. POLICY CONSIDERATIONS

6.1 POLICY CONTEXT

National Planning Policy Framework (2021)

2. Achieving sustainable development
4. Decision-making
9. Promoting sustainable transport
11. Making effective use of land
12. Achieving well-designed places
14. Meeting the challenge of climate change, flooding and coastal change

6.2 London Plan (2021)

- D4 Delivering good design
- D5 Inclusive design
- D11 Safety, security and resilience to emergency
- D13 Agent of Change
- D14 Noise
- S4 Play and informal recreation
- S5 Sports and recreation facilities
- G1 Green infrastructure
- G4 Open Space
- G5 Urban greening
- G6 Biodiversity and access to nature

G7 Trees and woodlands
SI 1 Improving air quality
SI 2 Minimising greenhouse gas emissions
SI 3 Energy infrastructure
SI 4 Managing heat risk
SI 5 Water infrastructure
SI 7 Reducing waste and supporting the circular economy
SI 8 Waste capacity and net waste self-sufficiency
SI 10 Aggregates
SI 12 Flood risk management
SI 13 Sustainable drainage
T1 Strategic approach to transport
T3 Transport capacity, connectivity and safeguarding
T4 Assessing and mitigating transport impacts
T5 Cycling
T6 Car parking
T7 Deliveries, servicing and construction

6.3 Merton Local Development Framework Core Strategy – 2011 (Core Strategy)

CS 12 Economic development
CS 13 Open space, leisure and nature conservation
CS 14 Design
CS 15 Climate change
CS 17 Waste management
CS 18 Active transport
CS 19 Public transport
CS 20 Parking servicing and delivery

6.4 Merton Sites and Policies Plan – 2014 (SPP)

DM F1 Support for flood risk management
DM F2 Sustainable urban drainage systems (SuDS) and; Wastewater and Water Infrastructure
DM O2 Nature conservation, Trees, hedges and landscape features
DM D2 Design considerations
DM EP2 Reducing and mitigating noise
DM EP3 Allowable solutions
DM EP4 Pollutants
DM T2 Transport impacts of development
DM T3 Car parking and servicing standards

6.5 Other relevant policy guidance:

Supplementary planning considerations
Draft Merton Local Plan
London Environment Strategy - 2018
Mayor's Air Quality Strategy - 2010
LB Merton – Air quality action plan - 2018-2023.
LB Merton - Draft Sustainable Drainage (SuDS) Design and Evaluation
Supplementary Planning Document (SPD) 2018
Merton's Playing Pitch Strategy 2019
Merton Indoor Sports Facility Study 2020

7. **PLANNING CONSIDERATIONS**

- Principle of development
- Impact on the character of the area
- Residential Amenity
- Transport, highway network, parking and sustainable travel
- Air quality and potentially contaminated land
- Flooding and site drainage

7.1 Principle of development

7.1.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 states that when determining a planning application, regard is to be had to the development plan, and the determination shall be made in accordance with the development plan, unless material considerations indicate otherwise.

7.1.2 Metropolitan Open Land

7.1.3 The site comprises Metropolitan Open Land. Paragraph 99 of the NPPF states:

“Existing open space, sports and recreational buildings and land, including playing fields, should not be built on unless:

- a) an assessment has been undertaken which has clearly shown the open space, buildings or land to be surplus to requirements; or
- b) the loss resulting from the proposed development would be replaced by equivalent or better provision in terms of quantity and quality in a suitable location; or
- c) the development is for alternative sports and recreational provision, the benefits of which clearly outweigh the loss of the current or former use.”

7.1.4 Policy G3 of the London Plan states:

Metropolitan Open Land (MOL) is afforded the same status and level of protection as Green Belt:

- 1) MOL should be protected from inappropriate development in accordance with national planning policy tests that apply to the Green Belt.

7.1.3 In addition, Merton Policy DMO1 states:

" b) In accordance with the NPPF, existing designated open space should not be built on unless:

- i. an assessment has been undertaken which has clearly shown the open space, buildings or land to be surplus to requirements; or,
- ii. the loss resulting from the proposed development would be replaced by equivalent or better provision in terms of quantity and quality in a suitable location; or,
- iii. the development is for alternative sports and recreational provision, the needs for which clearly outweigh the loss.

c) Development proposals within designated open spaces, which have met the conditions set in part b) above, will be required to meet all the following criteria:

- i. the proposals are of a high quality design and do not harm the character, appearance or function of the open space;

- ii. the proposals retain and/or improve public access between existing public areas and open spaces through the creation of new and more direct footpath and cycle path links; and,
- iii. the character and function of leisure walks and green chains are preserved or enhanced."

7.1.4 The London Plan sets out at policy S5 that existing sports and recreational land (including playing fields) and facilities for sports and recreation should be retained unless:

- 1) an assessment has been undertaken which clearly shows the sports and recreational land or facilities to be surplus to requirements (for the existing or alternative sports and recreational provision) at the local and sub-regional level. Where published, a borough's assessment of need for sports and recreation facilities should inform this assessment; or
- 2) the loss resulting from the proposed development would be replaced by equivalent or better provision in terms of quantity and quality in a suitable location; or
- 3) the development is for alternative sports and recreational provision, the benefits of which clearly outweigh the loss of the current or former use.

7.1.5 Therefore, a key aspect of this assessment will be whether the need for the proposed AWP outweighs the loss of the part of the rugby and cricket pitch space.

7.1.6 The applicant has stated that the proposed replacement sports facilities would enable greater participation of a variety of sports by school students. The proposed development is for alternative sports provision that would be of benefit to the school students with access to the site. The AWP would allow for year round use and would provide a substantial benefit to hockey, tennis, netball, cricket and football.

7.1.7 However, the provision of the AWP would reduce the space available for the grassed pitch and would reduce the size of the cricket pitch and reduce the number of pitches available for rugby use.

7.1.8 The Merton Playing Pitch Strategy identifies this site as being of importance for the protection and enhancement of both rugby and cricket

7.1.9 Sport England has raised objection to the loss of cricket and rugby pitch space. Sport England indicates that the impact could be mitigated for by providing a suitable level of community use to the wider community. On this basis, the applicant has extended the hours for community use to include 2 weekday evenings (6pm-10pm) and 8am-6pm on Sundays (a year round offer). Sport England has indicated that community use should be available every weekday evening and 8am to 9pm Saturdays and Sundays, in order to provide adequate mitigation.

7.1.10 Therefore, the objection of Sport England is maintained.

7.1.11 It is noted that Sport England has raised concern that ball strike netting of at least 18m height netting is required to the western and eastern boundaries for recreational cricket activity. However, it is noted that the level of cricket on site would be reduced over the existing and there is no such netting on site currently. Whilst this is a recommendation of the Labosport's Ball Strike Trajectory Assessment that this netting be included and ideally every recommendation should be carried out, officers

conclude that its provision is not required in relation to the proposed development of an AWP, given that the cricket use would be reducing from its current level.

7.1.12 It would appear that historically the cricket pitches and rugby pitches have been available at various times at weekends and weekday evenings. The school has stated that they are under no legal obligation to provide community usage but nonetheless they are keen to facilitate as much community use as reasonable practicable. The hours offered in the CUA would represent the minimum number of hours available and the school indicates that they would provide more when possible. However, Sport England raise concern that this would not provide a consistent and reliable framework for booking in advance.

7.1.13 The AWP would facilitate a range of sports for use by the school, whilst a degree of rugby and cricket usage would be retained. Therefore, the proposed development is considered to be for 'alternative sports and recreational provision'. The key issue is whether the benefit clearly outweighs the loss of the current use as part of the grassed playing field. The level of community usage is key to this assessment.

7.1.14 The applicant sets out that "when considered on a year-round basis, the offer is comparable to the OEA's past usage of the site because the clubs' usage was variable. Partly due to the seasonal nature of cricket and rugby, the clubs would not consistently play sport every week during their allotted times (which were two weekday evenings, Saturday afternoons, and Sundays). The AWP will be made available for a broader range of community users to play a variety of sports, and be suitable for year-round use, so it is hoped the hours offered will be taken up by community use throughout the year.

7.1.15 During the school summer holidays, the same minimum hours (2 weekday evenings plus Sundays) are to be offered but it is expected the school will be able to offer additional community access to the AWP (over and above the minimum hours) during the holidays. This would be an improvement on the OEA's past arrangement, which did not include additional hours of use during the summer beyond the clubs' standard allotted times."

7.1.16 In order to seek to overcome the objection raised by Sport England, the applicant has provided a detailed breakdown of the annual usage by the Old Emmanuel Cricket Club and Old Emmanuel Rugby Club in comparison to the proposed year round offer for community use. The previous use by the cricket club and rugby club totalled approximately 748 hours per annum. The proposed community use of the AWP would be 1050 hours per annum.

7.1.16 Overall, officers conclude that the applicant has made reasonable efforts to provide for community usage, in addition to the commitment to their Outreach Programme. The concerns of Sport England are noted and have been carefully considered. Despite negotiations and changes to the CUA an agreement with Sport England has not been reached. Nonetheless, the proposal is for a replacement facility which would remain in a sporting use, albeit offering different sporting facilities to the existing, which the school has identified a need for.

7.1.17 It is noted that there is currently no community use agreement for any of the sporting facilities on site and if the application is not approved the school would be under no obligation to provide continued community access. It is noted that the Old Emmanuel Cricket Club no longer have an option to use the land, as the club is moving to an

alternative site. Therefore, the application would provide for community use over and above the existing scenario.

7.1.18 On the basis that the available community usage would be increased from the existing scenario, officers conclude that the proposal has demonstrated that the proposed development is for outdoor facility for sport, the provision of which would be of sufficient benefit to the development of sport as to outweigh the detriment caused by the loss, or prejudice to the use, of the area of playing field. Thereby meeting Exception 5 of Sport England's guidance.

7.1.19 The concerns of Sport England has been carefully considered. However, Officers conclude that the applicant has reasonably demonstrated a need for the proposal and has demonstrated that community use would be increased and would be secured (whereas currently there is no requirement for any community use and if the application is not approved there would be no mechanism to secure any community use in the future).

7.1.20 Officers also note that the GLA has commented on the breakdown of existing and proposed community use hours set out by the applicant and concludes that there is an uplift from the previous in terms of hours available for both weekdays and weekends which translates to an overall uplift in hours to be made available. This also translates to an increase to the number of weeks in a year the facilities would be made available due to the nature/different maintenance requirements of the AWP, and a wider variety of sports available to be played by community groups. This would support London Plan Policy S5 and Paragraph 5.1.10 (as specified in the GLA's Stage 1 report) and is considered a positive improvement. Therefore, the GLA have now indicated support in terms of the assessment against Sporting policies of the London Plan.

7.1.21 Officers consider that the proposed replacement facilities would be an improvement on the existing.

7.1.22 Conclusion on principle of development:

7.1.23 Whilst the Council's preference would be for a greater extent of community use made available on the site, it is noted that the replacement sports facility would provide a benefit for various sporting activities carried out at the school. The reduction in space for cricket and rugby is regrettable but it is noted that both clubs are due to move to alternative sites.

7.1.24 The proposed development is considered to be acceptable in terms of the MOL designation as it would provide for replacement sports facilities, the benefits of which outweigh the loss.

7.1.25 Subject to condition to secure the Community Use Agreement, the proposal is considered to comply with Paragraph 99 of the NPPF, Policies G4 and S5 of the London Plan, Policy CS13 and Policy DM 01 of the Sites and Policies Plan 2014. The application would be required to go through a Stage 2 consultation to the GLA, prior to formal determination, given the objection of Sport England is maintained.

7.2 Impact on the character of the area

- 7.2.1 The NPPF, London Plan policies D3 and D4, Core Strategy policy CS 14 and SPP Policy DM D2 require well designed proposals which make a positive contribution to the public realm, are of the highest quality materials and design and which are appropriate in their context. Thus, development proposals must respect the appearance, materials, scale, bulk, proportions and character of their surroundings.
- 7.2.2 The NPPF, London Plan policies D3 and D4, Core Strategy policy CS 14 and SPP Policy DM D2 require well designed proposals which make a positive contribution to the public realm, are of the highest quality materials and design and which are appropriate in their context. Thus, development proposals must respect the appearance, materials, scale, bulk, proportions and character of their surroundings.
- 7.2.3 The proposed AWP, fencing and lighting, would result in a change to the currently open playing field. However, the light spill from the proposed floodlights would be limited and the appearance would be consistent with the sporting use. Officers conclude that any limited visual impact would be outweighed by the benefit of providing a year round sporting facility.
- 7.2.4 In terms of existing trees, the majority of the wider school's boundaries are well treed, the proposed development would result in the loss of two trees due to the position of the AWP. There would be some encroachment into the RPAs of another tree on site by reason of the floodlighting columns, however, it is recommended that this be carried out under supervision to minimise damage to the roots, rather than removal. The proposed replacement tree planting to the boundaries is considered to provide a reasonable mitigation to the minimal tree loss proposed.
- 7.2.5 The proposed development is considered to be acceptable in accordance with London Plan Policies D3 and D4, Core Planning Strategy Policy CS14 and Policy DM D2 of the Sites and Policies Plan 2014.

7.3 Residential Amenity

- 7.3.1 Policy DM D2 seek to ensure that development does not adversely impact on the amenity of nearby residential properties.
- 7.3.2 The proposed AWP would be well separated from neighbouring residential properties, with the closest being situated approximately 200m away to the southwest (at Onslow Road). The separation distance, coupled with the intervening tree screening, would limit the visual impact of the proposed AWP and lighting. In addition, conditions are recommended to ensure that the floodlighting is not used after 10.15pm.
- 7.3.3 The proposed development would not have a significant impact on the amenities of the users of the adjacent industrial estate or Tesco superstore due to the separation distance.
- 7.3.4 The proposed development is considered to be acceptable in terms of the impact on neighbouring amenity and would comply with Policy DM D2 of the Sites and Policies Plan.

7.4 Transport, highway network, parking and sustainable travel

- 7.4.1 Policy T6 (Car Parking) of the London Plan sets out that The maximum car parking standards set out in Policy T6 .1 Residential parking to Policy T6.5 Non-residential disabled persons parking should be applied to development proposals, Appropriate disabled persons parking for Blue Badge holders should be provided as set out in Policy T6.1 Residential parking to Policy T6 .5 Non-residential disabled persons parking and where car parking is provided in new developments, provision should be made for infrastructure for electric or other Ultra-Low Emission vehicles
- 7.4.2 At a local level Policy CS20 requires developers to demonstrate that their development will not adversely affect on-street parking or traffic management. Policies DMT1-T3 seek to ensure that developments do not result in congestion, have a minimal impact on existing transport infrastructure and provide suitable levels of parking.
- 7.4.3 The existing car park serving the site would be of a sufficient size to accommodate the parking of vehicles, as the proposed development would not result in a significant increase in vehicle movements.
- 7.4.4 The school intends to manage the car park with the following measures:
- Promote shared travel arrangements and car-sharing (maximising car occupancy);
 - Promote a drop-off / collection system;
 - Promote active travel including walking, cycling and the use of public transport modes;
 - Provide and display local bus route information;
 - Marshal parking areas during the peak of use to ensure the parking area functions in an efficient manner,
 - Review effectiveness of the parking control measures and overarching promotion of sustainable transport, and;
 - Organise staggered start times / changeover times.
- 7.4.5 In addition, on the basis of comments from the Council's Transport Planner, officers advise that a condition be imposed requiring cycle parking to be provided on site also. Subject to this condition, the proposed development is considered to be acceptable in terms of transport considerations.
- 7.4.6 It is noted that the GLA and TfL have set out a number of additional requirements in relation to:
- Healthy Streets
 - Reduction in car parking
 - Increase in cycle parking
- 7.4.7 The GLA and TfL has requested an assessment against the Healthy Streets criteria, on the basis that the application is referable (this is due to Sport England's objection rather than the scale of the development). Given that the majority of use of the AWP would be by the school, with a similar level of community use to that which exists currently officers do not consider that additional information relating to the Healthy Streets indicators is directly relevant to the proposal. Nonetheless, the applicant has sought to provide additional information in this regard by way of the submission of additional information which takes into account the Healthy Streets indicators, Vision Zero (eliminating deaths and serious injuries), including Personal Injury Collision

Analysis. In addition, the applicant has provided further details in relation to the modal share assessment. The applicant has also confirmed that 59 cycle parking spaces would be provided as part of the proposals (to the southeast side of the proposed AWP). Whilst there are no London Plan requirements for electric vehicle parking for an AWP. There is a general requirement for new development to provide 20% electric vehicle parking (EVP) on new developments. The applicant has confirmed that 20% of the available spaces would be EVPs (10 of the 50 existing bays would be EVP). Formal comments from TfL in response to this additional detail is awaited and will be reported to the committee. However, officers consider that the additional information provide assurance that the proposal would be acceptable in terms of highway impacts.

7.4.8 In terms of car parking, the existing car park is adequate to accommodate users of the site. The existing use and proposed use would be similar and therefore, notwithstanding the overall aim of fostering sustainable modes of transport, a reduction in size of the car park is not considered to be directly justified by this proposal.

7.4.9 The request to provide 80 cycle parking spaces is also noted. However, the site is accessed off the A3, a major three lane road, which could present a danger to cyclists, which may include young people visiting the site. The request for 80 cycle parking spaces is based solely on the overall site area (8000sqm) and therefore does not relate directly to the proposal for the AWP. Therefore, whilst additional cycle parking on the site may have some benefits in terms of sustainable transport, it is not justified by this application for an AWP. Notwithstanding that, as set out above the applicant has agreed to provide 59 cycle parking spaces to overcome the concern raised.

7.4.10 The proposal would not significantly increase traffic movements and the existing facilities are adequate to accommodate users of the development without a significant impact on the local highway network. The proposal is considered to comply with Policies T4, T5, T6 and T7 of the London Plan 2021, Policies CS20 of the Core Planning Strategy 2011 and Policies DM T1, DM T2 and DM T3 of the Sites and Policies Plan 2014.

7.5 Air quality and potentially contaminated land

7.5.1 The whole of Merton is an Air Quality Management Area (AQMA).

7.5.2 The London Plan requires both major and minor development to be air quality neutral and in light of Merton's recently published Air Quality Action Plan, which seeks to minimise emissions from gas boilers and minimise the levels of localised PMs (Particulate Matter) and NO₂ throughout the construction phase, it is important that the impact on air quality is minimised.

7.5.3 The application is accompanied by an Air Quality Assessment, which demonstrates that the proposed development would be air quality neutral. Subject to conditions to ensure that the dust mitigation measures set out in the assessment are carried out, officers conclude that the impact on air quality from both the construction and operational phase would be acceptable.

7.5.4 In terms of ground contamination, the submitted Site Investigation Report (JPP, July 2021) finds no evidence of fuel contamination, vapour risk, asbestos risk, or ground

gas risk. The ground, made-ground and topsoil on site are classified as non-hazardous. The site was historically an orchard and meadows associated with Blagdons Farm, and since the 1920s it has been used as playing fields, meaning there are no known historic contamination risks.

7.5.5 Subject to a condition to secure a watching brief the Council's contaminated land officer has raised no objection.

7.6 Flooding and site drainage

7.6.1 Policy SI 13 of the London Plan (Sustainable drainage) sets out that development proposals should aim to achieve greenfield run-off rates and ensure that surface water run-off is managed as close to its source as possible. There should also be a preference for green over grey features.

7.6.2 London Plan policies SI 2 to SI 5 and CS policy CS15 seek to ensure the highest standards of sustainability are achieved for developments which includes minimising carbon dioxide emissions, maximising recycling, sourcing materials with a low carbon footprint, ensuring urban greening and minimising the usage of resources such as water.

7.6.3 The Environment Agency's online flood mapping indicates that the Site lies within Flood Zones 2 and 3. The proposals for an All Weather Pitch are classed as Water Compatible development, and are therefore considered acceptable in this location.

7.6.4 The proposed drainage strategy will provide attenuation within the permeable sub-base of the pitch. Surface water will outfall to Pyl Brook located to the north of the site, restricted to 2.5 l/s via a Hydrobrake.

7.6.5 The proposed AWP will include a 0.26m sub-base, which is sufficient to accommodate the 1 in 30 year and 1 in 100 year events. During the 1 in 100 year plus 40% climate change event, a 24.3m³ volume of flooding is triggered, in excess of the attenuation provided within the sub-base. The pitch will be surrounded by 200mm barriers, which will contain the remaining volume during the 1 in 100 year plus 40% climate change event and also manage and exceedance flows.

7.6.6 Subject to conditions to ensure that the mitigation measures in the submitted FRA are carried out and the provision of a SUDS management plan for the new SUDS proposal, the proposal is considered to be acceptable in terms of flooding and site drainage.

7.7 Biodiversity considerations

7.7.1 The Northern boundary of Blagdons Sports Ground is within, or immediately adjacent to; a Site of Importance for Nature Conservation (SINC) (the site itself and development proposal is located a considerable distance from the SINC), the site is also part of a Green Corridor.

7.7.2 The AWP itself would be on an area of grassed land and would have a limited impact on biodiversity.

- 7.7.3 The cricket net facility that was part of the original proposal has been omitted from the application and is now being assessed under a separate concurrent application, as further survey works were required prior to a determination could be made.
- 7.7.4 The proposed tree planting would provide some overall benefit to biodiversity and the proposed development would result in a biodiversity gain, albeit marginal.

8. **Conclusion**

- 8.1 The existing playing field has historically provided for community cricket and rugby use, in addition to fulfilling the school's sporting requirements. The school has an identified need for an AWP to provide for a wider range of sporting activities. The proposal is for a replacement sport facility, the benefit of which outweighs the loss.
- 8.2 The concerns of Sport England have been carefully considered, however, the community use put forward by the school is considered to provide reasonable mitigation for the partial loss of cricket and rugby pitch space. Officers note that the GLA has now indicated some support in terms of the community use proposed by the applicant.
- 8.3 Officers consider that the proposal would have a very limited impact on the highway network but both the GLA and TfL has requested additional information on the basis that the application is referable. Whilst some of the requirements would usually only relate to a much more significant scheme, Officers consider that the additional information submitted provides sufficient assurance that the impact on the highway network would be acceptable.
- 8.4 The proposal is therefore considered to be acceptable in planning terms.
- 8.5 The application would be required to go through a Stage 2 consultation to the GLA, prior to determination, if Members resolve to grant the application, as the objection of Sport England is maintained.

9.0 **RECOMMENDATION:** Grant Permission Subject to the following Conditions:

1. Time limit
2. Approved Plans and documents
3. Non standard condition. External Materials and surfacing materials as detailed
4. The development hereby approved shall be carried out only in accordance with the measures set out in the submitted Construction Logistics Plan and Working Method Statement and Transport Statement Addendum. No development shall be carried out except in full accordance with the approved details.

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies T4 and T7 of the London Plan 2021, policy CS20 of Merton's Core Planning Strategy 2011 and policy DM T2 of Merton's Sites and Policies Plan 2014.

5. Prior to the first use of the development hereby permitted, a Travel Plan shall be submitted to and approved in writing by the Local Planning Authority. The Travel Plan

shall be implemented upon first occupation of the development hereby permitted and shall be so maintained for the duration of the use, unless the prior written approval of the Local Planning Authority is first obtained to any variation.

Reason: To ensure the safety of pedestrians and vehicles and the amenities of the surrounding area and to comply with the following Development Plan policies for Merton: policies T4 and T7 of the London Plan 2021, policy CS20 of Merton's Core Planning Strategy 2011 and policy DM T2 of Merton's Sites and Policies Plan 2014.

6. The AWP shall be constructed to meet FIH specifications.
7. Hours of use and hours of floodlighting.
8. Tree planting in line with proposed site plan D19-041/ DWG/ 0004 rev. 08 within the first planting season, and planting to be maintained thereafter for a period of 5 years
9. Air quality mitigation measures detailed in submitted Air Quality Assessment to be implemented.
10. Cycle Parking (secure & undercover) to be provided in line with the Transport Statement Addendum (59 cycle parking spaces).
11. The development shall be carried out in accordance with the mitigation measures set out in the submitted Flood Risk Assessment dated November 2021 and prepared by JPP Consulting Ltd, as follows:
 - o Surface water from the development will be discharged to the Pyl Brook at a rate of 2.5 l/s
 - o The proposed all weather pitch will include a permeable sub-base, and will be surrounded by 200mm barriers to contain all surface water discharge up to the 100yr plus 40% climate change event is contained on site.

Reason: To ensure satisfactory means of surface water drainage, to reduce the risk of flooding and to comply with the following Development Plan policies for Merton: policy SI 12 of the London Plan 2021, policy CS16 of Merton's Core Planning Strategy 2011 and policy DM F2 of Merton's Sites and Policies Plan 2014.

12. The applicant shall prepare a SUDS management plan for the new SUDS proposal and ensure it forms part of the site maintenance plan.

Reason: To ensure satisfactory means of surface water drainage, to reduce the risk of flooding and to comply with the following Development Plan policies for Merton: policy SI 12 of the London Plan 2021, policy CS16 of Merton's Core Planning Strategy 2011 and policy DM F2 of Merton's Sites and Policies Plan 2014.

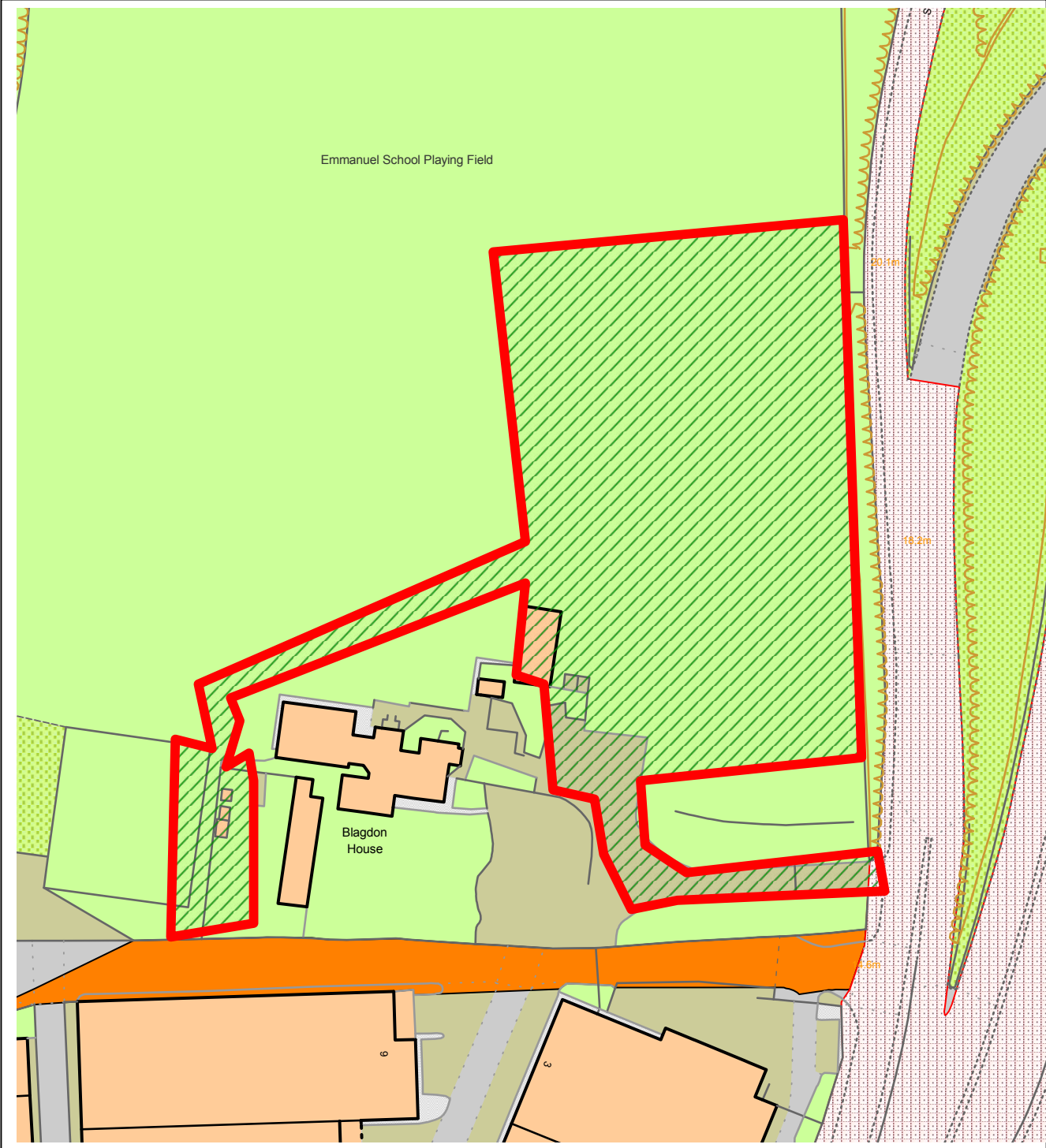
13. The All-Weather Pitch hereby approved and the cricket nets approved under associated application 22/P0956, shall be made available for community use in accordance with a Community Use Agreement, which shall be submitted to and approved in writing prior to the first use of the All-Weather Pitch hereby approved.

Reason: In order to comply with Policies S5 and G4 of the London Plan 2021, Policy CS13 of the Core Planning Strategy 2011 and Policy DM O2 of the Sites and Policies Plan 2014.

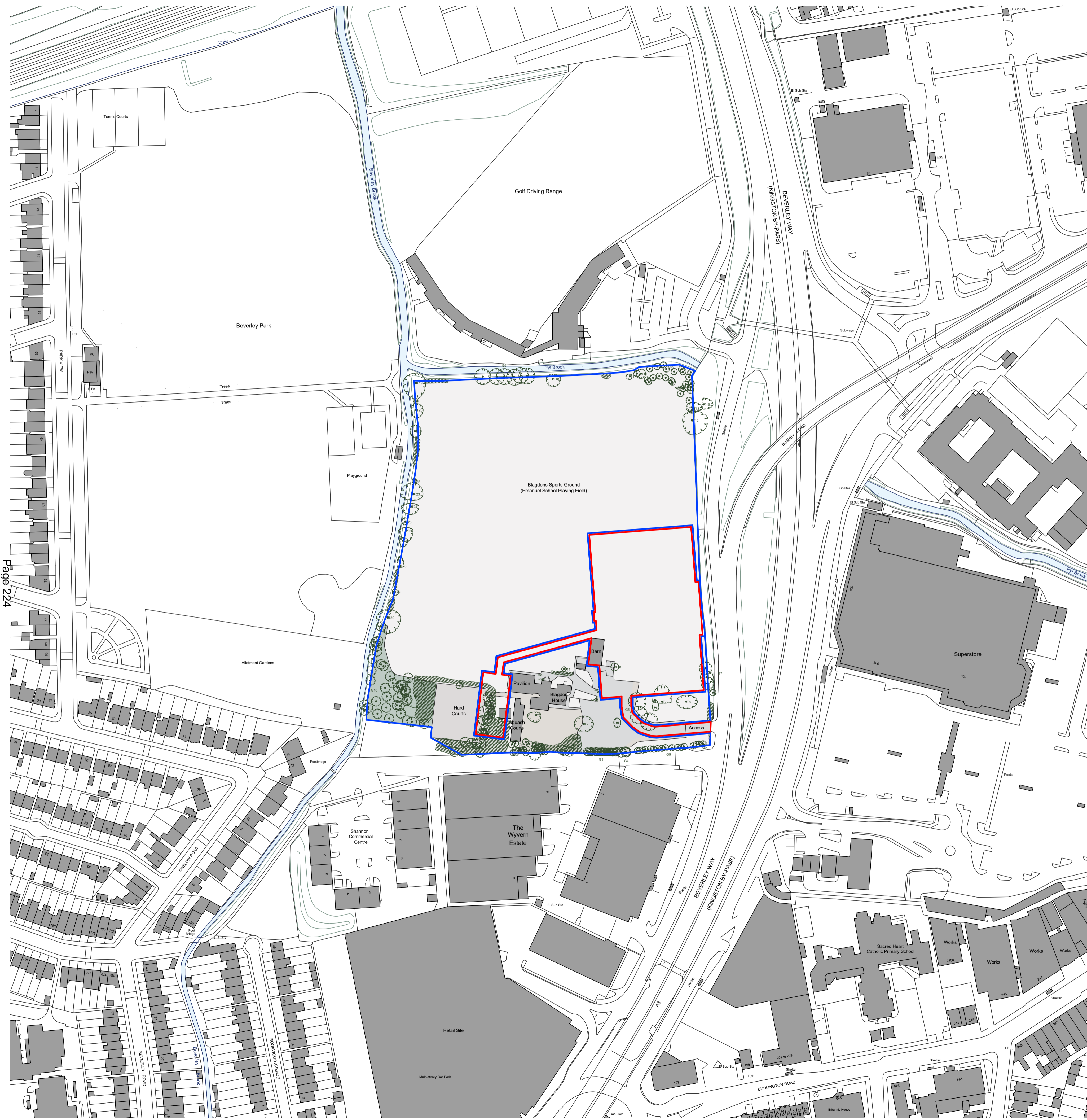
Informatives:

1. INF 15 Discharge conditions prior to commencement of work (or similar wording)
2. Note To Applicant - Scheme Amended During Application Lifecycle

NORTHGATE SE GIS Print Template


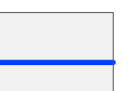


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Page 224

LEGEND

-  APPLICATION SITE AREA (8000M²) REQUIRED FOR PROJECT CONSTRUCTION PHASE WITH ACCESS ADJOINING BEVERLEY WAY
-  LAND AREA UNDER OWNERSHIP OF EMANUEL SCHOOL

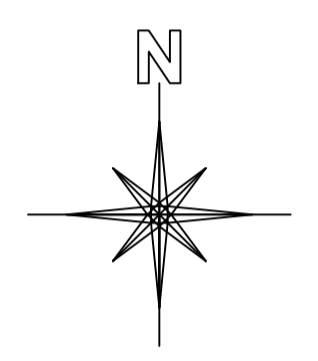
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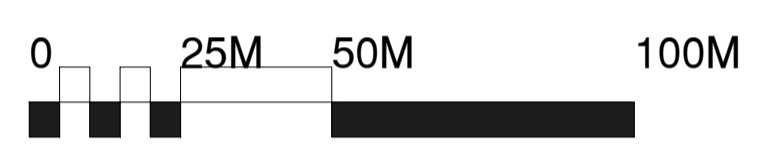
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1:1250 SCALE



REVISION	DATE	DRAWN	DESCRIPTION
.07	20 10 2021	TB	FINAL PLANNING PROPOSAL



Emanuel School

CLIENT
EMANUEL SCHOOL

PROJECT
ALL-WEATHER SPORTS PITCH (AWP)

LOCATION
BLAGDONS SPORTS GROUND
BEVERLEY WAY
NEW MALDEN
KT3 4PU

DRAWING TITLE
LOCATION PLAN

STATUS
SPATIAL COORDINATION (PLANNING)

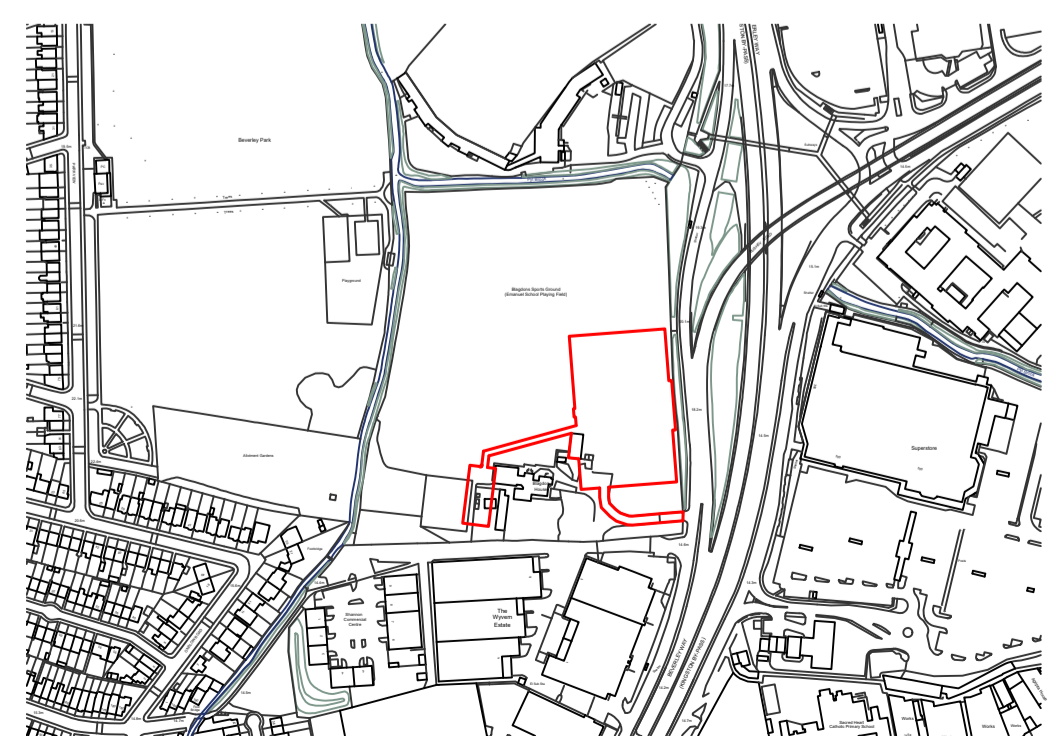
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D19-041

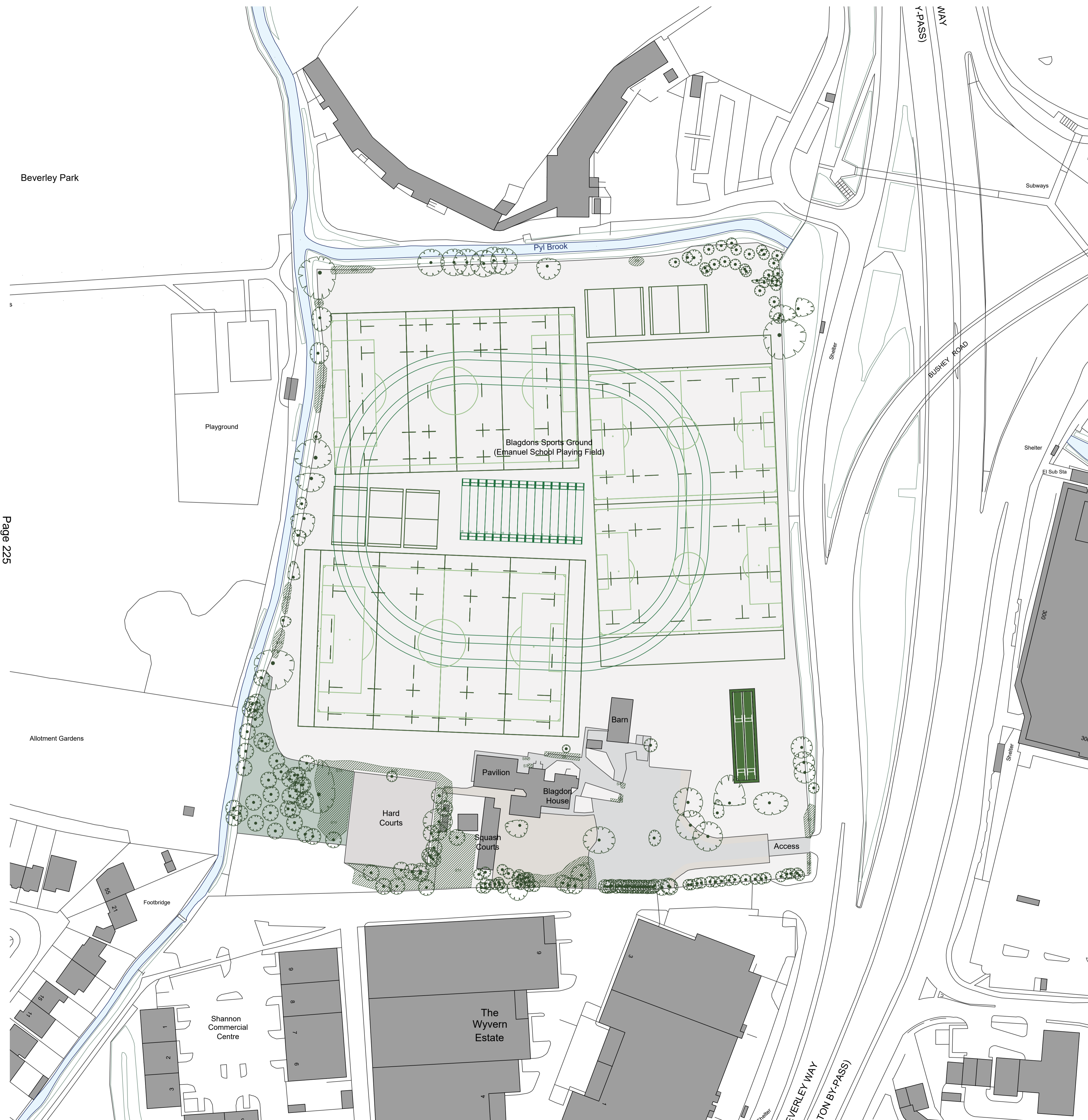
DRAWING NO.
D19-041 / DWG / 0001

REVISION NO.
.07

SCALE	SHEET	DRAWN	CHECKED
1:1250	A1	TB	

PLAN KEY





LEGEND

-  EXISTING TREE
-  EXISTING SCRUB AREA
-  EXISTING AUTUMN GRASS PLAYING PITCH
-  EXISTING SPRING GRASS PLAYING PITCH
-  EXISTING SUMMER GRASS PLAYING PITCH
-  EXISTING NON-TURF CRICKET PRACTICE FACILITY

PLAYING FIELD ARRANGEMENT

AUTUMN TERM

1NO. 15 V 15 RUGBY UNION PITCH
100 X 69M FIELD OF PLAY
10.6M DEEP IN GOALS

1NO. 15 V 15 RUGBY UNION PITCH
93.2 X 66.2M FIELD OF PLAY
6M DEEP IN GOALS

1NO. 15 V 15 RUGBY UNION PITCH
79.3 X 60.2M FIELD OF PLAY
6M DEEP IN GOALS

5NO. MINI RUGBY UNION PITCHES
21.9 X 18M FIELD OF PLAY (2NO.)
21.9 X 12.5M FIELD OF PLAY (3NO.)

SPRING TERM

2NO. 11V11 YOUTH FOOTBALL PITCHES
91 X 55M FIELD OF PLAY

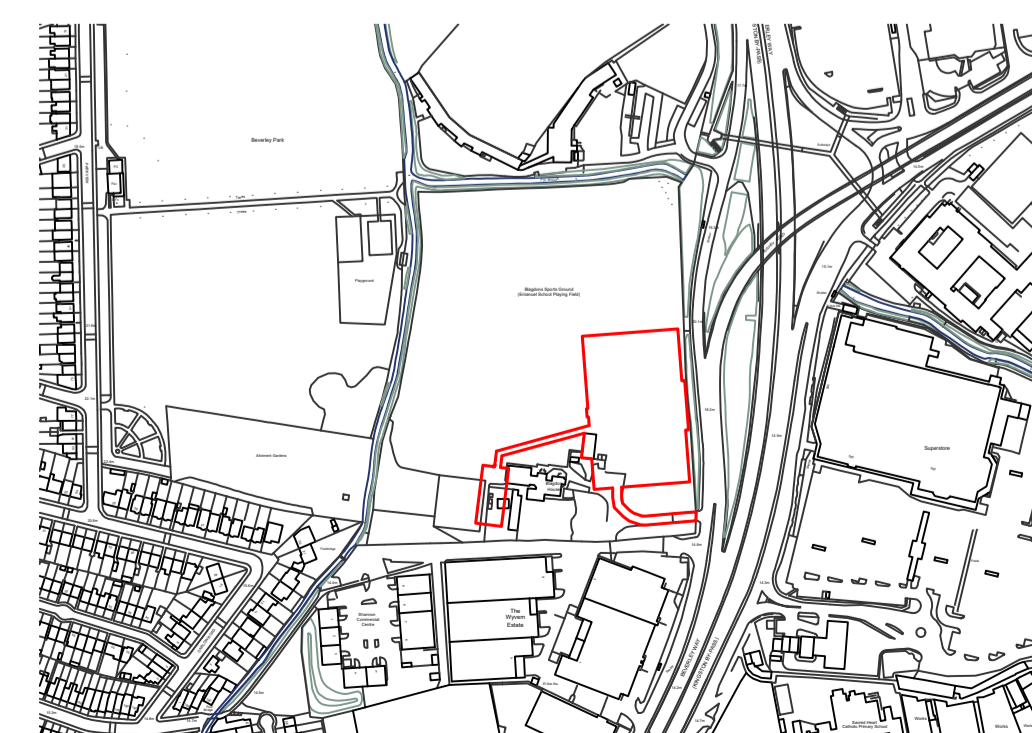
2NO. 9V9 YOUTH FOOTBALL PITCHES
68.6 X 47M FIELD OF PLAY

SUMMER TERM

CRICKET SQUARE
15NO. 20.12 X 3.05 CRICKET PITCHES
46M SENIOR OUTFIELD BOUNDARY
37M JUNIOR OUTFIELD BOUNDARY
3M SAFETY ZONES AROUND BOUNDARIES

2NO. NON-TURF CRICKET PRACTICE PITCHES
30 X 3.66M

PLAN KEY



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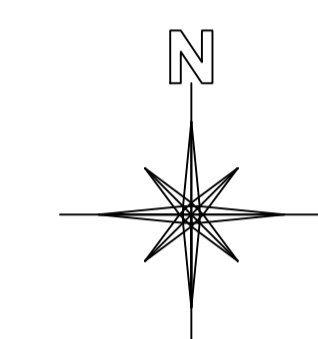
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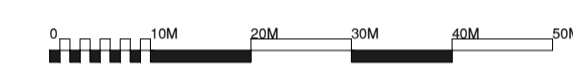
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REVISION	DATE	DRAWN	DESCRIPTION
.07	20 10 2021	TB	FINAL PLANNING PROPOSAL



Emanuel School

CLIENT
EMANUEL SCHOOL

PROJECT
ALL-WEATHER SPORTS PITCH (AWP)

LOCATION
**BLAGDONS SPORTS GROUND
BEVERLEY WAY
NEW MALDEN
KT3 4PU**

DRAWING TITLE
EXISTING SITE PLAN

STATUS
SPATIAL COORDINATION (PLANNING)

PROJECT NO.
D19-041

DRAWING NO.
D19-041 / DWG / 0003




REVISION NO.
.07

SCALE: 1:750 SIZE: A1 DRAWN: TB CHECKED:



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LEGEND

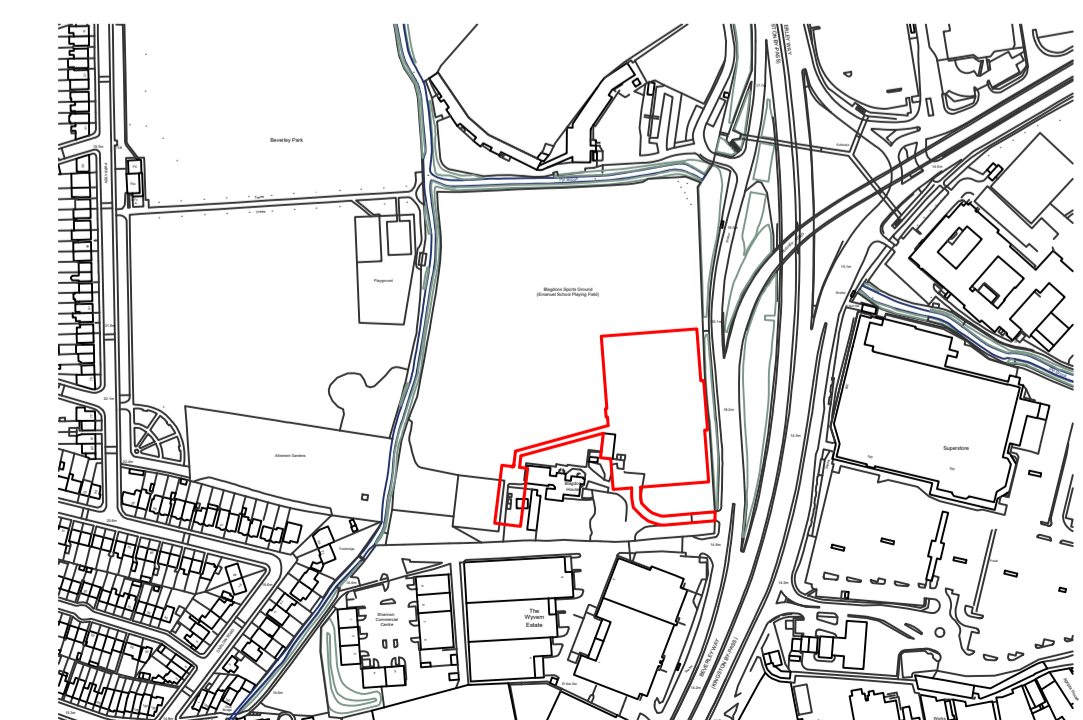
-  FIELD OF PLAY
-  TEAM SHELTERS
-  BALL STOP FENCING
-  FLOODLIGHTS
-  DIVIDING NET SYSTEM
-  GATED ENTRANCE

**ALL-WEATHER SPORTS PITCH (AWP)
FLEXIBLE SPORTS ACTIVITIES**

ALL-WEATHER SPORTS PITCH PROVIDING FLEXIBLE SPORTS ACTIVITIES WITH:

- 1NO. 11V11 HOCKEY FIELD (91.4 X 55M)
- 2NO. 7V7 MINI HOCKEY PITCHES (55 X 43M)
- 4NO. TENNIS COURTS (23.77 X 10.97M EA.)
- 3NO. NETBALL COURTS (30.15 X 15.25M EA.)
- 2NO. CRICKET PITCHES (SOFTBALL / WIND-BALL)

PLAN KEY



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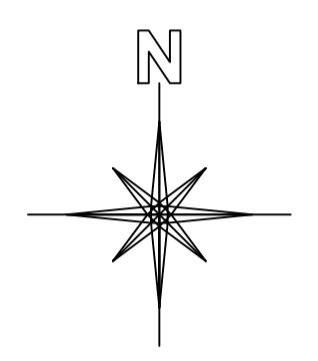
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Emanuel School

CLIENT
EMANUEL SCHOOL

PROJECT
ALL-WEATHER SPORTS PITCH (AWP)

LOCATION
**BLAGDONS SPORTS GROUND
BEVERLEY WAY
NEW MALDEN
KT3 4PU**

DRAWING TITLE
**PROPOSED AWP
FLOOR PLAN**

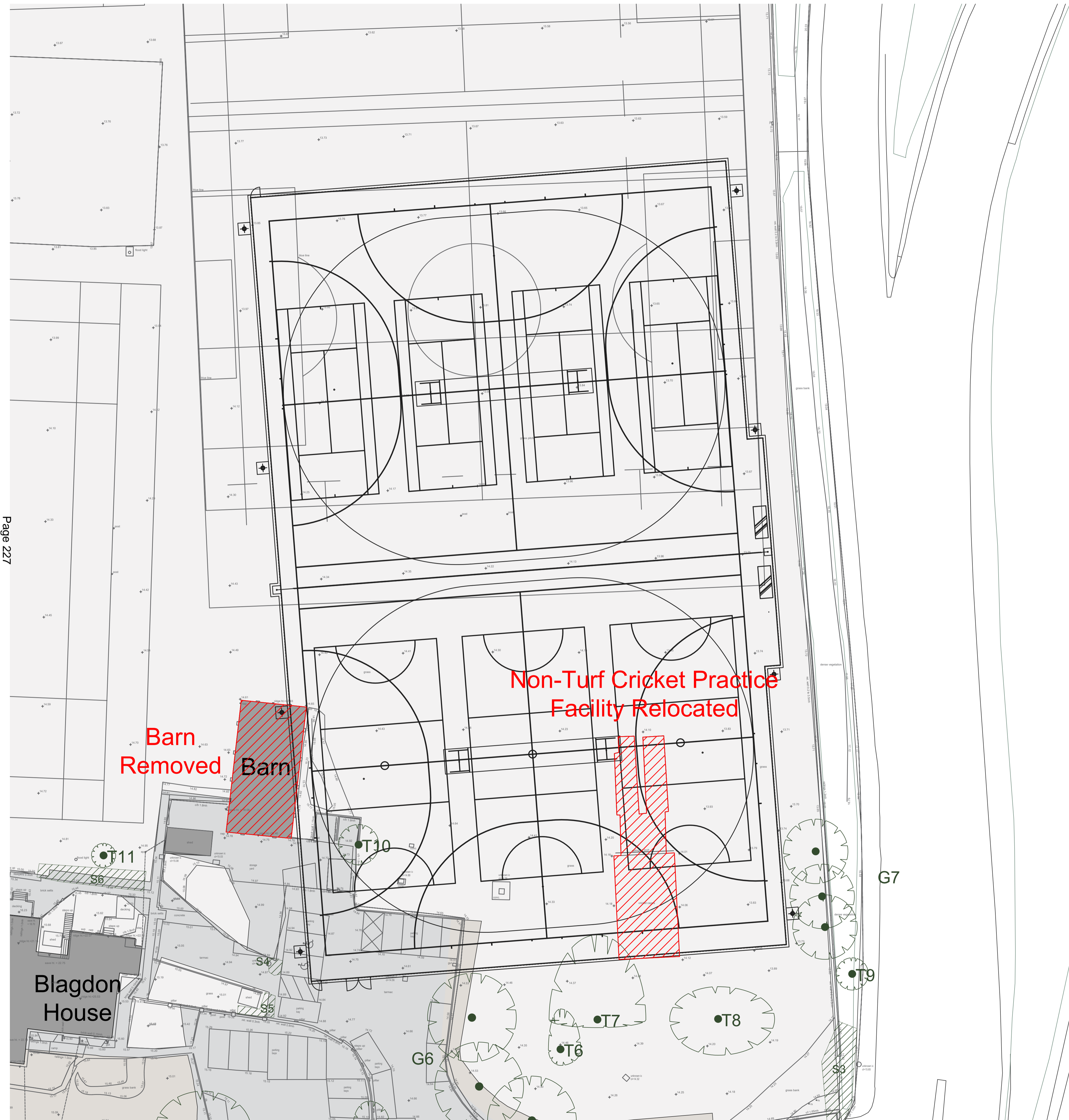
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**SPATIAL COORDINATION
(PLANNING)**

PROJECT NO.
D19-041


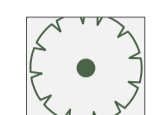
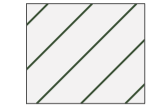
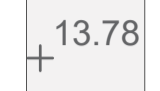
DRAWING NO.
D19-041 / DWG / 0005

SCALE: 1:250
SITE: A1
DRAWN: TB
CHECKED: TB

REVISION NO.
.07



LEGEND

-  ALL-WEATHER SPORTS PITCH (AWP)
-  EXISTING TREE
-  EXISTING SCRUB AREA
-  TOPOGRAPHICAL / LAND SURVEY

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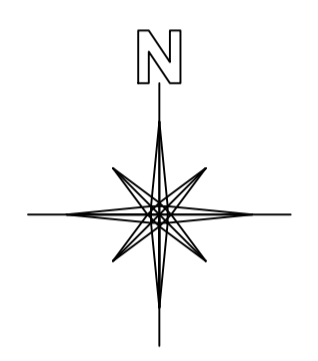
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INFORMATION RELATING TO USE, MAINTENANCE, AND DEMOLITION: TO BE DETAILED WITHIN A BUILDING MANUAL AND HEALTH AND SAFETY FILE.



1:250 SCALE



REVISION	DATE	DRAWN	DESCRIPTION
.07	20 10 2021	TB	FINAL PLANNING PROPOSAL



Emanuel School

CLIENT
EMANUEL SCHOOL

PROJECT
ALL-WEATHER SPORTS PITCH (AWP)

LOCATION
BLAGDONS SPORTS GROUND
BEVERLEY WAY
NEW MALDEN
KT3 4PU

DRAWING TITLE
PROPOSED AWP LAYOUT

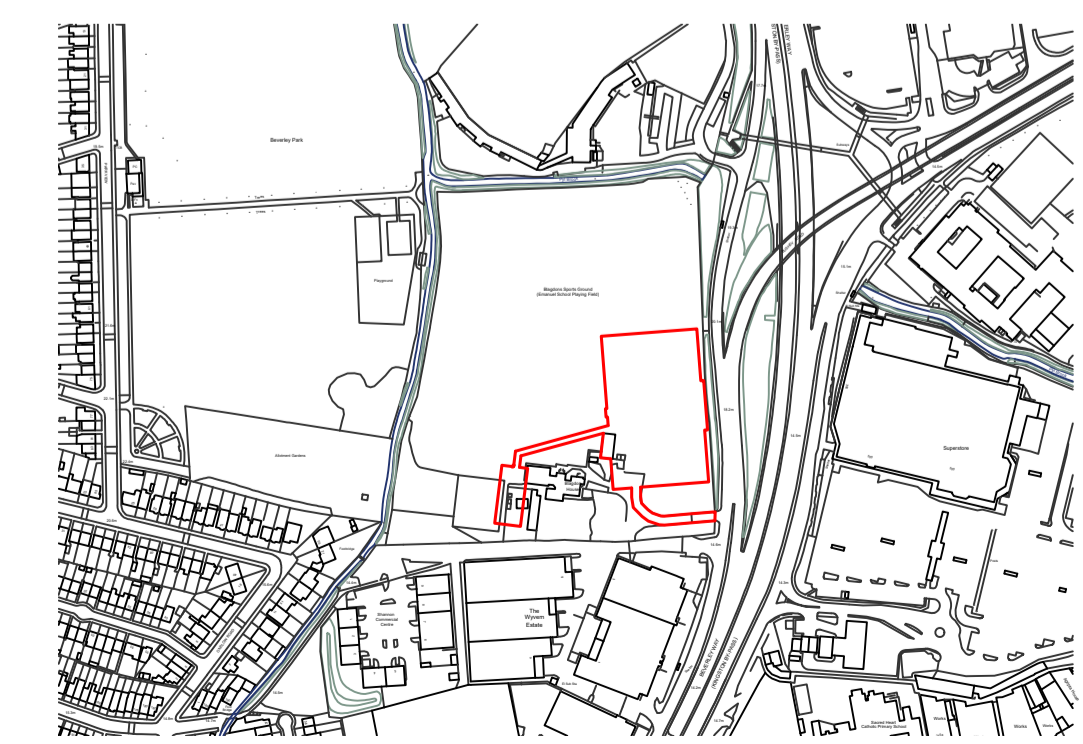
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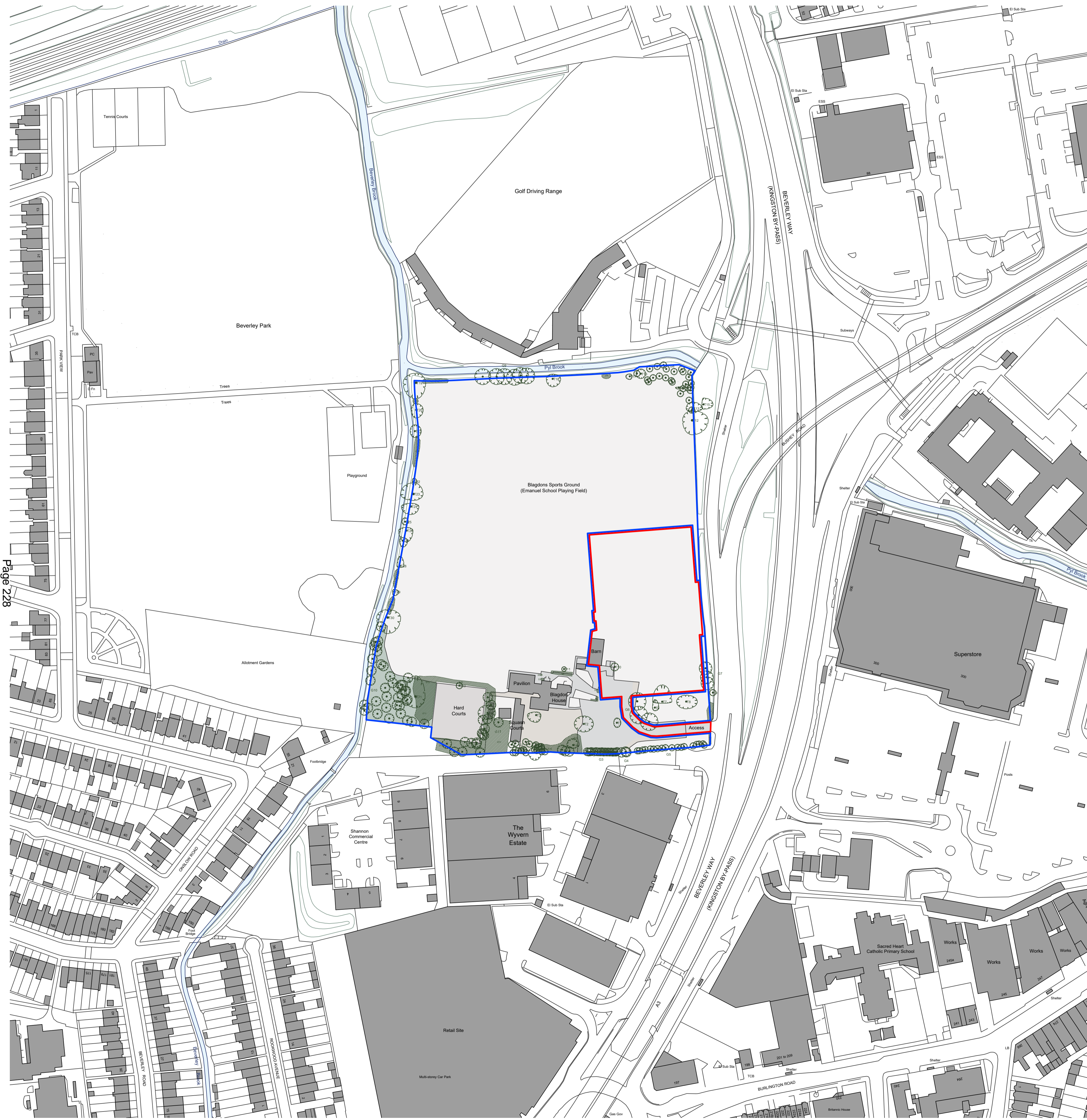
PROJECT NO.
D19-041

DRAWING NO.
D19-041 / DWG / 0006

REVISION NO.
.07

PLAN KEY





LEGEND

- APPLICATION SITE AREA (7075M2) REQUIRED FOR PROJECT CONSTRUCTION PHASE WITH ACCESS ADJOINING BEVERLEY WAY
- LAND AREA UNDER OWNERSHIP OF EMANUEL SCHOOL

NOTES

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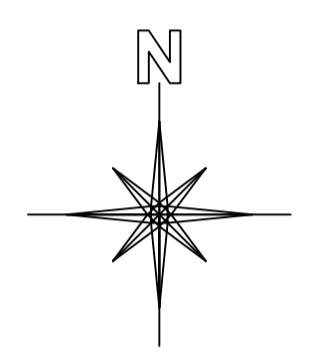
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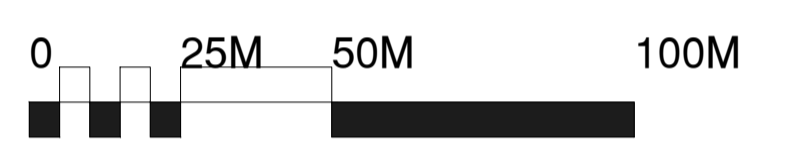
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Emanuel School

CLIENT
EMANUEL SCHOOL

PROJECT
ALL-WEATHER SPORTS PITCH (AWP)

LOCATION
**BLAGDONS SPORTS GROUND
 BEVERLEY WAY
 NEW MALDEN
 KT3 4PU**

DRAWING TITLE
LOCATION PLAN

STATUS
SPATIAL COORDINATION (PLANNING)

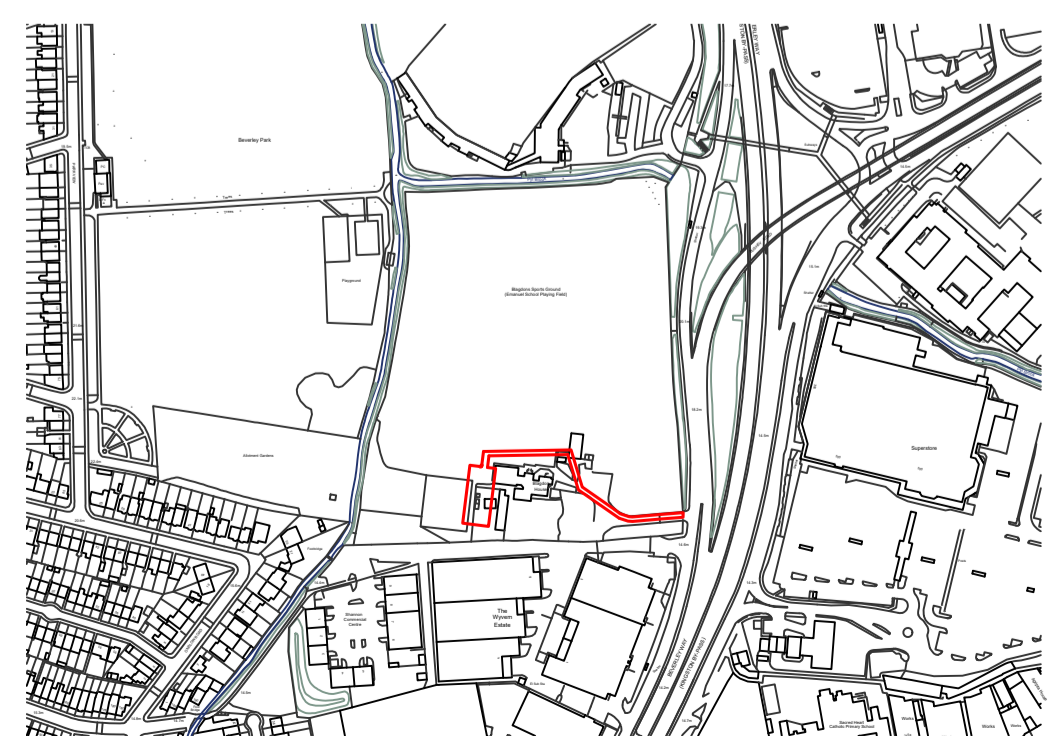
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D19-041

DRAWING NO.
D19-041 / DWG / 0001

REVISION NO.
.08

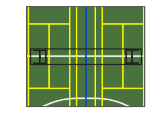
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1:1250	A1	TB	

PLAN KEY





LEGEND



ALL-WEATHER SPORTS PITCH (AWP)



SOFT PLANTING / LANDSCAPING

Page 2/29

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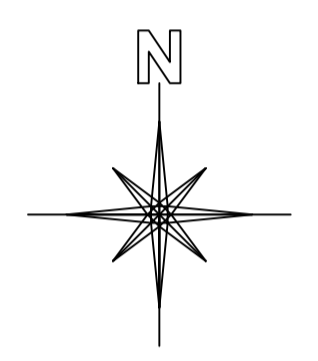
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Emanuel School

CLIENT
EMANUEL SCHOOL

PROJECT
ALL-WEATHER SPORTS PITCH (AWP)

LOCATION
**BLAGDONS SPORTS GROUND
 BEVERLEY WAY
 NEW MALDEN
 KT3 4PU**

DRAWING TITLE
SITE PLAN

STATUS
**SPATIAL COORDINATION
 (PLANNING)**

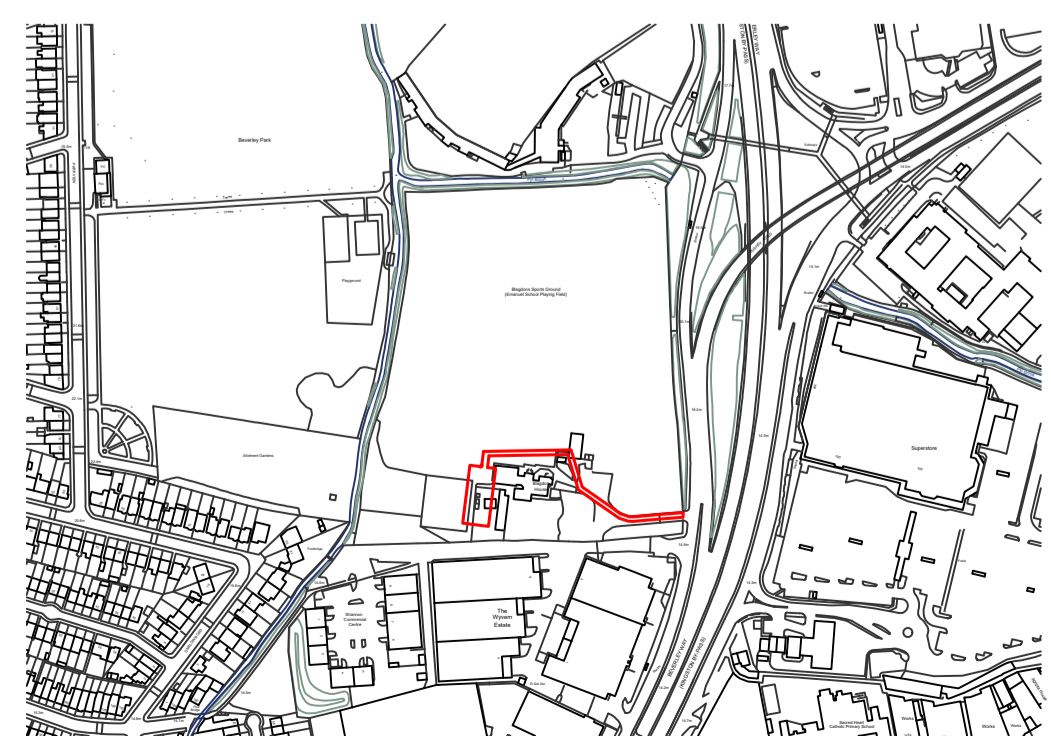
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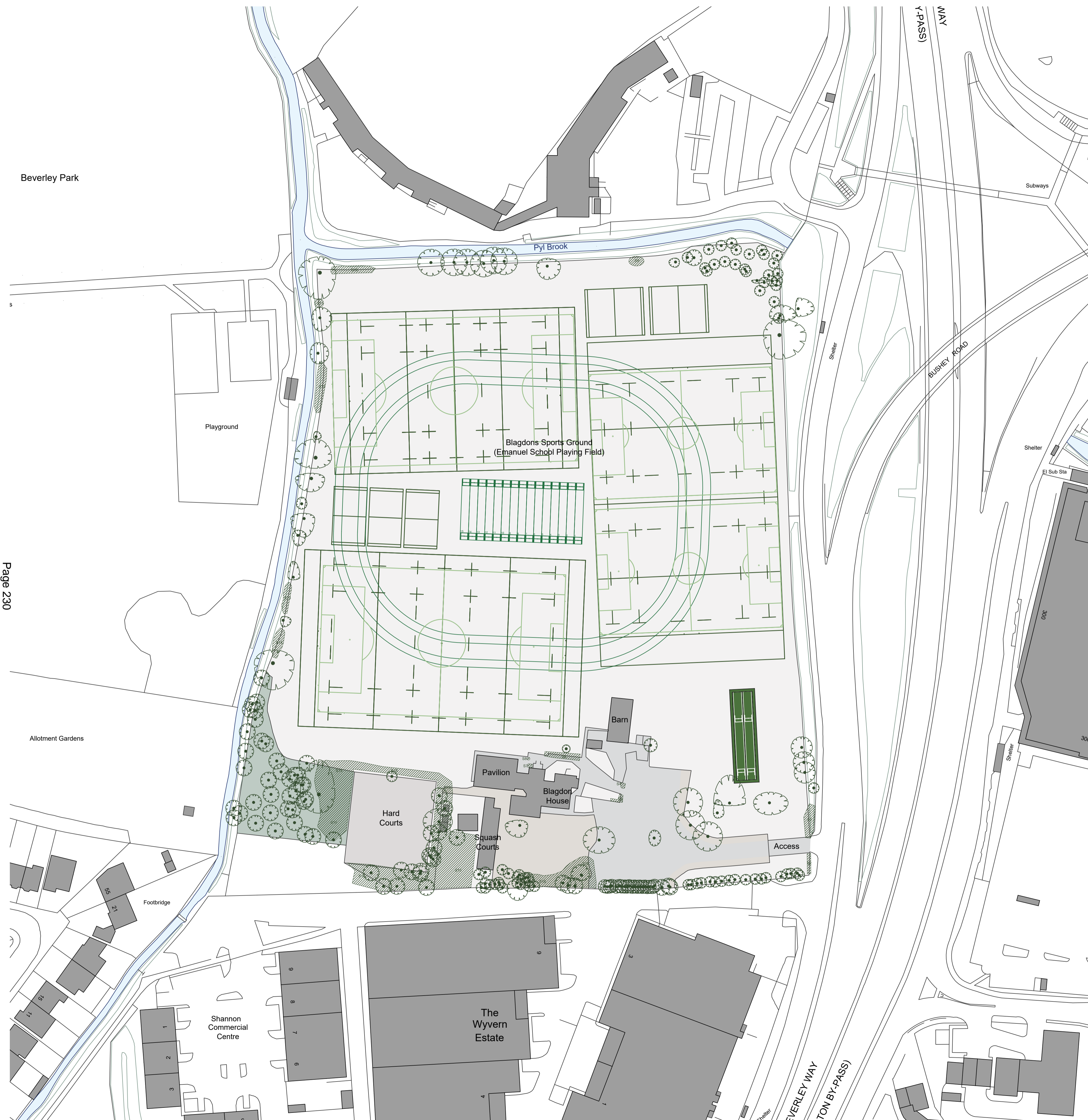
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D19-041 / DWG / 0002

REVISION NO.
.08






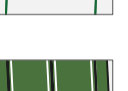
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1:1250	A1	TB	

PLAN KEY





LEGEND

-  EXISTING TREE
-  EXISTING SCRUB AREA
-  EXISTING AUTUMN GRASS PLAYING PITCH
-  EXISTING SPRING GRASS PLAYING PITCH
-  EXISTING SUMMER GRASS PLAYING PITCH
-  EXISTING NON-TURF CRICKET PRACTICE FACILITY

PLAYING FIELD ARRANGEMENT

AUTUMN TERM

1NO. 15 V 15 RUGBY UNION PITCH
100 X 69M FIELD OF PLAY
10.6M DEEP IN GOALS

1NO. 15 V 15 RUGBY UNION PITCH
93.2 X 66.2M FIELD OF PLAY
6M DEEP IN GOALS

1NO. 15 V 15 RUGBY UNION PITCH
79.3 X 60.2M FIELD OF PLAY
6M DEEP IN GOALS

5NO. MINI RUGBY UNION PITCHES
21.9 X 18M FIELD OF PLAY (2NO.)
21.9 X 12.5M FIELD OF PLAY (3NO.)

SPRING TERM

2NO. 11V11 YOUTH FOOTBALL PITCHES
91 X 55M FIELD OF PLAY

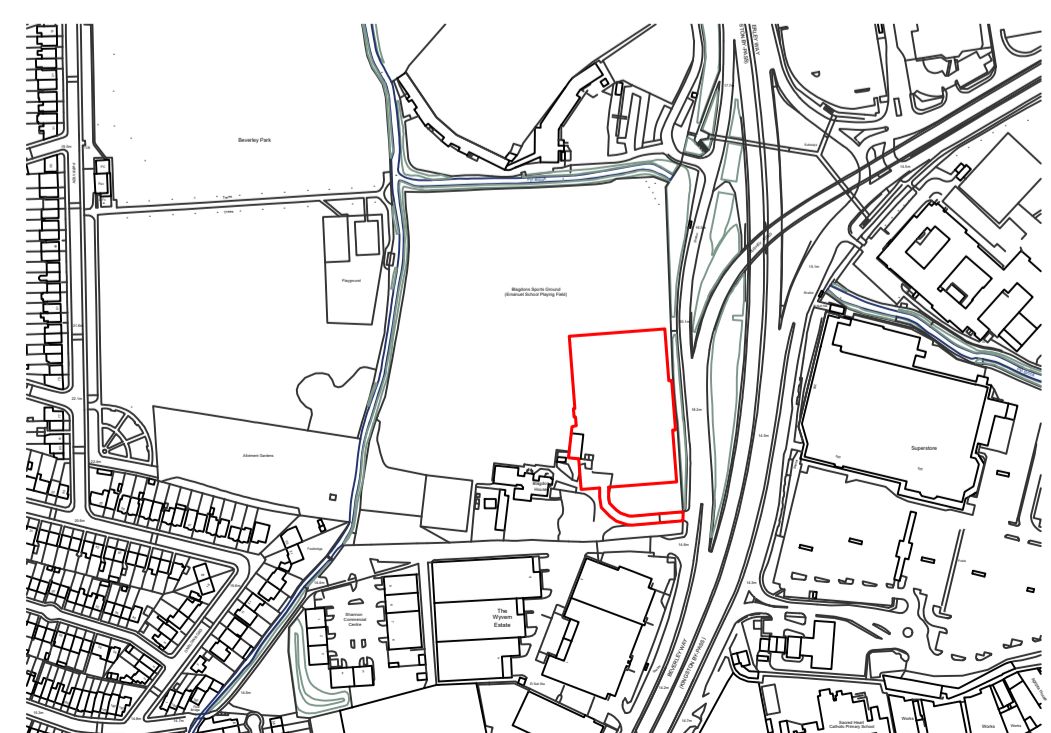
2NO. 9V9 YOUTH FOOTBALL PITCHES
68.6 X 47M FIELD OF PLAY

SUMMER TERM

CRICKET SQUARE
15NO. 20.12 X 3.05 CRICKET PITCHES
46M SENIOR OUTFIELD BOUNDARY
37M JUNIOR OUTFIELD BOUNDARY
3M SAFETY ZONES AROUND BOUNDARIES

2NO. NON-TURF CRICKET PRACTICE PITCHES
30 X 3.66M

PLAN KEY



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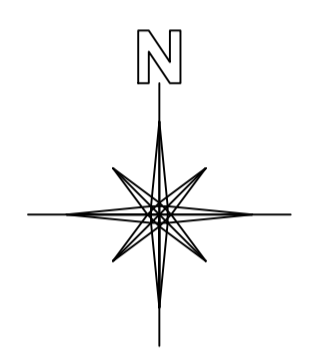
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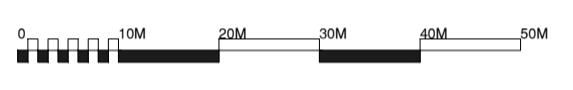
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Emanuel School

CLIENT

EMANUEL SCHOOL

PROJECT

ALL-WEATHER SPORTS PITCH (AWP)

LOCATION

BLAGDONS SPORTS GROUND
BEVERLEY WAY
NEW MALDEN
KT3 4PU

DRAWING TITLE

EXISTING SITE PLAN

STATUS

SPATIAL COORDINATION (PLANNING)

PROJECT NO.

D19-041

DRAWING NO.

D19-041 / DWG / 0003

REVISION NO.

.08

SCALE

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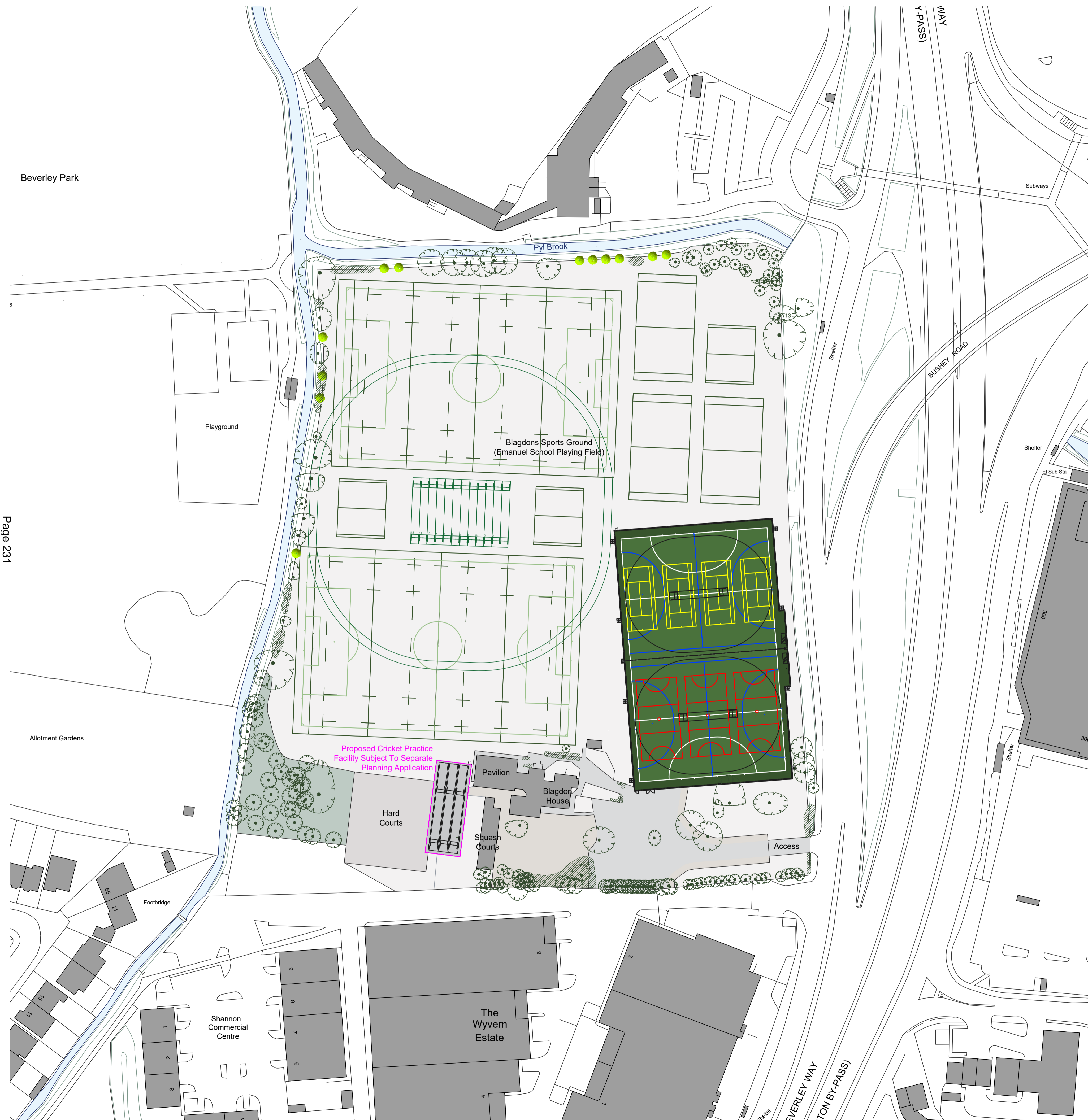
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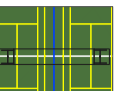




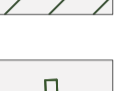
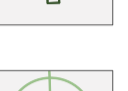

DRAWN

TB

CHECKED



LEGEND

-  ALL-WEATHER SPORTS PITCH (AWP)
-  REPLACEMENT NON-TURF CRICKET PRACTICE FACILITY SUBJECT TO SEPARATE PLANNING APPLICATION
-  SOFT PLANTING / LANDSCAPING
-  EXISTING TREE
-  EXISTING SCRUB AREA
-  FUTURE AUTUMN GRASS PLAYING PITCH
-  FUTURE SPRING GRASS PLAYING PITCH
-  FUTURE SUMMER GRASS PLAYING PITCH

PLAYING FIELD ARRANGEMENT

AUTUMN TERM

2NO. 15 V 15 RUGBY UNION PITCHES
94 X 70M FIELD OF PLAY
6M DEEP IN GOALS

3NO. MINI RUGBY UNION PITCHES
40 X 22M FIELD OF PLAY

3NO. MINI RUGBY UNION PITCHES
30 X 20M FIELD OF PLAY

SPRING TERM

2NO. 11V11 SENIOR FOOTBALL PITCHES
100 X 64M FIELD OF PLAY

SUMMER TERM

1NO. SENIOR CRICKET SQUARE
12NO. 20.12 X 3.05 CRICKET PITCHES
37M JUNIOR OUTFIELD BOUNDARY
46M SENIOR OUTFIELD BOUNDARY
3M SAFETY ZONE AROUND BOUNDARY

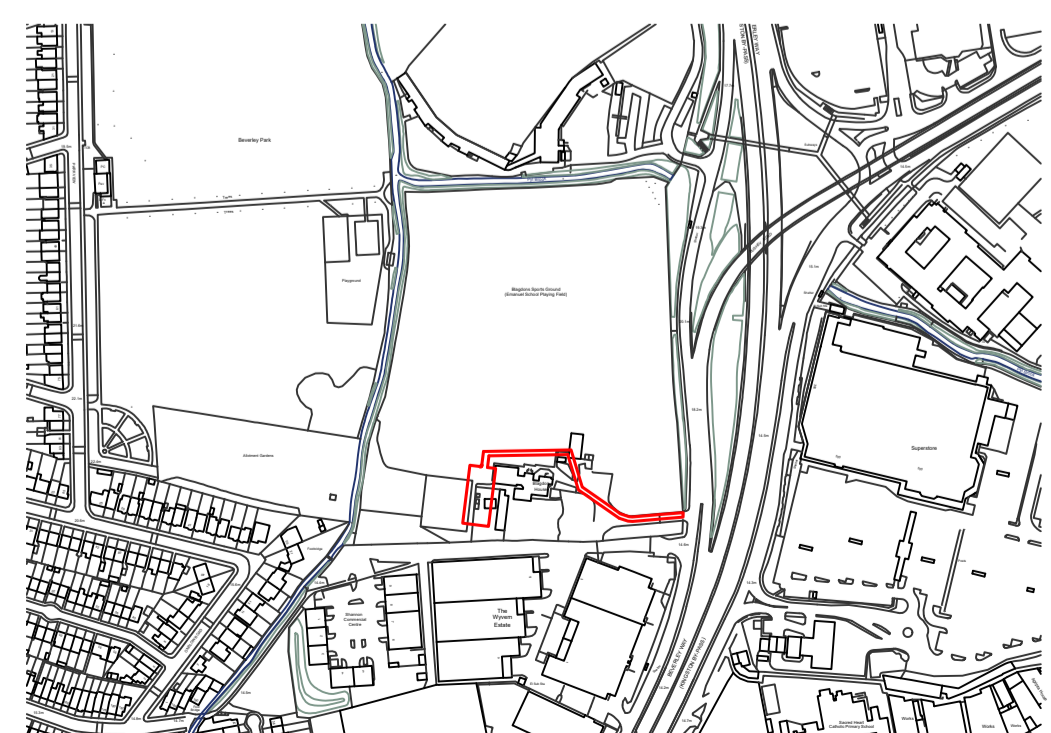
1NO. NON-TURF CRICKET PRACTICE FACILITY
3NO. 30 X 3.66M PRACTICE PITCHES
(SUBJECT TO SEPARATE PLANNING APPLICATION)

ALL YEAR

1NO. ALL-WEATHER SPORTS PITCH (AWP) PROVIDING FLEXIBLE SPORTS ACTIVITIES WITH:

HOCKEY / TENNIS / NETBALL / CRICKET

PLAN KEY



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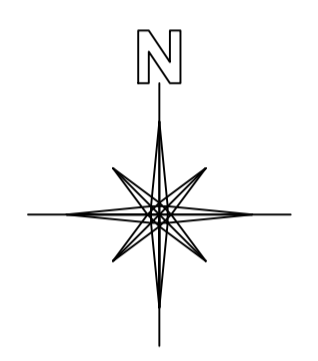
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Emanuel School

CLIENT
EMANUEL SCHOOL

PROJECT
ALL-WEATHER SPORTS PITCH (AWP)

LOCATION
**BLAGDONS SPORTS GROUND
BEVERLEY WAY
NEW MALDEN
KT3 4PU**

DRAWING TITLE
PROPOSED SITE PLAN

STATUS
SPATIAL COORDINATION (PLANNING)

PROJECT NO.
D19-041

DRAWING NO.
D19-041 / DWG / 0004


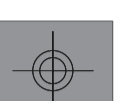
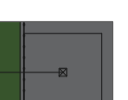

REVISION NO.
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SCALE	SHEET	DRAWN	CHECKED
1:750	A1	TB	



Page 232

LEGEND

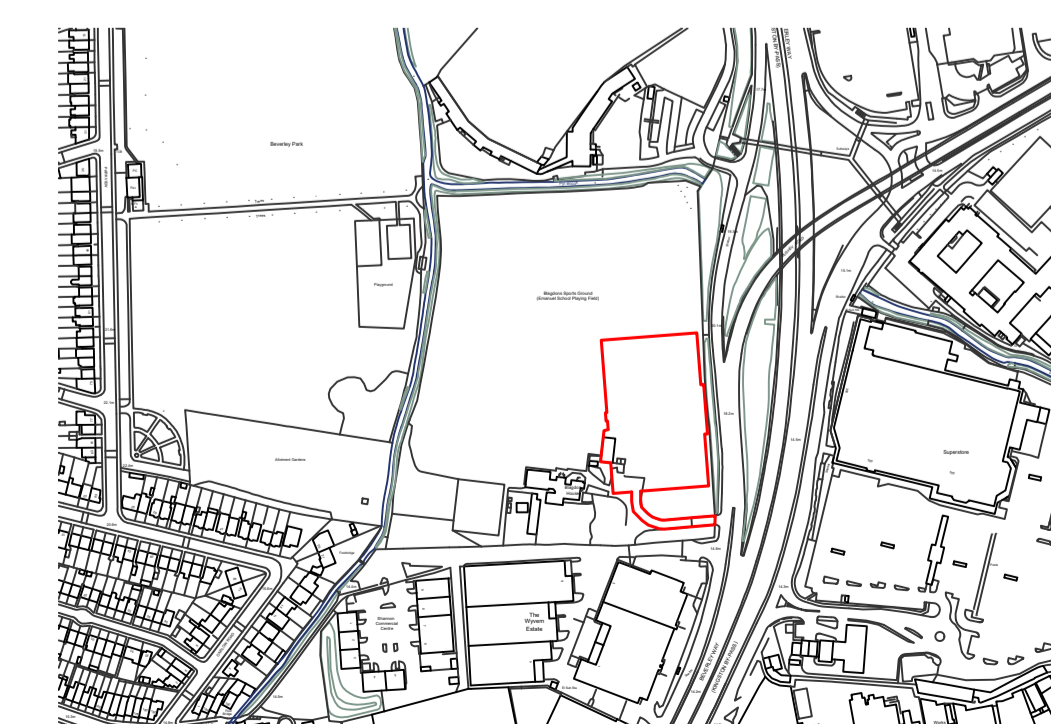
-  FIELD OF PLAY
-  TEAM SHELTERS
-  BALL STOP FENCING
-  FLOODLIGHTS
-  DIVIDING NET SYSTEM
-  GATED ENTRANCE

**ALL-WEATHER SPORTS PITCH (AWP)
FLEXIBLE SPORTS ACTIVITIES**

ALL-WEATHER SPORTS PITCH PROVIDING FLEXIBLE SPORTS ACTIVITIES WITH:

- 1NO. 11V11 HOCKEY FIELD (91.4 X 55M)
- 2NO. 7V7 MINI HOCKEY PITCHES (55 X 43M)
- 4NO. TENNIS COURTS (23.77 X 10.97M EA.)
- 3NO. NETBALL COURTS (30.15 X 15.25M EA.)
- 2NO. CRICKET PITCHES (SOFTBALL / WIND-BALL)

PLAN KEY



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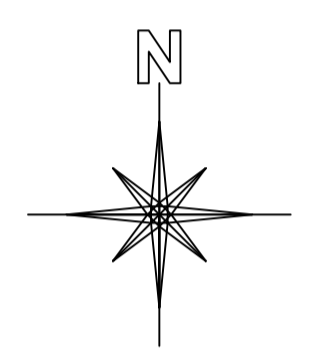
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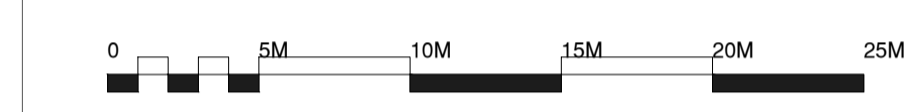
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REVISION	DATE	DRAWN	DESCRIPTION
.07	20 10 2021	TB	FINAL PLANNING PROPOSAL
.08	23 02 2022	TB	PROPOSED CRICKET PRACTICE FACILITY SEPARATED FROM AWP APPLICATION



Emanuel School

CLIENT
EMANUEL SCHOOL

PROJECT
ALL-WEATHER SPORTS PITCH (AWP)

LOCATION
**BLAGDONS SPORTS GROUND
BEVERLEY WAY
NEW MALDEN
KT3 4PU**

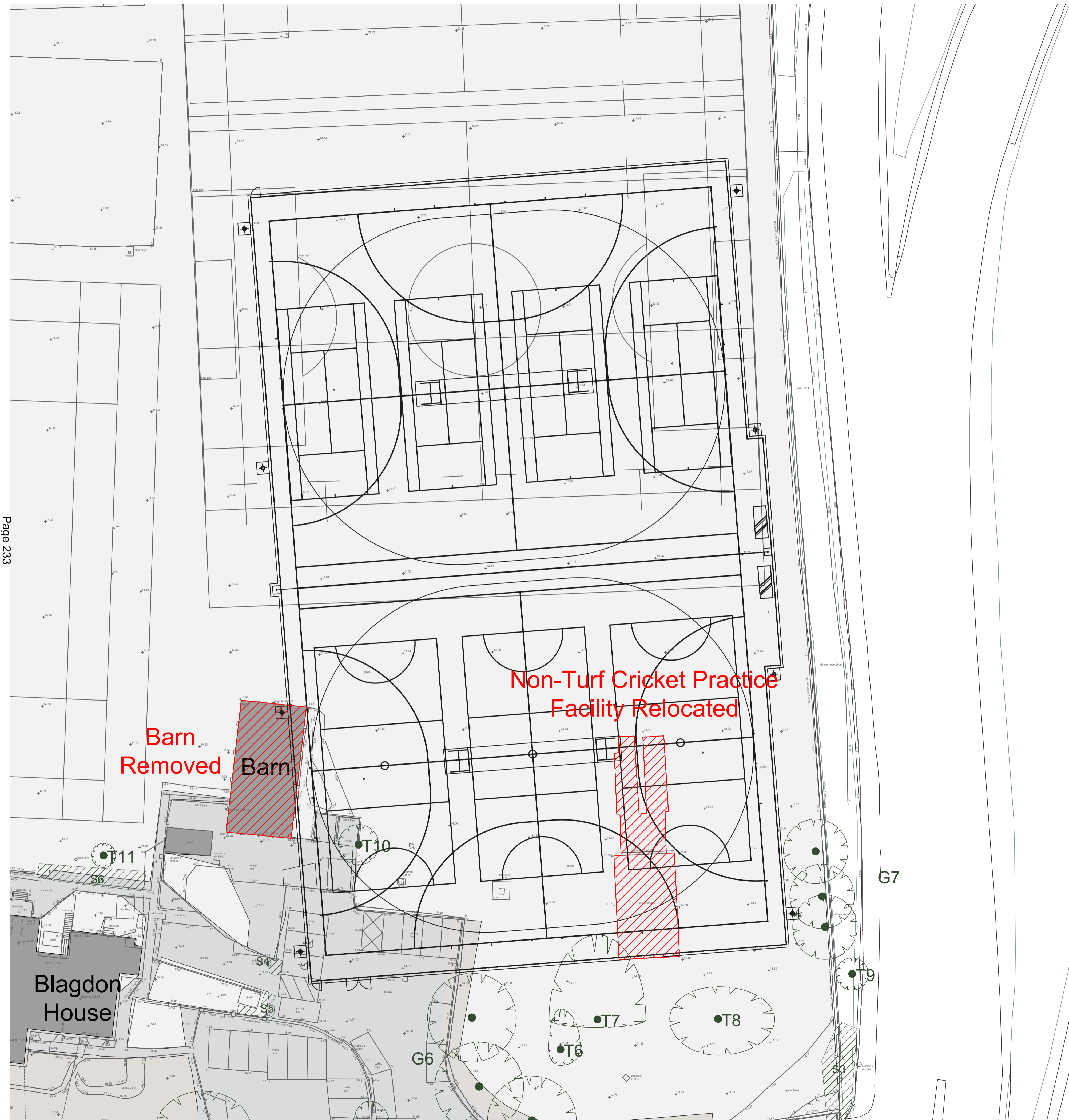
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**PROPOSED AWP
FLOOR PLAN**

STATUS
**SPATIAL COORDINATION
(PLANNING)**

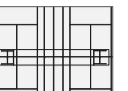
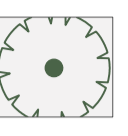

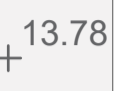
PROJECT NO.
D19-041

DRAWING NO.
D19-041 / DWG / 0005

SCALE: 1:250 SIZE: A1 DRAWN: TB CHECKED: TB REVISION NO.: .08



Page 233

- LEGEND
-  ALL-WEATHER SPORTS PITCH (AWP)
 -  EXISTING TREE
 -  EXISTING SCRUB AREA
 -  TOPOGRAPHICAL / LAND SURVEY

NOTES

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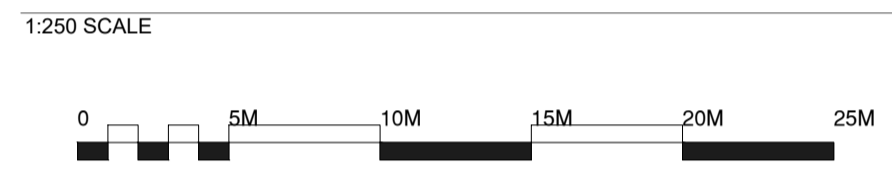
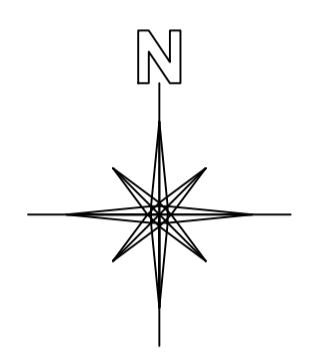
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Emanuel School

CLIENT
 EMANUEL SCHOOL

PROJECT
 ALL-WEATHER SPORTS PITCH (AWP)

LOCATION
 BLAGDONS SPORTS GROUND
 BEVERLEY WAY
 NEW MALDEN
 KT3 4PU

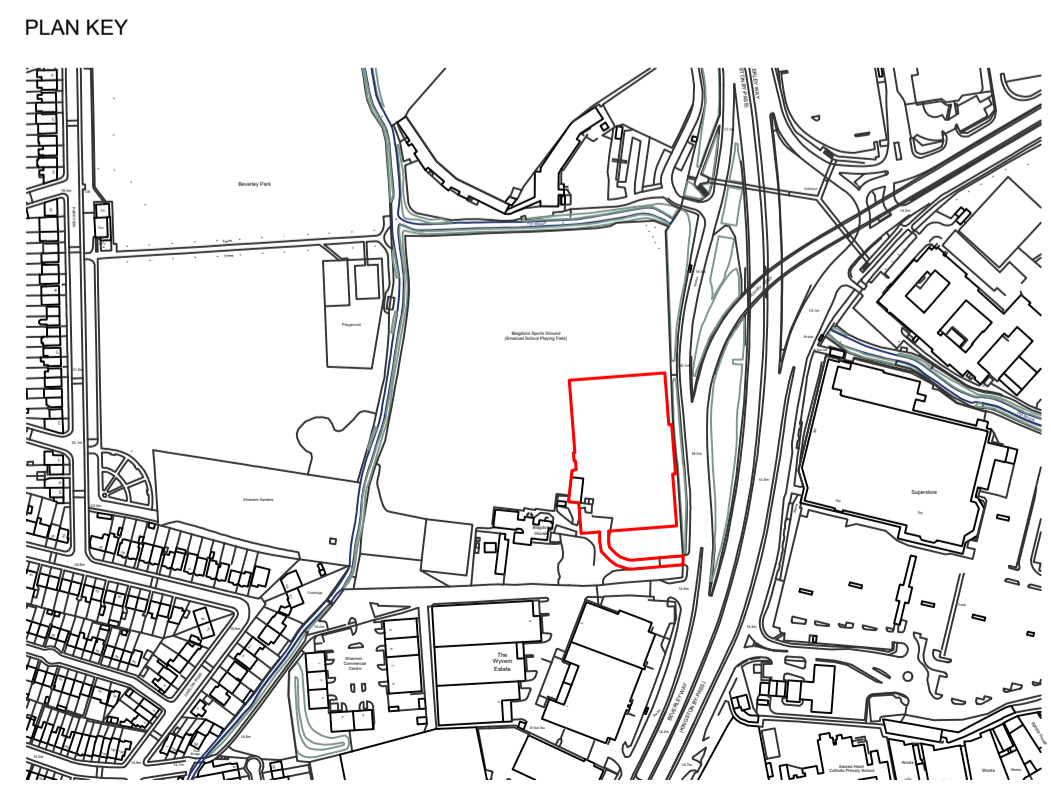
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PROPOSED AWP LAYOUT

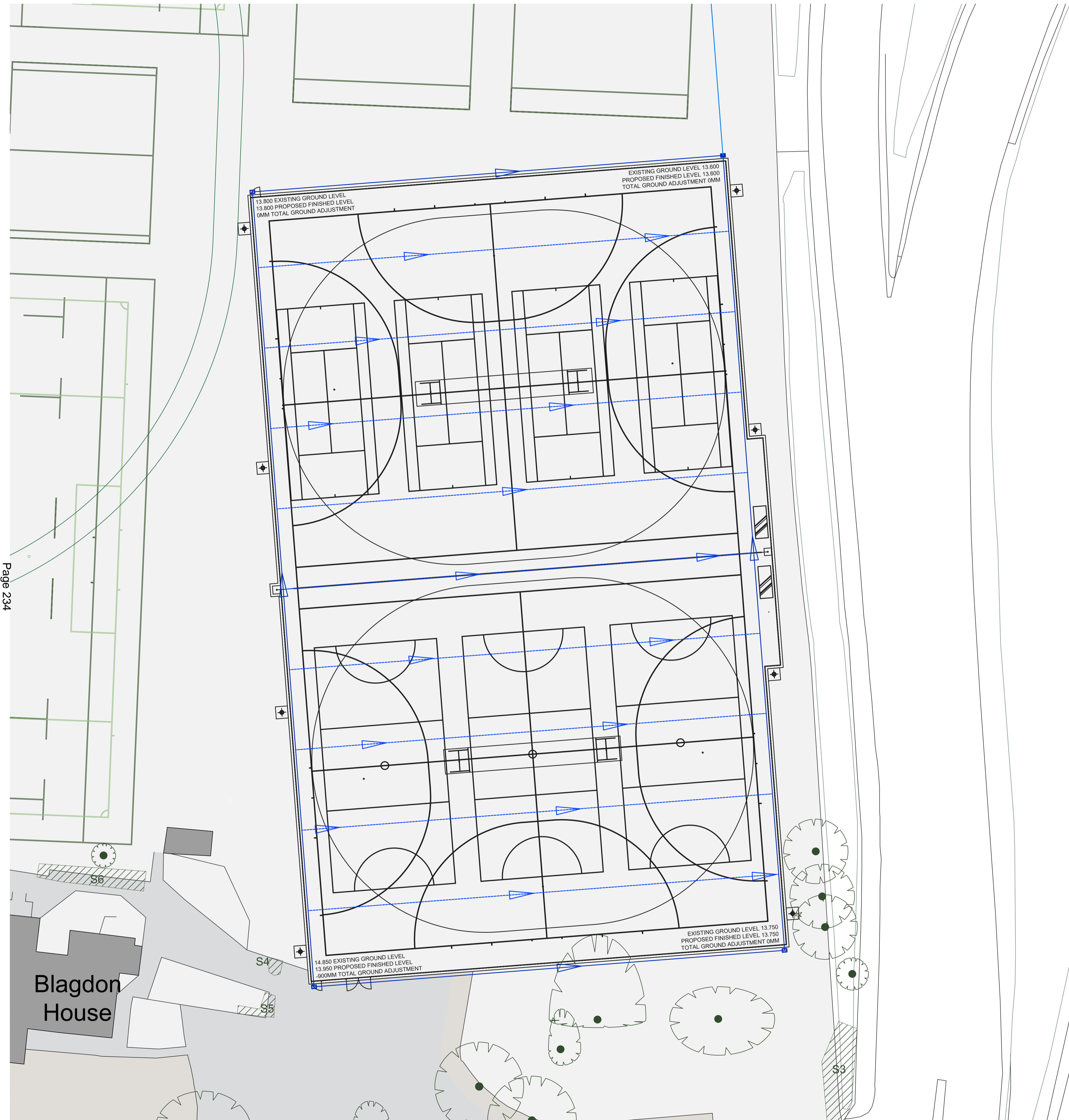
STATUS
 SPATIAL COORDINATION (PLANNING)

PROJECT NO.
 D19-041

DRAWING NO.
 D19-041 / DWG / 0006

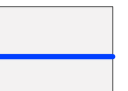
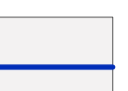


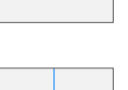
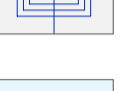
SCALE	SHEET	DRAWN	CHECKED	REVISION NO.
1:250	A1	TB		.08





Page 234

LEGEND

-  80MM Ø UPVC PERFORATED CORRUGATED LATERAL DRAINAGE PIPES
-  160MM Ø UPVC PERFORATED CORRUGATED CARRIER DRAINAGE PIPES
-  225MM Ø UPVC PERFORATED CARRIER DRAINAGE PIPE
-  600MM LONG X 600MM WIDE X 1000MM DEEP PRE-CAST CONCRETE (PCC) SILT-TRAP / INSPECTION CHAMBERS (ICS)
-  VORTEX FLOW CONTROL CONFORMING TO BS7158 WITHIN PRE-CAST CONCRETE (PCC) SILT-TRAP / INSPECTION CHAMBER (IC)
-  SURFACE WATER OUTFALL CONNECTING TO LOCAL WATER COURSE

SURFACE WATER OUTFALL TO PYL BROOK



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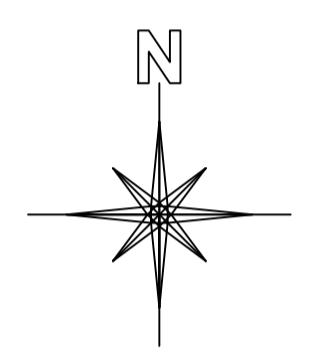
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1:250 SCALE



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Emanuel School

CLIENT
EMANUEL SCHOOL

PROJECT
ALL-WEATHER SPORTS PITCH (AWP)

LOCATION
**BLAGDONS SPORTS GROUND
BEVERLEY WAY
NEW MALDEN
KT3 4PU**

DRAWING TITLE
PROPOSED AWP DRAINAGE

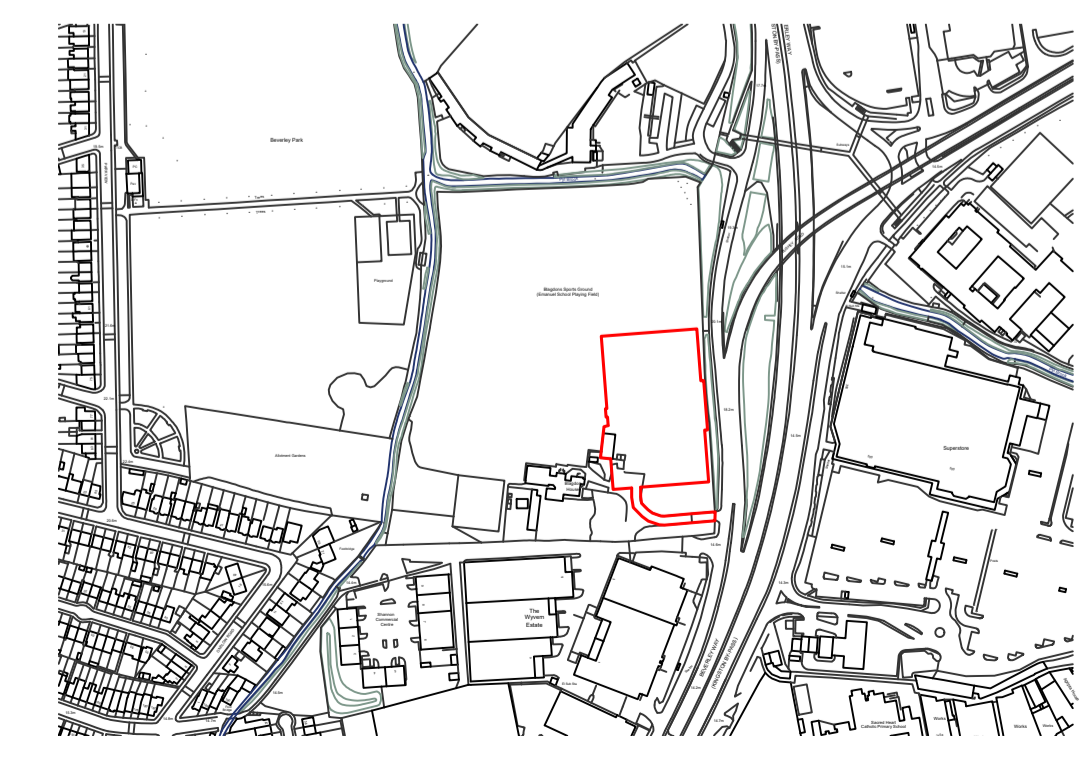
STATUS
SPATIAL COORDINATION (PLANNING)

PROJECT NO.
D19-041

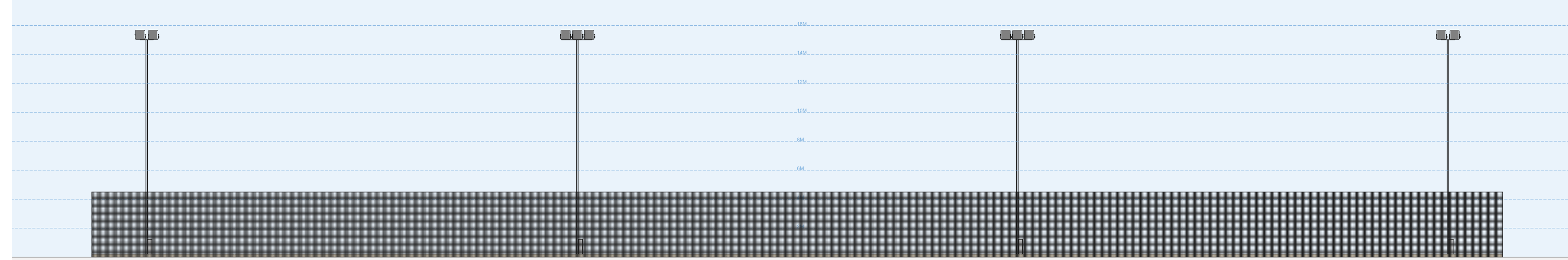
DRAWING NO.
D19-041 / DWG / 0007

SCALE	SIZE	DRAWN	CHECKED	REVISION NO.
1:250	A1	TB		.08

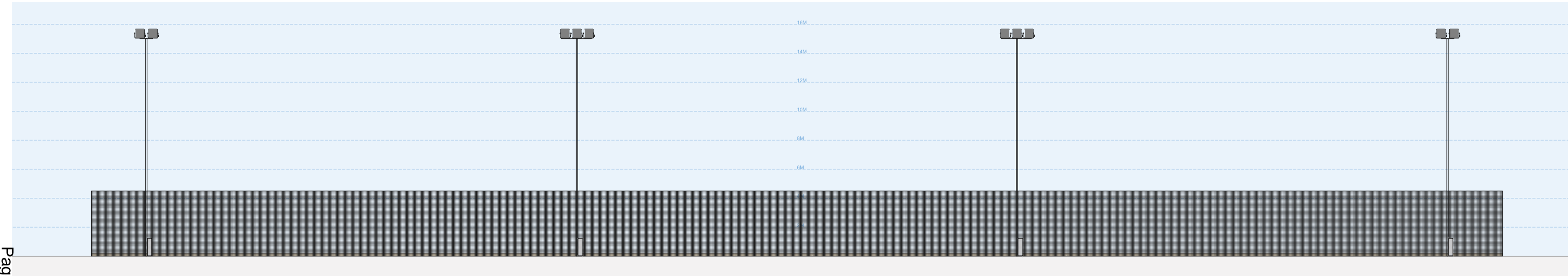
PLAN KEY



EASTERN ELEVATION



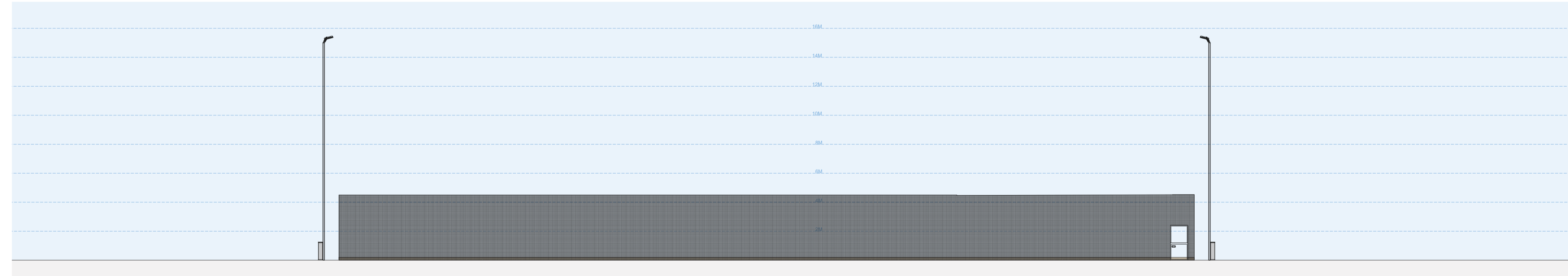
WESTERN ELEVATION



SOUTHERN ELEVATION

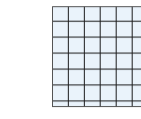


NORTHERN ELEVATION



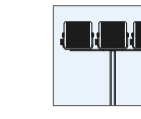
Page 235

LEGEND



4.50M HIGH BALL STOP FENCING

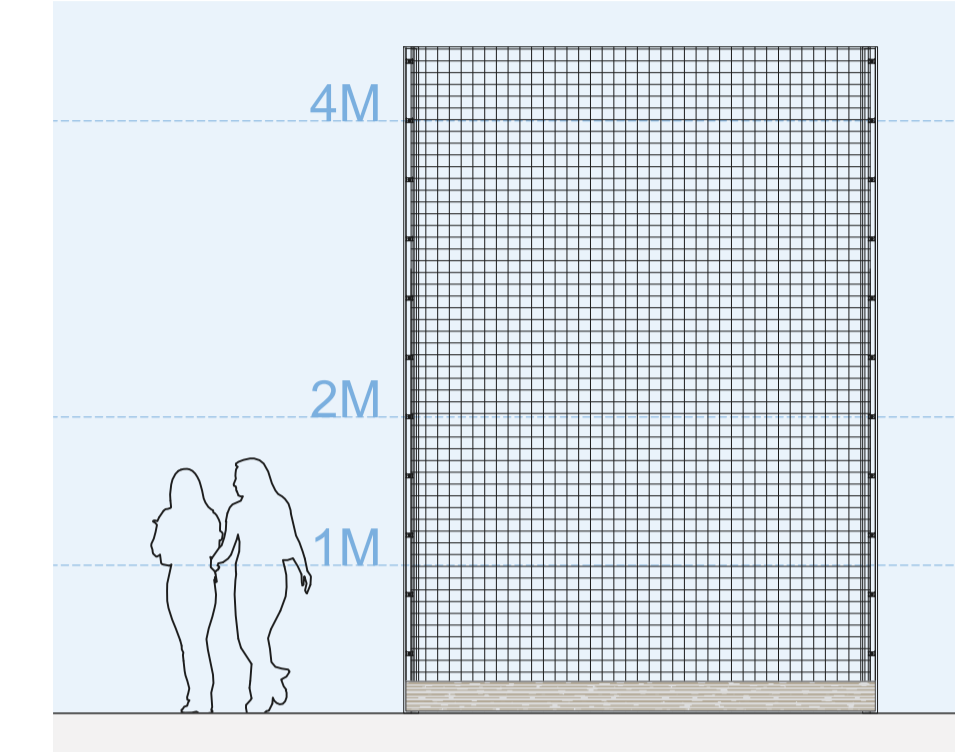
COMPRISING PLASTIC COATED 50X50MM WELD MESH COLOURED RAL9005 JET BLACK PLUS 200X50MM TIMBER KICK BOARDS



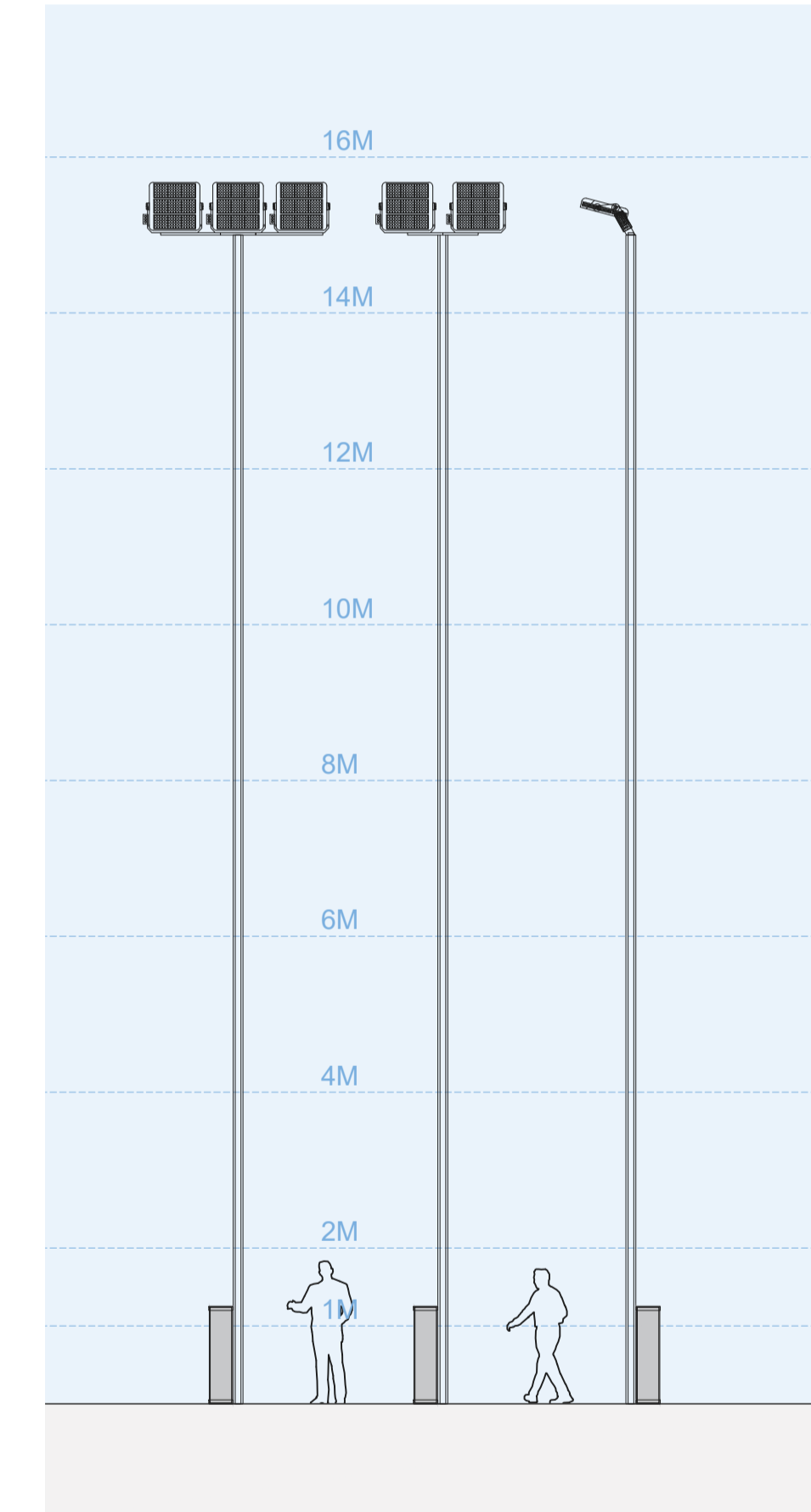
15.00M HIGH LED FLOODLIGHTS

COMPRISING STEEL MASTS FINISHED GALVANISED (Z275) SELF-COLOURED MOUNTED WITH LED LUMINAIRES WITHIN DIE-CAST HOUSINGS FINISHED RAL7001 SILVER GREY

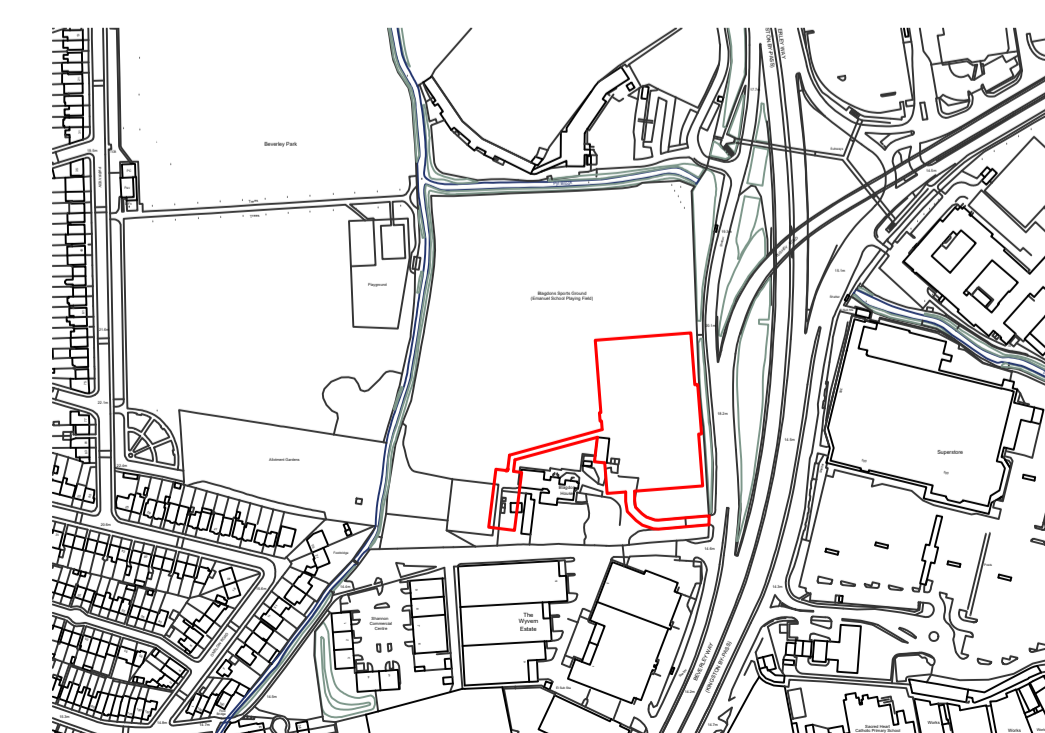
BALL STOP FENCING



FLOODLIGHTS



PLAN KEY



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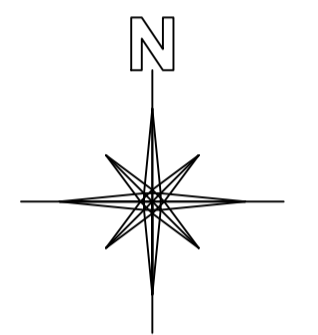
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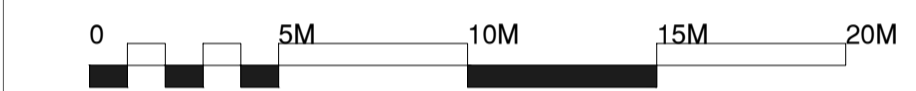
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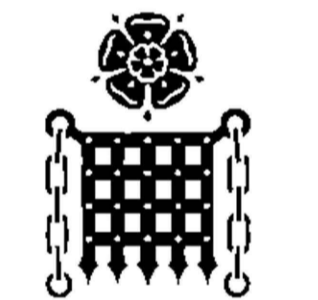
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1:200 SCALE



REVISION	DATE	DRAWN	DESCRIPTION
.07	20 10 2021	TB	FINAL PLANNING PROPOSAL



Emanuel School

CLIENT

EMANUEL SCHOOL

PROJECT

ALL-WEATHER SPORTS PITCH (AWP)

LOCATION

BLAGDONS SPORTS GROUND
BEVERLEY WAY
NEW MALDEN
KT3 4PU

DRAWING TITLE

PROPOSED AWP ELEVATIONS

STATUS

SPATIAL COORDINATION (PLANNING)

PROJECT NO.

D19-041

DRAWING NO.

D19-041 / DWG / 0008

REVISION NO.

.07

SCALE

1:200

SIZE

A1

DRAWN


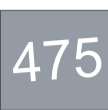

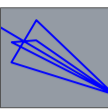
TB

CHECKED



Page 236

LEGEND

-  LUX CONTOUR (50 LUX / 25 LUX / 15 LUX / 5 LUX / 1 LUX)
-  475 LUX MEASUREMENT ONTO FIELD OF PLAY (TASK LIGHT)
-  0 LUX MEASUREMENT ONTO SURROUNDING LAND (SPILL LIGHT / BACK LIGHT)
-  LED LUMINAIRE AIMING DIRECTION

FLOODLIGHTING EQUIPMENT:

- 8 X 15M MASTS (M1-M8)
- EACH CARRYING THE FOLLOWING FLOODLIGHTS:
- M1,M4,M5 & M8 - 2 X PHILIPS BVP528 NB/A35/BL
- M2,M3,M6 & M7 - 2 X PHILIPS BVP528 NB/A35/BL AND 1 X PHILIPS BVP518 NB/A35/BL

INITIAL ILLUMINANCE LEVEL:

(100HRS) E.I.AVE = 449LUX

MAINTAINED ILLUMINANCE LEVEL:

(4000HRS) E.I.AVE = 404LUX

UNIFORMITY:

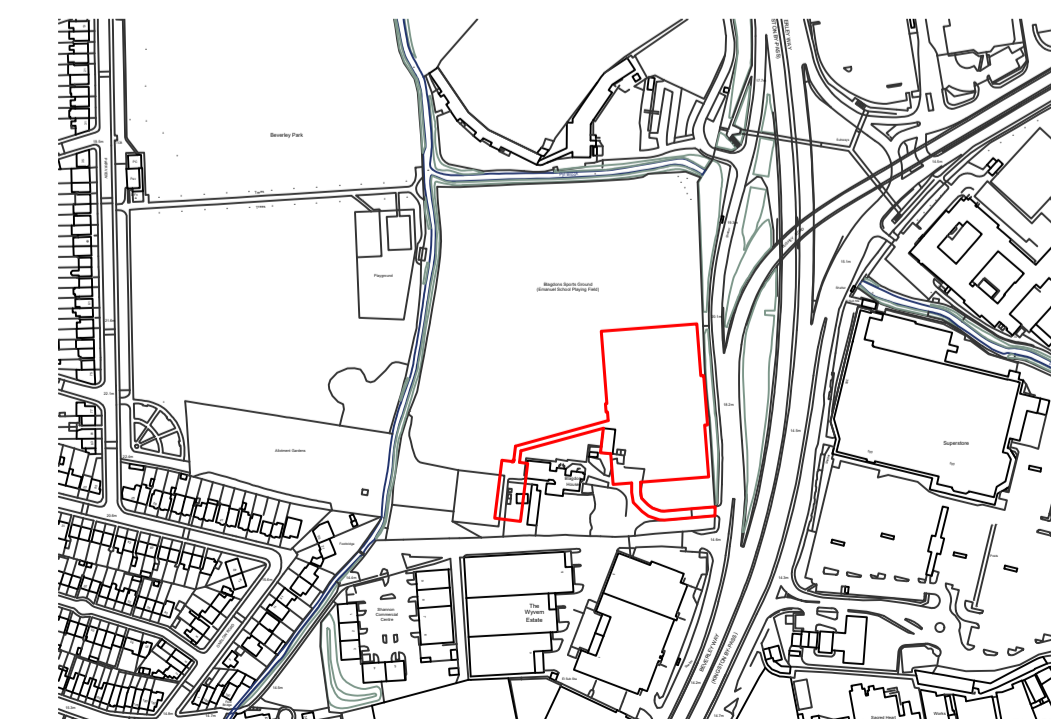
EMIN/EAVE = 0.71

GRID VALUES IN LUX (INITIAL)
 GRID INTERVAL 5M
 CONTOUR VALUES IN LUX (INITIAL)
 CONTOURS 1, 5, 15, 25, 50 LUX

MAINTENANCE FACTOR:

0.90

PLAN KEY



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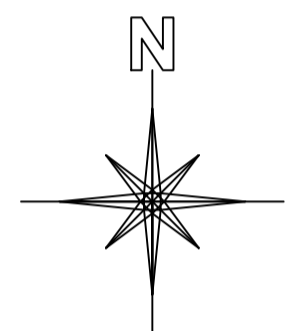
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1:750 SCALE



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Emanuel School

CLIENT
EMANUEL SCHOOL

PROJECT
ALL-WEATHER SPORTS PITCH (AWP)

LOCATION
**BLAGDONS SPORTS GROUND
 BEVERLEY WAY
 NEW MALDEN
 KT3 4PU**

PROPOSED AWP FLOODLIGHTS

STATUS
SPATIAL COORDINATION (PLANNING)

PROJECT NO.
D19-041

DRAWING NO.
D19-041 / DWG / 0013

SCALE	SHEET	DRAWN	CHECKED
1:750	A1	TB	.07

Committee: Planning Applications

Date: 18th August 2022

Subject: Planning Appeal Decisions

Lead officer: Head of Sustainable Communities

Lead member: Chair, Planning Applications Committee

Recommendation:

That Members note the contents of the report.

1. PURPOSE OF REPORT AND EXECUTIVE SUMMARY

- 1.1 For Members' information recent decisions made by Inspectors appointed by the Secretary of State for Communities and Local Government in respect of recent Town Planning Appeals are set out below.
- 1.2 The relevant Inspectors decision letters are not attached to this report but can be viewed by following each individual link. Other agenda papers for this meeting can be viewed on the Committee Page of the Council Website via the following link:

[LINK TO COMMITTEE PAGE](#)

DETAILS

Application Number	20/P1675
Appeal number:	APP/T5720/W/21/3275987
Site:	RO Dawlish Avenue, Former Wellington Works, Wimbledon Park
Development:	OUTLINE PERMISSION ('APPEARANCE' RESERVED) FOR DEMOLITION OF INDUSTRIAL BUILDINGS + TELECOM TOWER AND ERECTION OF A PART 2/ PART 4 STOREY BUILDING PROVIDING 23 x FLATS WITH ROOF TERRACE AND ASSOCIATED PARKING.
Recommendation:	Refuse (Delegated)
Appeal Decision:	DISMISSED
Date of Appeal Decision:	5 th July 2021

[click LINK TO DECISION NOTICE](#)

DETAILS

Application Number **20/P1665**
Appeal number: APP/T5720/W/21/3275985
Site: RO Dawlish Avenue, Former Wellington Works, Wimbledon Park
Development: OUTLINE APPLICATION ('APPEARANCE' RESERVED) FOR
DEMOLITION OF INDUSTRIAL BUILDINGS + TELECOM TOWER AND
ERECTION OF A PART 2/ PART 3/ PART 4 STOREY BUILDING WITH
FLEXIBLE B1 FLOORSPACE AND 16 x FLATS WITH ROOF TERRACE
AND ASSOCIATED PARKING.
Recommendation: Refuse (Delegated)
Appeal Decision: **DISMISSED**
Date of Appeal Decision: 5th July 2021

[click LINK TO DECISION NOTICE](#)

DETAILS

Application Number **21/P2671**
Appeal number: APP/T5720/W/21/3288313
Site: 139 Galpins Road, Thornton Heath CR7 6EU
Development: ERECTION OF A FRONT AND REAR ROOF EXTENSION A 1ST FLOOR
REAR EXTENSION
Recommendation: Refuse (Delegated)
Appeal Decision: **DISMISSED**
Date of Appeal Decision: 22nd July 2021

[click LINK TO DECISION NOTICE](#)

DETAILS

Application Number **21/P4277**
Appeal number: APP/T5720/W/22/3296346
Site: 60 Greenwood Road, Mitcham CR4 1PE
Development: RETENTION OF A SECONDARY SINGLE STOREY REAR EXTENSION.
Recommendation: Refuse (Delegated)
Appeal Decision: **DISMISSED**
Date of Appeal Decision: 27th July 2021

Alternative options

- 3.1 The appeal decision is final unless it is successfully challenged in the Courts. If a challenge is successful, the appeal decision will be quashed and the case returned to the Secretary of State for re-determination. It does not follow necessarily that the original appeal decision will be reversed when it is re-determined.
- 3.2 The Council may wish to consider taking legal advice before embarking on a challenge. The following applies: Under the provision of Section 288 of the Town & Country Planning Act 1990, or Section 63 of the Planning (Listed Buildings and Conservation Areas) Act 1990, a person or an establishment who is aggrieved by a decision may seek to have it quashed by making an application to the High Court on the following grounds: -
 1. That the decision is not within the powers of the Act; or
 2. That any of the relevant requirements have not been complied with; (relevant requirements means any requirements of the 1990 Act or of the Tribunal's Land Enquiries Act 1992, or of any Order, Regulation or Rule made under those Acts).

1 CONSULTATION UNDERTAKEN OR PROPOSED

- 1.1. None required for the purposes of this report.

2 TIMETABLE

- 2.1. N/A

3 FINANCIAL, RESOURCE AND PROPERTY IMPLICATIONS

- 3.1. There are financial implications for the Council in respect of appeal decisions where costs are awarded against the Council.

4 LEGAL AND STATUTORY IMPLICATIONS

- 4.1. An Inspector's decision may be challenged in the High Court, within 6 weeks of the date of the decision letter (see above).

5 HUMAN RIGHTS, EQUALITIES AND COMMUNITY COHESION IMPLICATIONS

5.1. None for the purposes of this report.

6 CRIME AND DISORDER IMPLICATIONS

6.1. None for the purposes of this report.

7 RISK MANAGEMENT AND HEALTH AND SAFETY IMPLICATIONS

7.1. See 6.1 above.

8 BACKGROUND PAPERS

8.1. The papers used to compile this report are the Council's Development Control service's Town Planning files relating to the sites referred to above and the agendas and minutes of the Planning Applications Committee where relevant.

Committee: Planning Applications Committee

Date: 18 August 2022

Wards: All

Subject: PLANNING ENFORCEMENT - SUMMARY OF CASES

Lead officer: HEAD OF SUSTAINABLE COMMUNITIES

**Lead member: CABINET MEMBER FOR HOUSING AND SUSTAINABLE
DEVELOPMENT – COUNCILLOR ANDREW JUDGE**

COUNCILLOR AIDAN MUNDY, CHAIR, PLANNING APPLICATIONS COMMITTEE

Contact Officers Ray Littlefield: 0208 545 3911
Ray.Littlefield@merton.gov.uk
Raymond Yeung: 0208 545 4352
Raymond.Yeung@merton.gov.uk

Recommendation:

That Members note the contents of the report.

1. Purpose of report and executive summary

This report details a summary of casework being dealt with by the Planning Enforcement Team and contains figures of the number of different types of cases being progressed, with brief summaries of all new enforcement notices and the progress of all enforcement appeals.

Current Enforcement Cases:	516	New Appeals:	(0)
New Complaints	31	Instructions to Legal	1
Cases Closed	32	Existing Appeals	1
New Enforcement Notices Issued		TREE ISSUES	
Breach of Condition Notice:	0	Tree Applications Received	104
New Enforcement Notice issued	0	% Determined within time limits:	58%
S.215: ³	0	High Hedges Complaint	0
Others (PCN, TSN)	1	New Tree Preservation Orders (TPO)	1
Total	1	Tree Replacement Notice	0
Prosecutions: (instructed)	0	Tree/High Hedge Appeal	0
		5-Day notice	5

Note (**figures are for the period from (from 5th July to 8th August 2022)**). The figure for current enforcement cases was taken directly from M3 crystal report.

¹ Totals in brackets are previous month's figures

² confirmed breach but not expedient to take further action.

³ S215 Notice: Land Adversely Affecting Amenity of Neighbourhood.

It should be noted that due to the pandemic the Planning Inspectorate have over a year's backlog of planning enforcement appeals to determine.

2.0 Recent Enforcement Actions

LAND AT 8A-F SOUTH PARK ROAD, WIMBLEDON, LONDON, SW19 8ST

Temporary Stop Notice

On 12th July 2022, the Council has issued this temporary stop notice alleging that there has been a breach of planning control on the land described in paragraph 4 below.

This temporary stop notice is issued by the Council, in exercise of their power in section 171E of the Act, because they think that it is expedient that the activity specified in this notice should cease on the land described. The Council now prohibits the carrying out of the activity specified in this notice.

A breach of planning control has taken place as a result of the commencement of development works on the Land carried out prior to the discharge of condition 6 of planning permission granted by the Council bearing reference number 21/P3487 for the erection of an additional floor comprising 3 x self-contained residential flats.

The commencement of development works creates an amenity harm to the neighbouring occupiers with regards to noise, dust and general safety, the pedestrians and traffic flow on the highway, the appearance of the property and adjacent highway is a detriment to the visual appearance of the property and streetscene as a result of the commencement of works

For the reasons above it is considered expedient to serve a Temporary Stop Notice to remedy the breach of planning control identified.

310 & 372 Grand Drive SW20 9NQ – Untidy land

Before



After



An unannounced visit was made by the council in April 2022, soon after the investigation by an enforcement officer and making contact to the property owners, the land was cleared in May 2022.

70 Linkway, SW20 9AZ. Unauthorised hardsurfacing of front garden.

Before



After



The breach has now been rectified the hardsurfacing or cement has been removed and the front garden has been reinstated with a grassed area and a wooden boundary fence.

Land at 22 Vectis Road London SW17 9RG -Untidy land

A section 215 notice has been served to the above property, the rubbish and vegetation to the front and the rear of the property increases its adverse impact on the amenity of the area. Section 215 of the Town and Country Planning Act (1990) provides local authorities with an additional discretionary power for requiring landowners to clean up 'land adversely affecting the amenity of the neighbourhood'

This matter concerns the adverse impact that the condition of the land at 22 Vectis Road has on the amenity of the surrounding area. The owner of the land has failed to clear rubbish and vegetation to the front and to the rear. To the front this includes, but is not limited to: large weeds in excess of three metres in height, trees and bushes, abandoned bins, abandoned car parts, household plastics, wooden boards, bricks which have been abandoned, motorcycle helmet, wooden boards, a mattress, a white household appliance. To the rear this includes, but is not limited to: overgrown vegetation, including overgrowth of seedling trees and shoots, household waste and appliances, garden waste and appliances and a derelict outbuilding which is in a state of disrepair.

Enforcement officers will be re-visiting the site soon to see if the notice has been complied with.

61a WORPLE ROAD LONDON SW19 4LB. A Breach of Condition Notice was served. The developers failed to provide screening required by condition on a planning permission for a new residential development, no screening leads to an overlooking and loss of privacy issue towards existing neighbouring adjoining residents.

The owner has not complied with the notice following a compliance site visit check. A letter of alleged offence was served at the beginning of August.

12A Deer Park Road, South Wimbledon, London SW19 3TL.

An enforcement notice was served from a change of industrial/office unit into a 16 unit House In Multiple Occupation (HMO), it did not receive planning permission and is expedient due to the creation of the poor residential accommodation in a commercial area. The notice requires the cessation of the HMO use requiring to remove kitchen and toilets from the units.

Land to the rear of 42 Tamworth Lane, Mitcham, CR4 1DA. This is concerning a s215 notice served on untidy land. A s215 notice was issued on 10th May 2021. This notice requires compliance at the end of July 2021 requiring the Land to be tidied up / cleared.

The Land is again being fly tipped a further s215 Notice was issued on 28th February 2022 including enclosing and clearing the untidy / overgrown Land.

The council is in process of taking direction action to clear the land again.

100 Garth Road, Morden, SM4 4LR. Relates to the unauthorised erection of a self-contained residential unit on top of an existing garage. An enforcement notice has been served dated 28th March 2022, the Notice will take effect on 2nd May 2022 with a 3 months compliance period unless an appeal is submitted. The notice requires: Completely demolish the Unit or Restore that part of the property to its condition prior to the breach of planning control by complying with approved drawing number E-1672-PJ-03A planning permission 17/P2214.

Land at 225-231 Streatham Road, SW16.

A Temporary Stop Notice was issued on 2nd February 2022 requiring the immediate cessation of use of the Land as a car wash. The notice took immediate effect, and the unauthorised use was ceased, and the Notice fully complied with.

Parkside House, 52/54 High Street, Wimbledon, London SW19 5AY. Commercial Unit on Land to the rear. A Temporary Stop Notice was issued on 31st December 2021 relating to works being undertaken creating an unauthorised rear ground floor extension. The Notice came into immediate effect, the Notice will cease to have an effect after 27th January 2022. Works

Stopped, Notice complied with. Further investigation was taken on the shopfront, this has since received planning permission. Also investigation is in process to the rear air-conditioning units.

193 London Road, CR4 2JD. This is concerning a s215 notice served on untidy land. The Land is cleared.

Successful Prosecution case-update

7 Streatham Road, Mitcham, CR4 2AD

A pre-application discussion has taken place and the owners are co-operating to mitigate the harm from the outbuilding, this followed a warning letter to the owners threatening direct action for the remedial works following non-compliance to enforcement notices. As previously mentioned, The Council served two enforcement notices on 6th June 2019, requiring the outbuilding to be demolished and to clear debris and all other related materials.

A letter has been written to the land owner to state that The Council is minded to take direct action by engaging a building contractor to undertake the works to secure compliance with the enforcement notices, pursuant to section 178, Town and Country Planning Act 1990 (as amended). This would result in further costs that would be recoverable from you directly. Before embarking on this course of action we wish to allow you a further opportunity to voluntarily comply with the enforcement notices.

The compliance date of the Enforcement Notice relating to the outbuilding to be demolished and to clear debris and all other related materials has now passed without compliance.

The owner has responded with a pre-application meeting which took place at the beginning of July 2022 and appears to be co-operating with the council to remedy the harm by offering to reduce the size of the said breaches.

A brief summary;

The plea hearing took place at Lavender Hill Magistrates Court, where the defendant pleaded not guilty and the second hearing is due on the 14th January 2020.

A second hearing was held on 14th January 2020, and adjourned until 4th February 2020 in order for the defendant to seek further legal advice.

The defendant again appeared in court and pleaded not guilty, a trial date was set for 21st May 2020. Due to the Covid-19 pandemic this has been postponed. The case has been listed for a 'non-effective' hearing on Tuesday 14 July 2020, where a new trial date will be set.

This was postponed until another date yet to be given. The Council has now instructed external Counsel to prosecute in these matters.

The next 'non-effective' hearing date is 2nd October 2020. This date has been re-scheduled to 27th November 2020. This was again re-scheduled to 4th January 2021. Outcome not known at the time of compiling this report.

A trial date has now been set for 28th and 29th April 2021.

At trial the defendant changed his plea from not guilty to guilty on the two charges of failing to comply with the two Planning Enforcement Notices, however due to the current appeals with the Planning Inspectorate relating to two planning application appeals associated with the two illegal developments, sentencing was deferred until 7th October 2021 at Wimbledon Magistrates Court.

The two planning appeals were dismissed dated 5th October 2021.

Sentencing was again deferred until 16th December 2021 at Wimbledon Magistrates Court.

The result of the sentencing hearing was:

1. Fine for the outbuilding EN: £6,000, reduced by 10% so £5,400
2. Fine for the dormer EN: £12,000, reduced by 10% so £10,800
3. Surcharge: £181
4. Costs: £14,580
5. Total being £30,961. To be paid over a period of three years in monthly instalments.

The defendant was fined for the outbuilding and the dormer extensions due to non-compliance with two enforcement notices.

Existing enforcement appeals

2

Appeals determined

0

New Enforcement Appeals

0

3.4 Requested update from PAC

None

4. Consultation undertaken or proposed

None required for the purposes of this report

- 5 **Timetable**
N/A
6. **Financial, resource and property implications**
N/A
7. **Legal and statutory implications**
N/A
8. **Human rights, equalities and community cohesion implications**
N/A
9. **Crime and disorder implications**
N/A
10. **Risk Management and Health and Safety implications.**
N/A
11. **Appendices – the following documents are to be published with this report and form part of the report Background Papers**
N/A
12. **Background Papers**
N/A

32 Glossary of terms

Glossary of Terms:

A complete glossary of planning terms can be found at the Planning Portal website: <http://www.planningportal.gov.uk/> The glossary below should be used as a guide only and should not be considered the source for statutory definitions.

Affordable Housing - Affordable Housing addresses the needs of eligible households unable to access or afford market housing. Affordable housing includes social rented and intermediate housing and excludes low cost market housing. Affordable housing should be available at a cost low enough for eligible households to afford, determined with regard to local incomes and local house prices. Provision of affordable housing should remain at an affordable price for future eligible households or if these restrictions are lifted, for the subsidy to be recycled for alternative affordable housing provision. Further information can be found in Planning Policy Statement 3 (Housing)

Area Action Plan (AAP) – Area action plans are part of the Local Development Framework. They should be used when there is a need to provide the planning framework for areas where significant change or conservation is needed. Area action plans should deliver planned growth areas, stimulate regeneration, protect areas particularly sensitive to change, resolve conflicting objectives in areas subject to development pressures or focus the delivery of area based regeneration initiatives.

Area for Intensification (AFI) – As defined in the London Plan. Areas that have significant potential for increases in residential, employment and other uses through development of sites at higher densities with more mixed and intensive use.

Brownfield land - Previously developed land is that which is or was occupied by a permanent structure (excluding agricultural or forestry buildings), and associated fixed-surface infrastructure. The definition covers the curtilage of the development. Planning Policy Statement Note 3 (Housing) has a detailed definition

Community Plan – refer to Sustainable Community Strategy (SCS).

Comparison Retailing - Comparison retailing is the provision of items not obtained on a frequent basis. These include clothing, footwear, household and recreational goods.

Convenience Retailing - Convenience retailing is the provision of everyday essential items, including food, drinks, newspapers/magazines and confectionery.

Conservation Area - An area of special architectural and historic interest, the character or appearance of which it is desirable to preserve or enhance.

Core Strategy (CS) - The Core Strategy is the key plan within the Local Development Framework. Every local planning authority should produce a core strategy which includes:

- an overall vision which sets out how the area and the places within it should develop;
- strategic objectives for the area focusing on the key issues to be addressed;
- a delivery strategy for achieving these objectives. This should set out how much development is intended to happen where, when, and by what means it will be delivered. Locations for strategic development should be indicated on a key diagram; and
- clear arrangements for managing and monitoring the delivery of the strategy.

Development Plan Documents (DPD) – The statutory component parts of the [local development framework](#). Core strategies, area action plans and site-specific allocations are all DPDs. The procedure for their creation is set out in [Planning Policy Statement 12](#).

District Centre – District centres comprise groups of shops often containing at least one supermarket or superstore, and a range of non-retail services, such as banks, building societies and restaurants, as well as local public facilities such as a library.

Floodplain - Generally low lying areas adjacent to a watercourse, tidal lengths of the river or sea, where water flows in times of flood or would flow but for the presence of flood defences.

Floorspace (gross) - all floorspace enclosed within the building envelope

Floorspace (net - for retail purposes) - area of a shop that is accessible to the public: sales floor, sales counters, checkouts, lobby areas, public serving and in-store cafes. Excludes goods storage, delivery, preparation area, staff offices, staff amenity facilities, plant rooms, toilets and enclosed public stairwells and lifts between floors.

Historic environment – All aspects of the environment resulting from the interaction between people and places through time, including all surviving physical remains of past human activity, whether visible, buried or submerged, and landscaped and planted or managed flora. Those elements of the historic environment that hold significance are called heritage assets.

Independent Examination - The process by which a planning inspector may publicly examine a [development plan document](#) before issuing a binding report.

Intermediate Housing – housing at prices and rents above social rent but below market price or rents. This includes various forms of shared equity products. Homes provided by the private sector or without grant funding may be included for planning purposes as affordable housing, providing these homes meet the affordable housing definition as set out in Planning Policy Statement 3 (Housing).

Issues, Options and Preferred Options - The pre-submission/ pre-production consultation stages on a [development plan document \(DPD\)](#) with the objective of gaining public consensus before submission of documents for [independent examination](#).

Listed Buildings - These are buildings or other built structures included in the statutory list of buildings of special architectural or historic interest of national significance, which is compiled by the Secretary of State for Culture, Media and Sport. Buildings are graded and are protected both internally and externally. [Listed building consent](#) is required for almost all works to a listed building.

Local Area Agreement (LAA) - Local Area Agreement is normally a three year agreement, based on the Sustainable Community Strategy vision that sets out improvement targets for the priorities of a local area. The agreement is made between central government and local authorities and their partners on the Local Strategic Partnership.

Local Centre - Local centres include a range of small shops of a local nature, serving a small catchment. Typically, local centres might include services such as a small supermarket, a newsagent, a sub-post office and a pharmacy. Other facilities could include a hot-food takeaway and launderette.

Local Development Framework (LDF) - The Local Development Framework is the collection of local development documents produced by the local planning authority which collectively delivers the spatial planning strategy for its area.

Local Development Scheme (LDS) - This is the business plan for production of the [local development framework](#) (LDF). It identifies and describes the [development plan documents](#) (DPDs) and when they will be produced. It covers a three-year timespan and is subject to updating following production of an annual monitoring report to check progress.

Local Planning Authority (LPA) - The statutory authority (usually the local council) whose duty it is to carry out the planning function for its area.

Local Strategic Partnerships (LSP) - Local Strategic Partnerships are not statutory bodies, but they bring together the public, private and third sectors to coordinate the contribution that each can make to improving localities.

London Plan - the London Plan is the Spatial Development Strategy for London. Produced by the Mayor of London, it provides the overall spatial vision for the entire region, identifying the broad locations for growth, often by identification of sub-regions, and major infrastructure requirements, together with the housing numbers to be provided for in boroughs' LDFs. It is part of the development plan for Merton together with the borough's LDF documents and should be used to guide planning decisions across London. All LDF documents have to be in general conformity with the London Plan

Major Centre – Major Centres normally have over 50,000 square metres of retail floorspace and their retail offer is derived from a mix of both comparison and convenience shopping. Some Major Centres, which have developed sizeable catchment areas, also have some leisure and entertainment functions.

Neighbourhood parades: convenience shopping and other services easily accessible to those walking or cycling or those with restricted mobility.

Planning Application - Administrative process by which permission to carry out development is sought from the [local planning authority](#). The form and content of the application is laid down in guidance. Application can be made in outline or detailed form for some categories of development.

Planning Permission - The consent given by the [local planning authority](#) for building operations as defined in the Town and Country Planning Act that do not constitute permitted development as defined in the general permitted development order (GPDO) or uses permitted by the use classes order. Usually subject to conditions and sometimes a legal agreement.

Planning Policy Guidance Notes (PPG) - Former central government guidance on a range of topics from transport to retail policy giving advice to regional and local policy makers on the way in which they should devise their policies in order to meet national planning goals. Most PPGs have now been replaced by [Planning Policy Statements](#).

Planning Policy Statements (PPS) - Planning Policy Statements set out the government's policies on different aspects of spatial planning in England.

Public realm - This is the space between and surrounding buildings and open spaces that are accessible to the public and include streets, pedestrianised areas, squares, river frontages etc.

Spatial Development Strategy - (see London Plan)

Registered Social Landlords (RSLs) - Technical name for independent, not-for-profit organisations registered with and regulated by the Housing Corporation. These organisations own or manage affordable homes and are able to bid for funding from the Housing Corporation.

Sequential Test - A planning principle that seeks to identify, allocate or develop certain types or locations of land before others.

Scattered Employment Sites - An employment site that is not a designated employment site (a site that is not a Strategic Industrial Location or Locally Significant Industrial Site) as detailed in Merton's adopted Proposals Map (as amended).

Sites of Importance for Nature Conservation (SINC) - Locally important sites of nature conservation adopted by local authorities for planning purposes and identified in the local development plan.

Site of Special Scientific Interest (SSSI) – A site identified under the Wildlife and Countryside Act 1981 (As amended by the Countryside and Rights of Way Act 2000) as an area of special interest by reason of its plants, animals and natural features relating to the earth's structure.

Socially Rented Housing - Planning Policy Statement 3 (Housing) states that this is rented housing owned and managed by local authorities and registered social landlords for which guideline target rents are determined through the national rent regime. It may also include rented housing owned or managed by other persons and provided under equivalent rental arrangements (specified in Planning Policy Statement Note 3) as agreed with the local authority or the Housing Corporation as a condition of grant.

Statement of Community Involvement (SCI) - This sets out how, when and where the council will consult with local and statutory stakeholders in the process of planning for the local authority area, both in producing development plan documents and in carrying out the development control function.

Strategic Environmental Assessment (SEA) - A process of environmental assessment of certain plans and programmes which are likely to have significant effects on the environment. It is required by European Directive 2001/42/EC (the Strategic Environmental Assessment or SEA Directive)

Supplementary Planning Documents (SPDs) - Supplementary planning documents (SPDs) give further guidance on specific policy topic areas such as [affordable housing](#) provision, that have been identified in core policy in the [local development framework](#) (LDF), or to give detailed guidance on the development of specific sites in the form of a [master plan](#) framework plan or [development brief](#).

Sustainability Appraisal (SA) - A form of assessment used in the UK, particularly for regional and local planning, since the 1990s. It considers environmental, social and economic effects of a plan and appraises them in relation to the aims of sustainable development. Sustainability appraisals that fully incorporate the requirements of the [SEA Directive \(2001/42/EC\)](#) are required for [local development documents](#) and [regional spatial strategies](#) in England under the Planning and Compulsory Purchase Act 2004.

Sustainable Community Strategy (SCS) – The overarching strategy for the local area, which sets out of the overall direction and vision for the borough over the next 10 years. It tells the story of a place, and what needs to be achieved for the local area and its residents, based on what is most important to them. It is also known as the Community Plan.

Sustainable development - Development which meets the needs of the present generation without compromising the ability of future generations to meet their own needs.

Town centre type uses – In accordance with PPS4 Planning for Sustainable Economic Development, the main uses to which town centres policies can apply are:

- retail development (including warehouse clubs and factory outlet centres)

- leisure, entertainment facilities, and the more intensive sport and recreation uses (including cinemas, restaurants, drive-through restaurants, bars and pubs, night-clubs, casinos, health and fitness centres, indoor bowling centres, and bingo halls)
- offices, and arts, culture and tourism development (including theatres, museums, galleries and concert halls, hotels and conference facilities)

Unitary Development Plan (UDP) - This is the former statutory development plan system and contains the framework to guide development in local areas. Under the Planning and Compulsory Purchase Act 2004 it was replaced by Local Development Frameworks. UDP policies will continue to operate for a time after the commencement of the new development plan system in 2004, by virtue of specific transitional provisions, though they should all be eventually superseded by LDF policies

Use Classes Order – Schedule of class of use for land and buildings under Town and Country Planning (Use Classes) Order 1987 as amended 2005:

A1	Shops
A2	Financial and Professional Services
A3	Restaurants and Cafés
A4	Drinking Establishments
A5	Hot Food Take-away
B1	Business
B2	General Industrial
B8	Storage or Distribution
C1	Hotels
C2	Residential Institution
C2A	Secure Residential Institution
C3	Dwelling Houses
D1	Non-residential Institutions
D2	Assembly and Leisure
Other	Sui Generis